

THE HIGHLAND COUNCIL

SOUTH PLANNING APPLICATIONS COMMITTEE
10 November 2015

Agenda Item	7.6
Report No	PLS/082/15

15/01520/FUL: Bridgend Filling Station
1 Bridgend Filling Station, Station Road, Beauly

Report by Area Planning Manager – South

SUMMARY

Description : Alterations to existing filling station shop and launderette to form sorting office, post office counter and erection of new building to form laundry/dry cleaners and store.

Recommendation – GRANT

Ward : 13 - Aird and Loch Ness

Development category : Local

Pre-determination hearing: None

Reason referred to Committee : Unresolved objections from a statutory consultee i.e. the Community Council.

1. PROPOSED DEVELOPMENT

- 1.1 The proposal involves internal ground floor alterations to the existing Bridgend filling station at Beauly to form a Royal Mail sorting office together with a new filling station shop area. It is also proposed to erect a separate 1½ storey building to re-house the expanded laundry business and this will be located to the southwest of the existing building. The upper floor area of the new building is to be used for storage of laundry. The original building will continue in use as the shop serving the filling station with part allocated as the proposed sorting office and launderette.
- 1.2 Informal pre-application advice was given per 14/03743/PREAPP advising that the proposed extension may be acceptable in principle provided information was submitted about the footprint, proposed uses, parking provision and surface water drainage.
- 1.3 There are two entrances to the filling station with on-site parking for the public to the southwest boundary. A Drainage Impact Assessment was submitted to assess the surface water drainage.
- 1.4 Design and Access Statement and Drainage Impact Assessment were included within the application submissions.

1.5 Variations

Drawing 05 A shows an additional 2 parking spaces to meet Transport Planning requirements.

Drawings 02 REV A, 03 REV A, 04 REV A, and 11 REV A show the location of the laundry within the new building.

2. SITE DESCRIPTION

- 2.1 The site is situated to the west of Beauly village, on the north side of the A892, and comprises the established existing filling station with an associated shop, launderette and separate retail unit accommodated within a one and a half storey building located to the rear of the site. The upper floors of the building have consent for use as two flats. The remainder of the site comprises associated car parking facilities. The site takes access from the main road serving Beauly.

3. PLANNING HISTORY

- 3.1 99/00947/FULIN - Use of existing residential accommodation as community nurse's base - Granted
- 3.2 03/01204/FULIN - Alterations and extension to shop and first floor flat - Granted
- 3.3 14/03743/PREAPP - Accept the proposed extension in principle subject to there being no adverse impact on the amenity of the area as a whole and subject to the detailed design and resolution of technical issues such as surface water drainage.

4. PUBLIC PARTICIPATION

- 4.1 Advertised : n/a

Representation deadline : 17.05.15 and 18.10.15 (further re-notification due to change in annotation of plans to clarify the location of the laundry - no further representations received)

Timeous representations : 1

Late representations : 3

- 4.2 Material considerations raised are summarised as follows:

- Traffic generation and insufficient parking for the post office and laundry
- Need for on-site cycle provision

- 4.3 All letters of representation are available for inspection via the Council's eplanning portal which can be accessed through the internet www.wam.highland.gov.uk/wam. Access to computers can be made available via Planning and Development Service offices.

5. CONSULTATIONS

- 5.1 **Beauly Community Council:** object to the proposal for a number of reasons predominantly associated with the increase in use by traffic, road safety implications and the impact of additional traffic on the existing road junctions. Their comments may be summarised as follows:-

Traffic and pedestrian safety - increase vehicle traffic to/from the filling station.

Worsen the current traffic problems at the two adjacent junctions being the Co-op store and the residential areas to the rear together with the parallel entrance/egress of the filling station. Increase in traffic volumes especially for access to the Post Office. It is also pointed out that there has been a noticeable increase in the number of vehicle “shunts” in this area since the Co-op store opened.

The vehicle exits at the filling station and the Co-op store/residents’ junction and the impending pedestrian controlled crossing, have to be considered in the whole and not individually, to devise a better traffic flow arrangement. At present, the filling station has two entrances and 2 exits (two-way traffic on-site). Improvement in safety could be made by making the filling station site have dedicated in and out access points. There is also an opportunity to provide yellow box “Safer route to School” markings on the site exits which would prevent vehicles stopping and parking over the pedestrians crossing points at the filling station entrance.

Proposed new build (laundry) - the current launderette is always busy and provides “service washes” for the local hotels and guest houses as well as a self-serve section for local residents. Cannot envisage how, by reducing the current launderette to a quarter of its current size in the existing building, it can still provide the same services.

Post Office/ Sorting Office - at the present location of the post office, up to 5 Royal Mail vans park overnight at the Phipps Hall. In the evenings and weekends there will be fewer available parking spaces on the new site as these will be taken up by overnight and weekend parking of Post Office vehicles. This should be restricted. Concerns that vans associated with the PO/Sorting Office are more likely to park dangerously outside the sorting office rather than in the allocated parking spaces, whilst loading/unloading.

Construction Phase - Transport Planning need to liaise with all parties involved in this application, to minimise the risks and impact caused by initially the building contractors etc involved and, especially, any potential risks to local school children using this route and the anticipated increased traffic.

5.2 **Transport Planning:**

Drainage – The Drainage Impact Assessment has been considered in respect of dealing with surface water drainage and the proposals for permeable paving are acceptable.

Access and Egress – Following a meeting with the Trading Standards Petroleum Officer they have amended their original comments. To try and reduce the amount of vehicle movements near to the Station Road/Priory Walk junction, Transport Planning had looked at the creation of a one way system in and out of the filling station. Due to the design of the filling station and design guidance based on ‘Design, Construction, Modification, Maintenance and Decommissioning of Filling Stations’ published by the Association for Petroleum and Explosives Administration (which has been adopted by the Council), a one-way system will result in delivery tankers having to make manoeuvres in reverse gear which is contrary to the above guidance. Rather, an improvement to the existing accesses is now sought through provision of road markings clearly demarcating the lanes into and out of the site with give way markings.

Vehicle Trips – satisfied the impact of traffic generated by post office vehicles will not have a significant impact as it will occur before the morning peak period. The applicant has stated that mail will be delivered at 6am by a single 7.5 tonne lorry and after sorting, the 9 small/medium vans driven by postal workers will have left the site. No postal vehicles are left on site overnight. It is likely that postal vehicles may need to return to the post office during the day to return un-delivered mail and packages but it is not anticipated that these trips will be of any significant volume which would have a detrimental effect.

The laundry provides a pick-up service which aims to reduce the number of vehicle movements to the site and there is unlikely to be a significant rise in the number of trips for petrol due to the continuing reduction in sales that the business has experienced.

There is the potential for the post office counter to create an increase trip generation at the site, however the site is well served by pedestrian facilities and there are likely to be a large number of linked trips to the site from people using the supermarket which will not create an increase in vehicle trips to the filling station. To encourage more linked trips and walking to the site, improvement of the existing path across the landscaped area to the footway on Priory Walk will be required.

Cycle Parking – require a dimensioned drawing showing the provision of cycle parking for the two flats and for visitors to the site with

- 2 cycle parking spaces per flat within a secure enclosed storage facility. This may be reduced to 1 space per flat if communal storage is provided.
- 2 cycle parking spaces for customers to be located as close to the main entrance as possible

Car Parking – satisfied with the provision of 14 car parking spaces, which is based on the maximum parking standards for the various uses within the proposed development. There is also the potential for parking on the forecourt which will also help to alleviate any pressure on the proposed car park.

Waste and Recycling – satisfied with the provision of waste and recycling facilities for the site. It is noted that there are car parking areas in front of the bin stores for the workshop and flats, therefore there is a potential risk of damage to vehicles as bins are moved for emptying.

5.3 **Trading Standards Petroleum Officer** – raised concerns regarding the preferred option identified by Transport Planning for a one way system within the site. Safety issues based on current Regulations make this option unacceptable. The revised proposal to take account of these Regulations is now considered acceptable.

5.4 **Environmental Health** – In terms of health and safety any laundry or dry cleaners would require to be well ventilated. Certain types of commercial tumble dryers require exhaust ducting to the atmosphere and so the laundry should be designed with at least one external wall. In addition the building partition walls must be sound to prevent any solvent fumes entering the neighbouring premises. As the premises also contain a dry cleaner which uses Perchloroethylene, guidance suggests that it should have an extractor fan to aid ventilation.

6. SUPPORTING INFORMATION

- 6.1 The business has come under pressure over the last few months since the Co-op store relocated from the centre of Beauly. This has resulted in a sharp reduction in revenue from the fuel/shopping side of the business. However, the launderette remains busy. To capitalise on the more viable parts of the business the applicant seeks to construct new facilities on-site to provide a purpose built commercial laundry and dry cleaners, and alter the ground floor of the existing building to add new facilities for a Royal Mail sorting office. The filling station and shop will remain, but relocated to another area of the ground floor.

7. DEVELOPMENT PLAN POLICY

The following policies are relevant to the assessment of the application

7.1 Highland Wide Local Development Plan 2012

28	Sustainable Design
29	Design Quality and Place-Making
34	Settlement Development Areas
41	Retail
64	Flood Risk
66	Surface Water Drainage

7.2 Inner Moray Firth Local Development Plan (October 2015)

8. OTHER MATERIAL CONSIDERATIONS

8.1 Highland Council Supplementary Planning Policy Guidance

Drainage Impact Assessment

8.2 Scottish Government Planning Policy and Guidance

Not applicable

9. PLANNING APPRAISAL

- 9.1 Section 25 of the Town and Country Planning (Scotland) Act 1997 requires planning applications to be determined in accordance with the development plan unless material considerations indicate otherwise.
- 9.2 This means that the application requires to be assessed against all policies of the Development Plan relevant to the application, all national and local policy guidance and all other material considerations relevant to the application.

9.3 Development Plan Policy Assessment

- 9.3.1 The proposal involves internal ground floor alterations to the existing filling station to form a new Royal Mail sorting office on the east end and move the filling station/shop area to the west end. There will also be a separate 1½ storey building to re-house the expanded laundry business and this will be located to the southwest of the existing building. This will comprise the laundry on the ground floor and storage room on the first floor. The existing building is an existing retail operation which has diversified to incorporate a laundry business. The internal layout of the existing building will be reconfigured to relocate the retail sales area to the west side of the building where the existing laundry is located. A small launderette will be retained and a postal sorting office will be formed in the east end of the building. This development will expand the existing retail facility.
- 9.3.2 The site lies within the settlement boundary of Beauly and therefore HwLDP Policy 34 applies. In terms of the relevant criteria the proposal is viewed as compatible with the existing pattern of development as this is an existing petrol filling station which includes other retail activities. The erection of a new building is an expansion of the existing laundry business and the floorspace is viewed as reasonable and does not amount to over-development of the site.
- 9.3.3 In terms of design the new building reflects the design of the existing units in terms of height, orientation and external finishes. Its location to the southwest of the existing building is considered the most appropriate and will not impinge on the forecourt and minimises visual impact. From that point of view the proposal meets HwLDP Policy 28 in terms of siting and the quality of design and use of materials.

9.4 Material Considerations

- 9.4.1 **Traffic Safety, Pedestrian Safety and Parking & Cycling Provision** – there is concern from the Community Council and two residents about the increase in traffic in the area with the re-location of the Co-op Store to the former House of Beauly building to the southwest. They perceive this proposal will increase traffic from the filling station and add to current traffic problems. Transport Planning are however satisfied with the information supplied by the applicant in terms of traffic generation from the site and it is not considered that the new uses will result in a significant increase in traffic. The site is well served by pedestrian facilities. The proposed improvement of the existing path across the landscaped area within the site to the footway on Priory Walk will encourage more linked trips and walking to the site. In addition, it is understood that the laundry operates a pick-up service which aims to reduce the number of vehicle movements to the site. The proposed pedestrian crossing is to be located further along the road at the Co-op store. Transport Planning are satisfied with the proposed provision for cycles and car parking for the post office and laundry.

Access and Egress – the proposal to retain the existing ‘in and out’ system is based on sound safety considerations and it is not possible to restrict this to a one way system. A balance has to be struck in terms of safety and the potential for conflict between HGV fuel tankers and other vehicle users and pedestrians. It is considered safer to retain the existing situation with improved pedestrian facilities.

Proposed new build (laundry) – the applicant has confirmed the new building will be used as a laundry. The hazardous materials store in the southwest corner is to be used for chemicals associated with the laundry business, presently stored within the existing building. Environmental Health are satisfied with the proposed laundry subject to provision of ventilation for the commercial tumble dryers.

The applicant is unsure at the moment as to whether the sale of LPG canisters will continue and, if so, they will need to be located to the front of the site.

Post Office/Sorting Office – the applicant advises there will be no vans parked on site after deliveries have been made. A condition to restrict overnight and weekend parking of Post Office vehicles loading/unloading would be unreasonable and difficult to enforce. The forecourt of the filling station remains open outwith working hours and there is residential accommodation above the filling station.

Construction Phase – it is accepted that there will be disruption during construction and the contractors will have to consider their operations and how to deal with vehicular and pedestrian movements both within and outwith the site. However, this cannot be controlled through condition.

9.5 **Other Considerations – not material**

- 9.5.1 The Community Council seeks a yellow box for “Safer route to School” markings on the site exits to stop vehicles stopping and parking over the pedestrians crossing points at the filling station. However, this is not a requirement of Transport Planning and it would therefore be unreasonable to require this. A pedestrian crossing will be provided on the Co-op storage frontage with the public road.

10. **CONCLUSION**

- 10.1 All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

11. **RECOMMENDATION**

Action required before decision issued N

Notification to Scottish Ministers N

Notification to Historic Scotland N

Conclusion of Section 75 Agreement N

Revocation of previous permission N

Subject to the above, it is recommended the application be **Granted** subject to the following conditions and reasons:

1. Prior to the first occupation of the development hereby approved, the car parking and access arrangements detailed on approved plan ref. 05 Rev A shall be constructed and completed in full and made available for use. Thereafter, all car parking spaces shall be maintained for this use in perpetuity.

Reason: In order to ensure that the level of off-street parking is adequate for the development proposed.

2. No development shall commence on site until full details of a covered and secure communal bicycle storage/racking system for bicycles for the existing and proposed uses have been submitted to, and approved in writing by, the Planning Authority. Thereafter, the storage/racking system shall be installed in accordance with these approved details prior to the first occupation of the development hereby approved.

Reason: In order to facilitate the use of a variety of modes of transport.

3. No development shall commence on site until full details of an improvement of the existing path into the site from Priory Walk have been submitted to, and approved in writing by, the Planning Authority. Thereafter, the improvements shall be formed and completed in accordance with the approved details prior to the first occupation of the development hereby approved.

Reason: In the interests of pedestrian safety.

4. No development shall commence on site until a detailed specification for all proposed external materials and finishes (including trade names and samples where necessary) has been submitted to, and approved in writing by, the Planning Authority. Thereafter, development and work shall progress in accordance with the approved details.

Reason: In the interests of visual amenity.

5. The proposed waste and recycling bin storage area shown on the approved plans shall be constructed and completed prior to the first occupation of the development hereby approved and thereafter maintained for that purpose to the satisfaction of the Planning Authority.

Reason: To ensure that suitable provision is made for the storage of communal waste and recycling bins.

6. All surface water drainage provision within the application site shall be implemented in accordance with the approved Drainage Impact Assessment submitted by HGA Engineers and shall be completed prior to the first occupation of the development.

Reason: To ensure that surface water drainage is provided timeously and complies with the principles of SUDS; in order to protect the water environment.

7. No development shall commence on site until details of road markings to clearly demarcate the lanes into and out of the site with give way markings in accordance with Council Guidelines have been submitted to and approved in writing by, the Planning Authority. The approved road markings shall thereafter be provided and completed to the satisfaction of the Planning Authority prior to the first occupation of the development hereby approved.

Reason: In the interests of road traffic safety.

8. All plant, machinery and equipment associated with ventilation, air-conditioning, heating and refrigeration services or similar and including fans, ducting and external openings shall be so installed, maintained and operated such that any associated operating noise does not exceed NR 20 when measured or calculated within any noise-sensitive premises with windows open for ventilation purposes. For the purposes of this condition, "noise-sensitive premises" includes, but is not necessarily limited to, any building, structure or other development the lawful use of which a) falls within Classes 7 (Hotels & Hostels), 8 (Residential Institutions) or 9 (Houses) of the Town and Country Planning (Use Classes) (Scotland) Order 1997 (as amended), or b) is as a flat or static residential caravan.

Reason: In order to safeguard the amenity of neighbouring properties and occupants.

9. No development shall commence until full details of all external ducting and other elements of the proposed ventilation system have been submitted to, and approved in writing by, the Planning Authority. Thereafter, development shall progress in accordance with these approved details prior to the first occupation of the development hereby approved.

Reason: In order to safeguard the amenity of neighbouring properties and occupants.

REASON FOR DECISION

The proposals accord with the provisions of the Development Plan and there are no material considerations which would warrant refusal of the application.

TIME LIMITS

In accordance with Section 58 of the Town and Country Planning (Scotland) Act 1997 (as amended), the development to which this planning permission relates must commence within THREE YEARS of the date of this decision notice. If development has not commenced within this period, then this planning permission shall lapse.

FOOTNOTE TO APPLICANT

Initiation and Completion Notices

The Town and Country Planning (Scotland) Act 1997 (as amended) requires all developers to submit notices to the Planning Authority prior to, and upon completion of, development. These are in addition to any other similar requirements (such as Building Warrant completion notices) and failure to comply represents a breach of planning control and may result in formal enforcement action.

1. The developer must submit a Notice of Initiation of Development in accordance with Section 27A of the Act to the Planning Authority prior to work commencing on site.
2. On completion of the development, the developer must submit a Notice of Completion in accordance with Section 27B of the Act to the Planning Authority.

Copies of the notices referred to are attached to this decision notice for your convenience.

Accordance with Approved Plans & Conditions

You are advised that development must progress in accordance with the plans approved under, and any conditions attached to, this permission. You must not deviate from this permission without consent from the Planning Authority (irrespective of any changes that may separately be requested at the Building Warrant stage or by any other Statutory Authority). Any pre-conditions (those requiring certain works, submissions etc. prior to commencement of development) must be fulfilled prior to work starting on site. Failure to adhere to this permission and meet the requirements of all conditions may invalidate your permission or result in formal enforcement action.

Flood Risk

It is important to note that the granting of planning permission does not imply there is an unconditional absence of flood risk relating to (or emanating from) the application site. As per Scottish Planning Policy (p.198), planning permission does not remove the liability position of developers or owners in relation to flood risk.

Scottish Water

You are advised that a supply and connection to Scottish Water infrastructure is dependent on sufficient spare capacity at the time of the application for connection to Scottish Water. The granting of planning permission does not guarantee a connection. Any enquiries with regards to sewerage connection and/or water supply should be directed to Scottish Water on 0845 601 8855.

Local Roads Authority Consent

In addition to planning permission, you may require one or more separate consents (such as dropped kerb consent, a road openings permit, occupation of the road permit etc.) from TECS Roads prior to work commencing. These consents may require additional work and/or introduce additional specifications and you are therefore advised to contact your local TECS Roads office for further guidance at the earliest opportunity.

Failure to comply with access, parking and drainage infrastructure requirements may endanger road users, affect the safety and free-flow of traffic and is likely to result in enforcement action being taken against you under both the Town and Country Planning (Scotland) Act 1997 and the Roads (Scotland) Act 1984.

Further information on the Council's roads standards can be found at:

<http://www.highland.gov.uk/yourenvironment/roadsandtransport>

Application forms and guidance notes for access-related consents can be downloaded from:

<http://www.highland.gov.uk/yourenvironment/roadsandtransport/roads/Applicationformsforroadoccupation.htm>

Mud & Debris on Road

Please note that it is an offence under Section 95 of the Roads (Scotland) Act 1984 to allow mud or any other material to be deposited, and thereafter remain, on a public road from any vehicle or development site. You must, therefore, put in place a strategy for dealing with any material deposited on the public road network and maintain this until development is complete.

Construction Hours and Noise-Generating Activities

You are advised that construction work associated with the approved development (incl. the loading/unloading of delivery vehicles, plant or other machinery), for which noise is audible at the boundary of the application site, should not normally take place outwith the hours of 08:00 and 19:00 Monday to Friday, 08:00 and 13:00 on Saturdays or at any time on a Sunday or Bank Holiday in Scotland, as prescribed in Schedule 1 of the Banking and Financial Dealings Act 1971 (as amended).

Work falling outwith these hours which gives rise to amenity concerns, or noise at any time which exceeds acceptable levels, may result in the service of a notice under Section 60 of the Control of Pollution Act 1974 (as amended). Breaching a Section 60 notice constitutes an offence and is likely to result in court action.

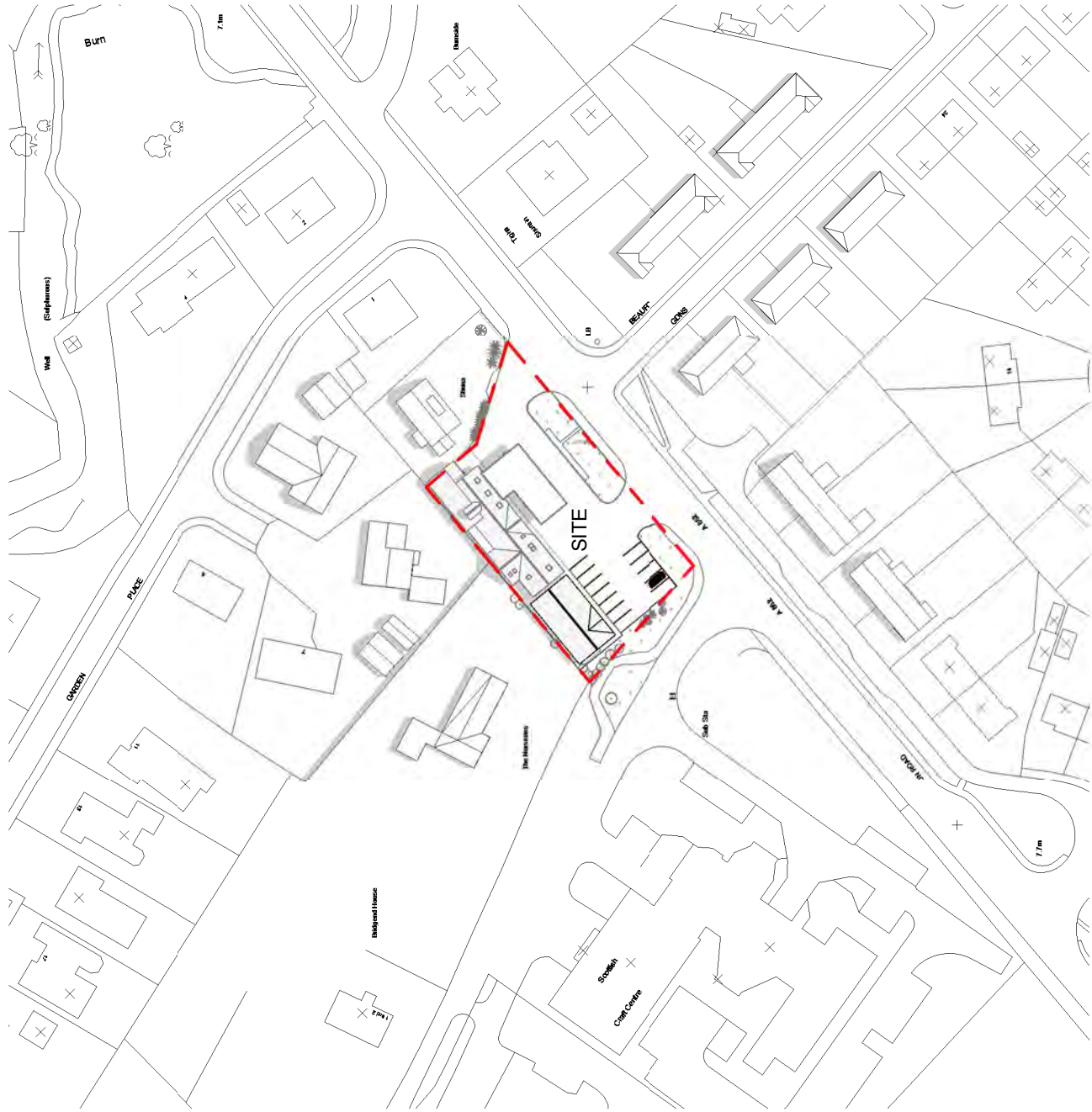
If you wish formal consent to work at specific times or on specific days, you may apply to the Council's Environmental Health Officer under Section 61 of the 1974 Act. Any such application should be submitted after you have obtained your Building Warrant, if required, and will be considered on its merits. Any decision taken will reflect the nature of the development, the site's location and the proximity of noise sensitive premises. Please contact env.health@highland.gov.uk for more information.

Signature: Allan J Todd
Designation: Area Planning Manager – South
Author: Keith Gibson
Background Papers: Documents referred to in report and in case file.
Relevant Plans: Plan 1 – Location Plan
Plan 2 – Existing floor plan/ elevations
Plan 3 – Proposed floor plan
Plan 4 – Proposed elevations
Plan 5 – Proposed site layout

NOTES AND AMENDMENTS

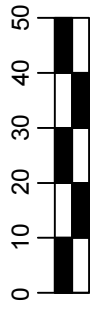
IT IS THE RESPONSIBILITY OF ALL PERSONS USING THIS DRAWING TO CHECK DIMENSIONS AND OTHER DETAILS PRIOR TO COMMENCEMENT OF WORK. ANY DISCREPANCIES MUST BE REFERRED TO THIS OFFICE FOR CLARIFICATION, OTHERWISE NO RESPONSIBILITY CAN BE ACCEPTED.

REV	Description	Drawn	Date



1 Location Plan

1 : 1250



Scale in metres

REPRODUCED FROM THE ORDNANCE SURVEY MAP WITH THE PERMISSION OF THE CONTROLLER OF HM STATIONARY OFFICE CROWN © COPYRIGHT RESERVED

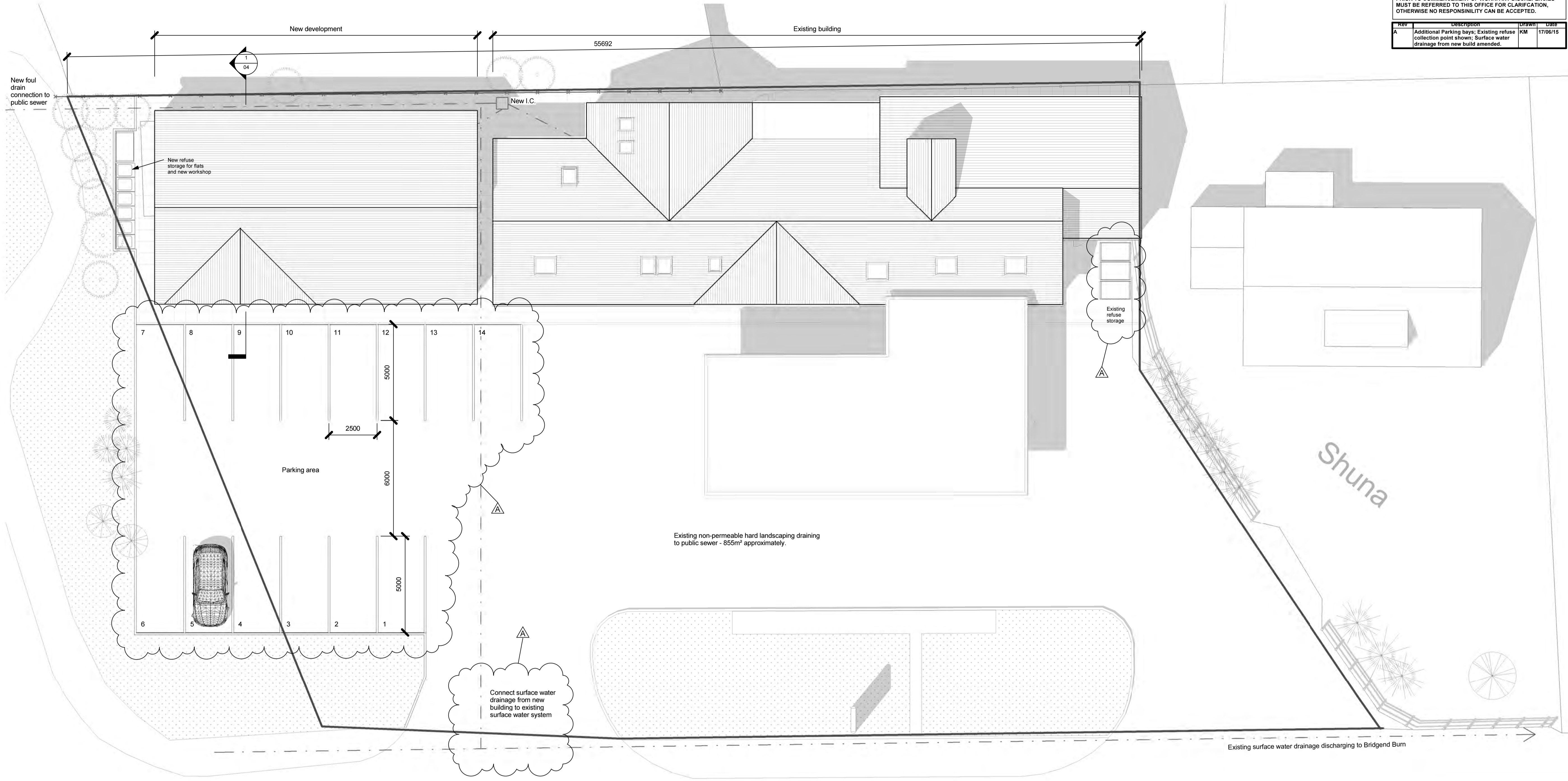
GRID REFERENCE: NH 521 460

 <p>KALM Design Chartered Architect</p> <p>45 Logan Way, Muir of Ord, IV6 7QJ</p> <p>tel : 01463 871280 email : kevin@kalmdesign.co.uk</p>	<p>Project</p> <p>Extension and Alterations Bridgend Filling Station Station Road Beaulieu IV4 7EH</p>	
	<p>Drawing Stage</p> <p>Design</p>	

Drawing Title			
Location Plan			
Drawn	Sheet Size	Date	
KM	A4	28/03/15	
Project No.	Drawing No.	Revision	
0021	06		

NOTES AND AMENDMENTS
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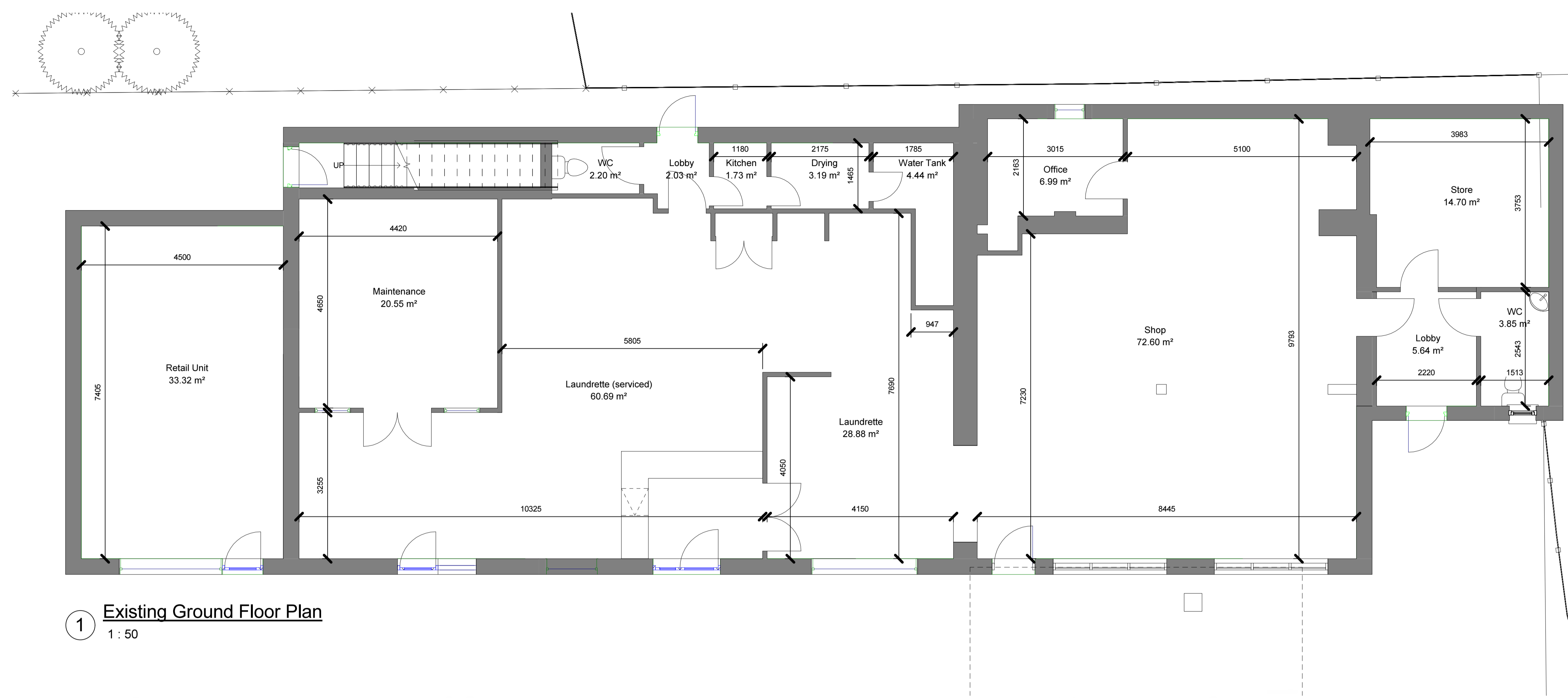
Rev	Description	Drawn	Date
A	Additional Parking bays; Existing refuse collection point shown; Surface water drainage from new build amended.	KM	17/06/15



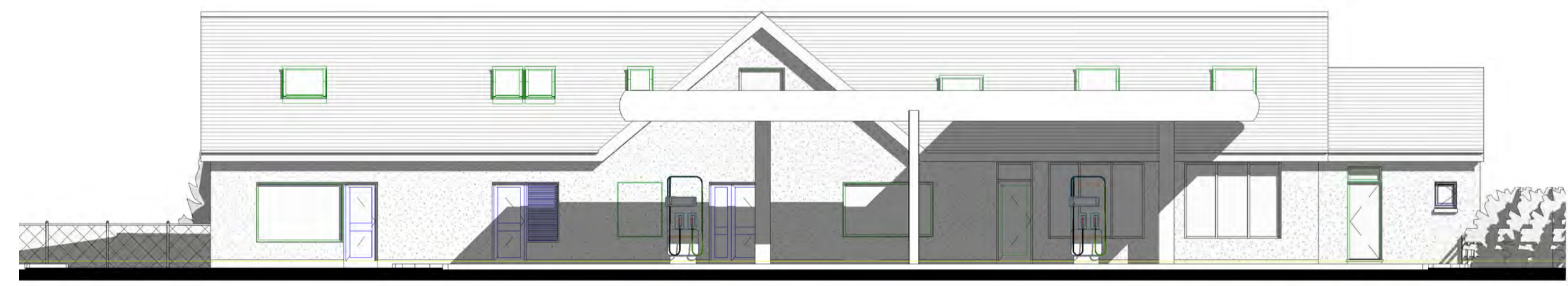
1 **Site Layout**
 1 : 100

K	KALM Design Chartered Architect	
	45 Logan Way, Muir of Ord, IV6 7QJ	
	tel : 01463 871280 email : kevin@kalmdesign.co.uk	
Project		
Extension and Alterations Bridgend Filling Station Station Road Beauly IV4 7EH		
Client		
Bridgend Filling Station		
Drawing Title		
Site Layout		
Drawing Stage		
Planning		
Drawn By	Sheet Size	Date
KM	A1	28/03/15
Project No.	Drawing No.	Revision
0021	05	A

NOTES AND AMENDMENTS
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① Existing Ground Floor Plan
 1 : 50



② Existing South East Elevation
 1 : 100

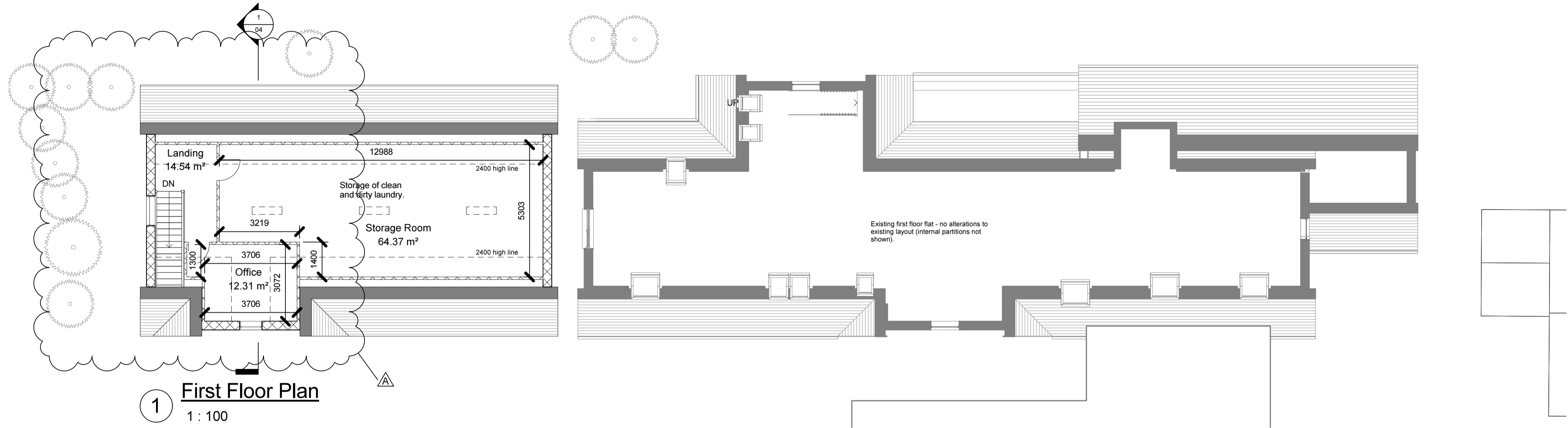


③ Existing South West Elevation
 1 : 100

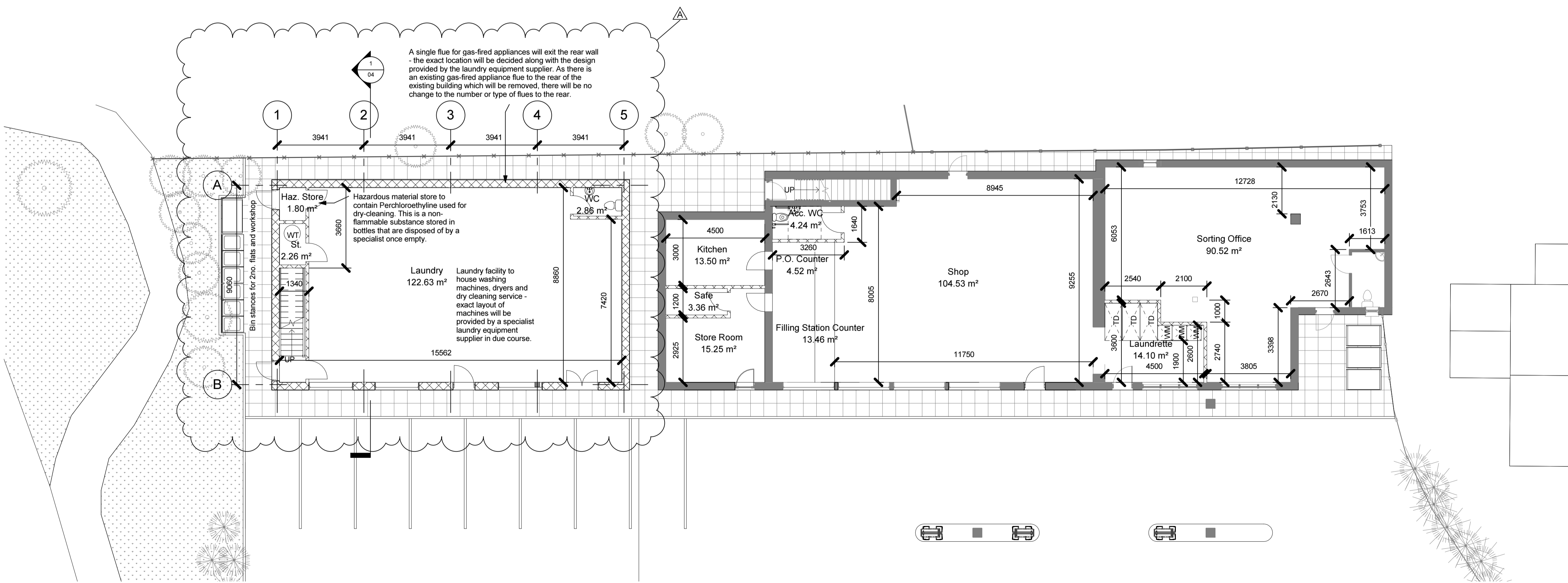


KALM Design Chartered Architect		
45 Logan Way, Muir of Ord, IV6 7QJ		
tel : 01463 871280 email : kevin@kalmdesign.co.uk		
Project		
Extension and Alterations Bridgend Filling Station Station Road Beauly IV4 7EH		
Client		
Bridgend Filling Station		
Drawing Title		
Existing Floor Plans		
Drawing Stage		
Planning		
Drawn By	Sheet Size	Date
KM	A1	01/01/15
Project No.	Drawing No.	Revision
0021	01	

NOTES AND AMENDMENTS			
IT IS THE RESPONSIBILITY OF ALL PERSONS USING THIS DRAWING TO CHECK DIMENSIONS AND OTHER DETAILS PRIOR TO COMMENCEMENT OF WORK. ANY DISCREPANCIES MUST BE REFERRED TO THIS OFFICE FOR CLARIFICATION. OTHERWISE NO RESPONSIBILITY CAN BE ACCEPTED.			
Rev	Description	Drawn	Date
A	New building stair revised and enclosed; external doors added to stair and hazard store; minor layout amendment to first floor; explanation notes added and Workshop room label changed to Laundry.	KM	18/09/15



1 **First Floor Plan**
1 : 100



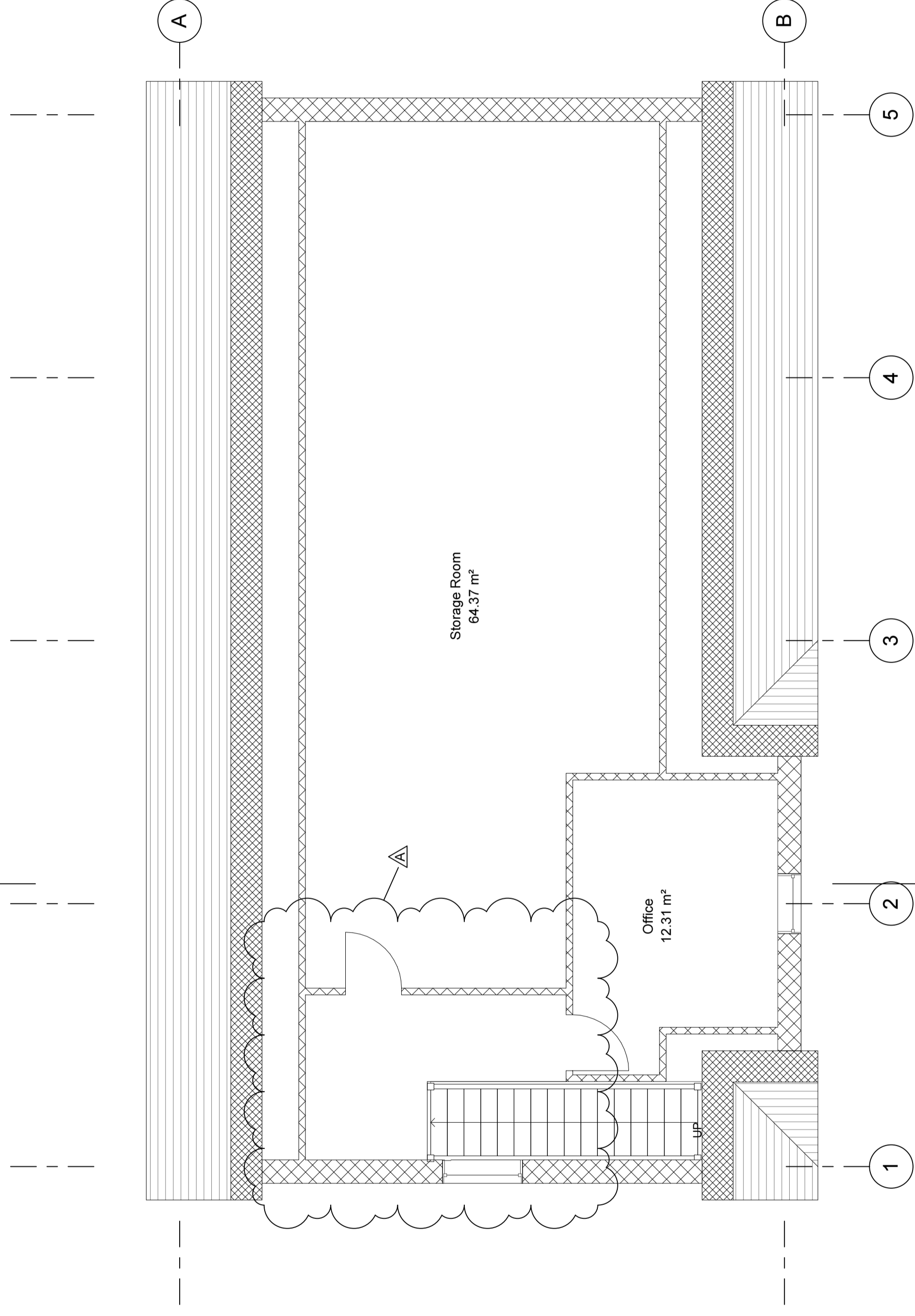
2 **Ground Floor Plan**
1 : 100

 KALM Design Chartered Architect		
45 Logan Way, Muir of Ord, IV6 7QJ tel : 01463 871280 email : kevin@kalmdesign.co.uk		
Project Extension and Alterations Bridgend Filling Station Station Road Beauly IV4 7EH		
Client Bridgend Filling Station		
Drawing Title Floor Plans		
Drawing Stage Planning		
Drawn By KM	Sheet Size A1	Date 20/03/15
Project No. 0021	Drawing No. 02	Revision A

NOTES AND AMENDMENTS

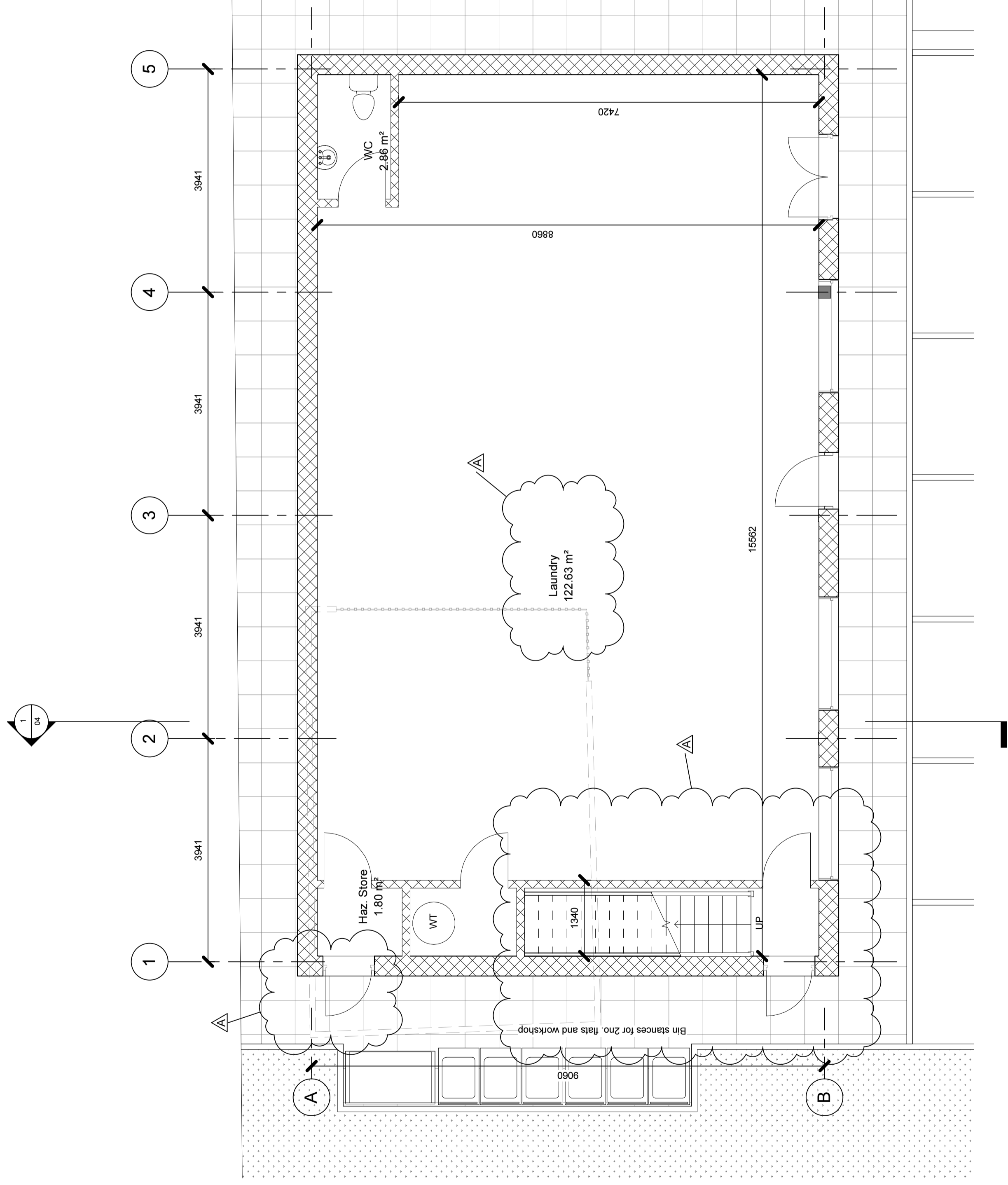
1. THE ARCHITECT'S RESPONSIBILITY IS TO CHECK DIMENSIONS AND OTHER DETAILS PRIOR TO COMMENCEMENT OF WORK. ANY DISCREPANCIES BETWEEN THE DRAWINGS AND THE INFORMATION PROVIDED BY THE CLIENT, OTHERWISE NO RESPONSIBILITY CAN BE ACCEPTED.

NO.	DESCRIPTION	DATE
A	New building structure and enclosed external doors added to stair and hazard room; minor alterations made to first floor; minor alterations made to first floor; minor alterations made to first floor; minor alterations made to first floor; Workshop room label changed to Laundry.	18/06/15



2 **Workshop First Floor Plan**

1 : 50



1 **Workshop Ground Floor Plan**

1 : 50



KALM Design
Chartered Architect

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Muir of Ord, IV6 7QJ
Tel : 01463 871290
email : kevin@kalmdesign.co.uk

Project
Extension and Alterations
Bridgend Filling Station
Station Road
Beauy
IV4 7EH
Client

Bridgend Filling Station
Drawing Title

Workshop Floor Plans

Drawing Stage

Planning

Drawn By

KM

Project No.

0021

Sheet Size

A1

Date

18/06/15

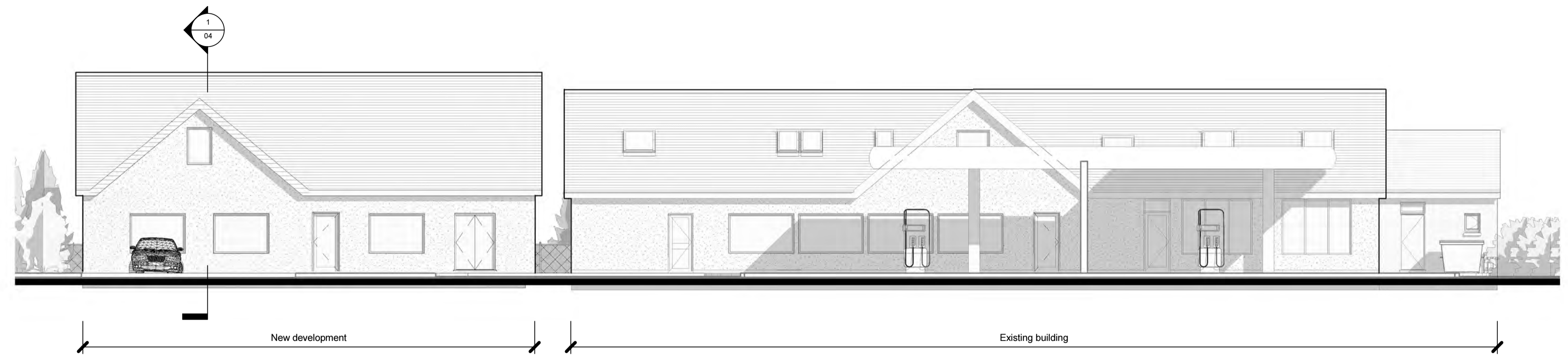
Revision

11

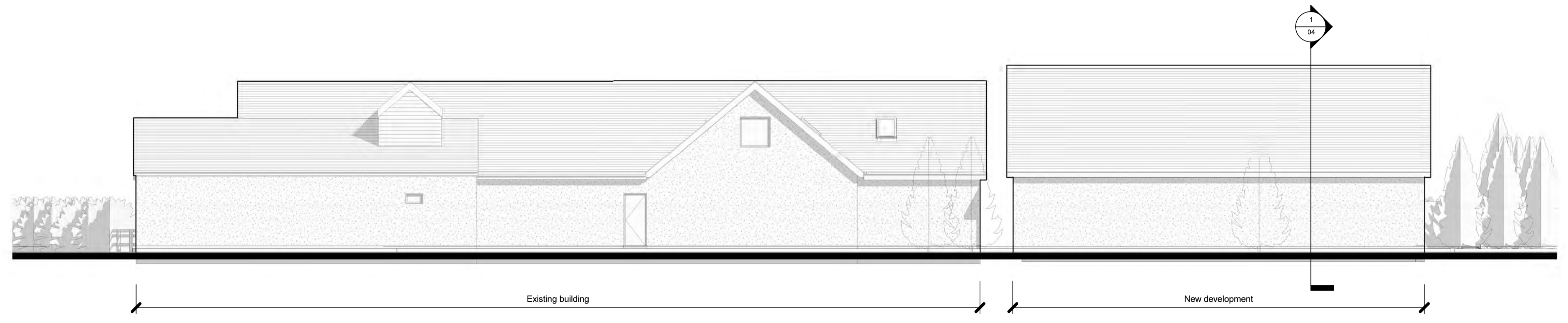
A

NOTES AND AMENDMENTS
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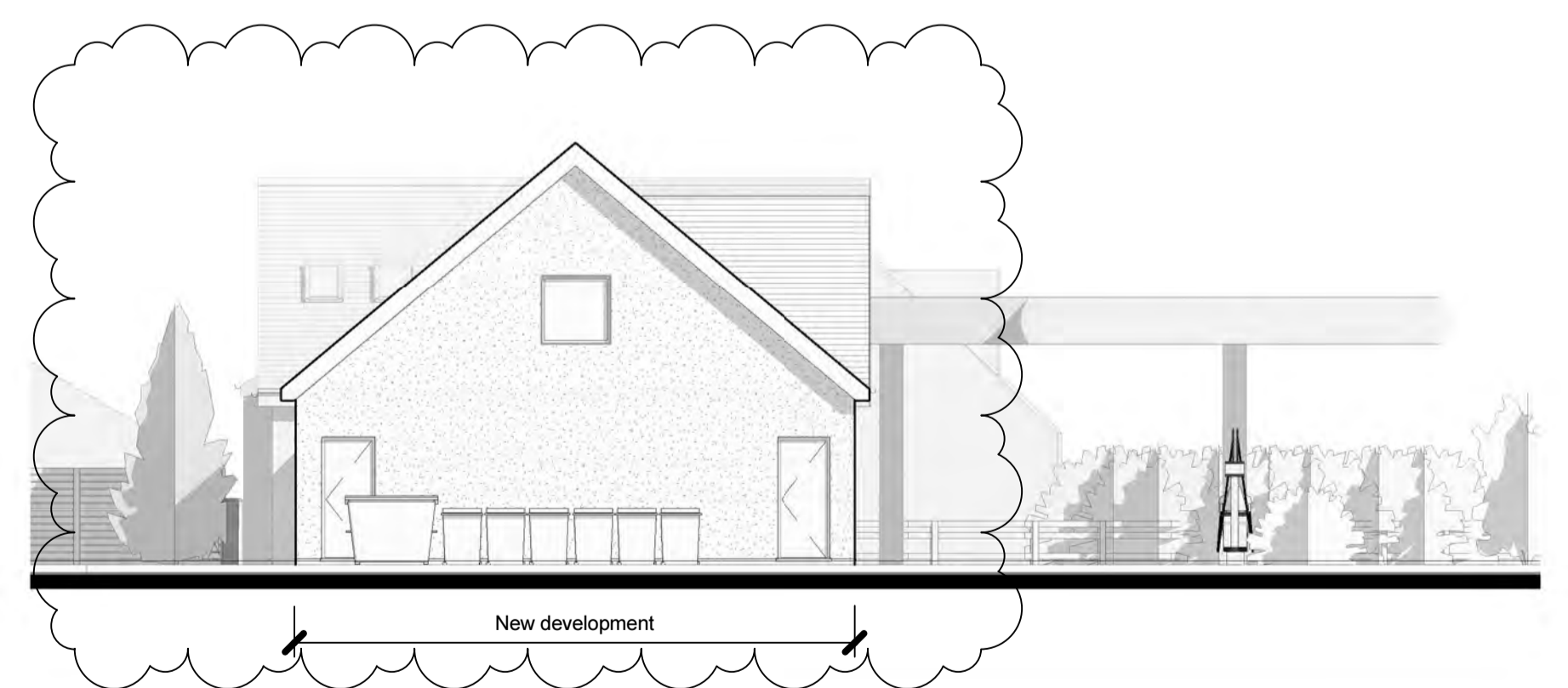
Rev	Description	Drawn	Date
A	New building stair revised and enclosed; external doors added to stair and hazard store; minor layout amendment to first floor; explanation notes added and Workshop room label changed to Laundry.	KM	18/09/15



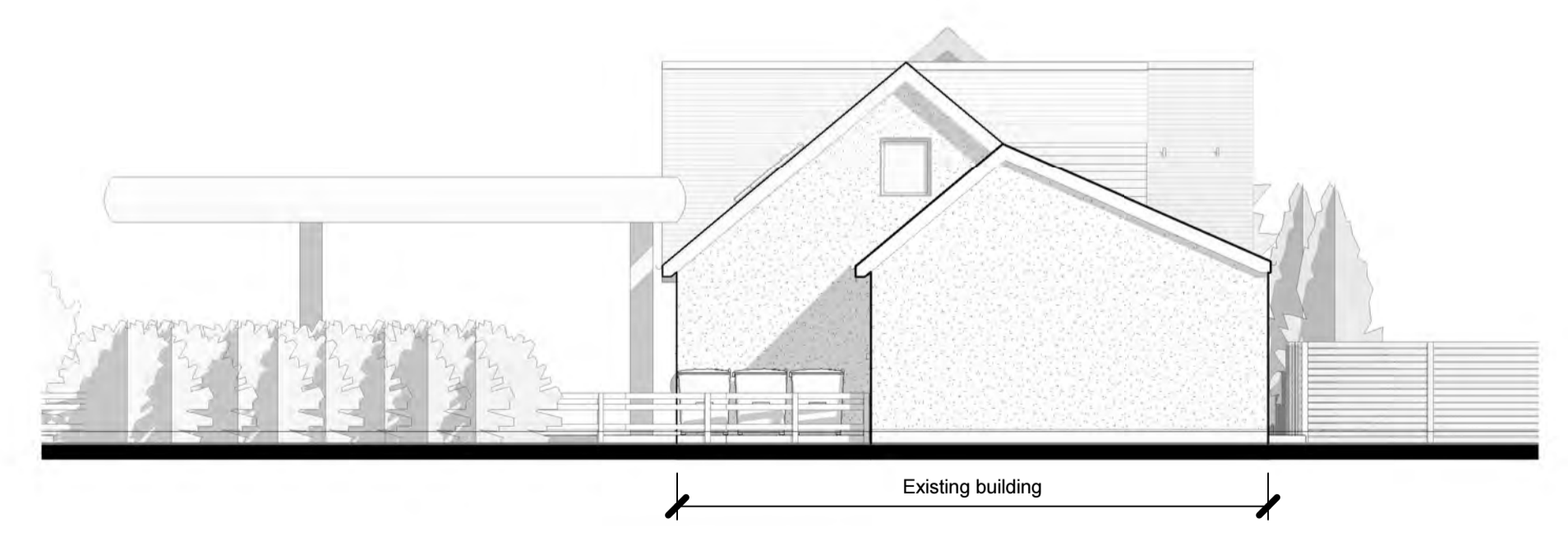
1 **South East Elevation**
1 : 100



2 **North West Elevation**
1 : 100



3 **South West Elevation**
1 : 100



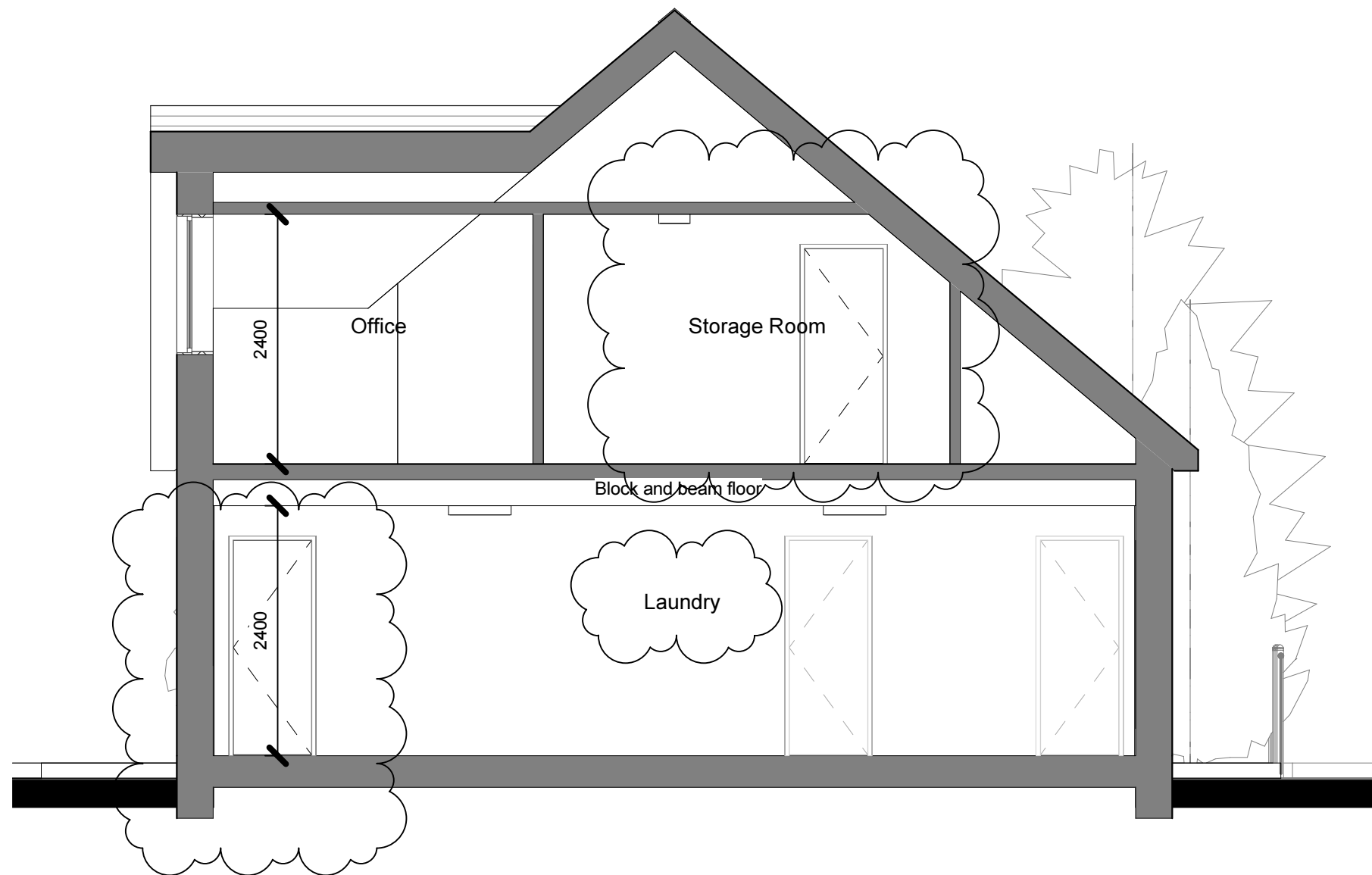
4 **North East Elevation**
1 : 100

	KALM Design Chartered Architect	
	45 Logan Way, Muir of Ord, IV6 7QJ tel : 01463 871280 email : kevin@kalmdesign.co.uk	
Project		
Extension and Alterations Bridgend Filling Station Station Road Beauly IV4 7EH		
Client		
Bridgend Filling Station		
Drawing Title		
Elevations		
Drawing Stage		
Planning		
Drawn By	Sheet Size	Date
KM	A1	27/03/15
Project No.	Drawing No.	Revision
0021	03	A

NOTES AND AMENDMENTS

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Rev	Description	Drawn	Date
A	New building stair revised and enclosed; external doors added to stair and hazard store; minor layout amendment to first floor; explanation notes added and Workshop room label changed to Laundry.	KM	18/09/15



1 Section - New Building
1 : 50

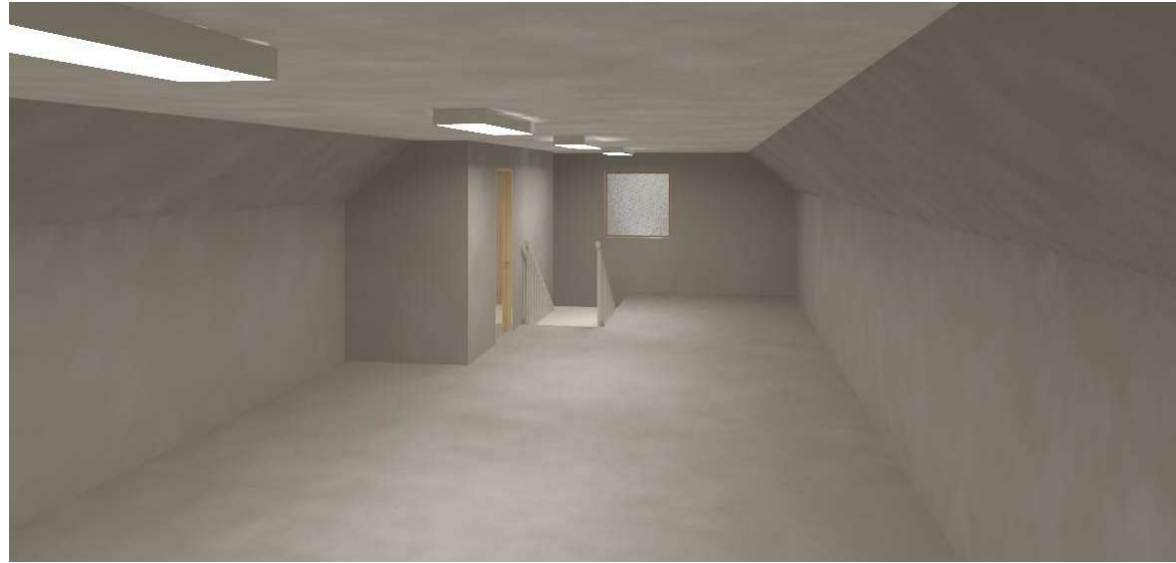
	KALM Design Chartered Architect	
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Extension and Alterations Bridgend Filling Station Station Road Beauly IV4 7EH		
Client		
Bridgend Filling Station		
Drawing Title		
Section		
Drawing Stage		
Planning		
Drawn By	Sheet Size	Date
KM	A3	27/03/15
Project No.	Drawing No.	Revision
0021	04	A

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Rev	Description	Drawn	Date
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① Workshop Floor
1:1



② Storage
1:1



③ New Development
1:1

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Client		
Bridgend Filling Station		
Drawing Title		
Visualisation		
Drawing Stage		
Design		
Drawn By	Sheet Size	Date
KM	A3	29/03/15
Project No.	Drawing No.	Revision
0021	07	