

THE HIGHLAND COUNCIL

NORTH PLANNING APPLICATIONS COMMITTEE
29 November 2016

Agenda Item	6.10
Report No	PLN/071/16

16/02104/FUL : Mr David Gill
Rosskeen Farms, Invergordon, IV18 0PL

Report by Area Planning Manager

SUMMARY

Description : Erection of agricultural anaerobic digester plant

Recommendation - GRANT

Ward : 07 - Cromarty Firth

Development category : Local Development

Pre-determination hearing : Not required

Reason referred to Committee : Community Council objection and more than 5 objections

1. PROPOSED DEVELOPMENT

- 1.1 Detailed planning permission is sought for an agricultural Anaerobic Digester (AD) plant. The proposed plant processes and converts farm feedstock into bio-methane which is then delivered to the gas network which runs nearby. The end product of this process is a nutrient rich fertiliser which is returned to the farmland that produced the original feedstock.
- 1.2 A new access is proposed from the public single track road to the plant running to the north of the existing cluster of development at Auchintoul. The plant will be fed from several local farms within an 8 mile radius of the site. The total amount of feedstock required per year to efficiently run the plant is approximately 36,000 tonnes. This will draw material from all 850 acres of Rosskeen Farm and approximately an equivalent acreage from the satellite feeder farms.
- 1.3 The plant itself consists of three concrete "silage clamps" where silage is stored for gradual feeding into the process. The biological process takes place within a digester tank and the end product digestate is stored in an enclosed tank. The gas produced will amount to approximately 3.9 million cubic metres of methane per annum. The development comprises of:
 - Digester Tank : 32m diameter and 14.4m high;
 - Digestate Storage Tank 1 : 32m diameter and 14.4m high;

- Digestate Storage Tank 2 : 32m diameter and 14.4m high;
- Silage Clamps : 3 pits each 70m x 27m with 4m high walls;
- Ancillary plant; and
- Control room, site office and weighbridge.

1.4 The following information was submitted in support of the application;

- Transport Assessment
- Noise Assessment
- Odour assessment
- Drainage Assessment
- Supporting statement

1.5 **Variations:** Route of access to site from public road and layout of plant amended since application lodged

1.6 There was no pre application enquiry ahead of the submission of the application.

2. SITE DESCRIPTION

2.1 The site is located at Rosskeen Farm approximately 1km north-west of Invergordon. The plant is to be located on a relatively flat unused and heavily gravelled parcel of agricultural land. The site is bounded to the south-east by Rosskeen burn and beyond this a band of mature trees and Invergordon Golf Course.

2.2 To the north of the site is the A9 trunk road and to the south is the B817 stretch of shore road between Alness and Invergordon. Directly to the west of the site is the U1474 single track road connecting the shore road with the A9 at the Rosskeen junction. The site is currently accessed via an unadopted road leading off the U1474 which serves Rosskeen engineering and several houses, the closest of which to the application site is 5 Auchintoul Farm Cottages located approx. 200m west of the edge of the plant.

2.3 The character of the area is predominantly agricultural with a scattering of farm buildings, properties and commercial businesses such as Rosskeen Engineering to the east and MacDonald scaffolding to the south.

3. PLANNING HISTORY

3.1 None.

4. PUBLIC PARTICIPATION

4.1 Advertised : Schedule 3 and Unknown Neighbour on 20.05.2015 and 12.08.2016
Representation deadline : 26.08.2016 and 03.06.2016

Representations received: 24 (23 objecting and 1 in support)

4.2 Material considerations raised in objection are summarised as follows:

- The process and storage of waste awaiting processing will result in odour nuisance to the surrounding area.
- Site poses a significant hazard to the nearby residences

- Access road runs close to residential properties, which will result in unacceptable noise, dust and light pollution and impact upon the amenity of neighbouring properties.
- Road leading from the A9 to shore road is in a poor state of repair and will be exacerbated by heavy traffic.
- The visual impact of the development is not acceptable.
- Junction with the A9 prone to accidents and low bridge and tight bend near shore road a hazard for heavy vehicles.
- Traffic will conflict with funerals taking place at Rosskeen cemetery.
- Impact upon the amenity of the golf course in terms of odour and views.
- Traffic movements will create gridlock on local road network.
- Supporting statement includes contradictory information regarding tonnage of feedstock required for the process.
- The proposal has limited visual impact compared to other forms of renewable energy generation.

In support of the application the following observations were made :

- Scottish government encouraging renewable forms of energy generation.
- Development supports the agricultural industry in the area under threat by challenging market conditions.
- Noise and odour from bio digester is unlikely to be intrusive to local residents. Operational plants provide evidence of this.
- There is already many traffic movements on farms at harvest time. For the majority of the year there will be limited traffic movements to and from the site.

4.3 All letters of representation are available for inspection via the Council's eplanning portal which can be accessed through the internet www.wam.highland.gov.uk/wam. Access to computers can be made available via Planning and Development Service offices.

5. CONSULTATIONS

5.1 **Transport Planning** : Not objecting - Transport Planning originally objected due to insufficient information. Following the submission of a Transport Assessment containing information on vehicle routing, vehicle movement numbers, traffic management, swept paths and details of upgrades required to the public road, Transport Planning have confirmed that they have no objection subject to conditions. The recommended conditions are summarised below:

1. A suspensive condition to agree and construct strengthening of the U1474.
2. A suspensive condition to agree and then construct six large passing places on the U1474.
3. A suspensive condition to construct improvements at the northern junction of the U1474 and the A9 Trunk road.
4. A suspensive condition to construct improvements at the southern junction of the U1474 and the Shore road (B817)
5. A suspensive condition to agree and then implement in perpetuity an Operational Traffic Management Plan.
6. A condition to provide the access including surfacing, drainage and provision and maintenance of the visibility splays from the new access onto the U1474 of 4.5x120m in both directions.

- 5.2 **Environmental Health** : Not objecting – Environmental Health initially objected and requested a noise and odour assessment and a revision to the site layout to reduce the impact upon noise sensitive locations. Following submission of this information which demonstrates that there is no significant impact in terms of noise or odour, Environmental Health have confirmed that they have no objection.
- 5.3 **Historic Environment Team** : Not objecting – The proposed development is located in an area of archaeological potential. Therefore, a condition is requested requiring an evaluation of the site in order to establish the archaeological content and potential.
- 5.4 **Flood Risk Management Team** : Not objecting. Initially objected on grounds of lack of information on flooding and drainage. However, following the submission of further information regarding flood risk and a drainage impact assessment, they have now removed their objection.
- 5.5 **Transport Scotland** : Not objecting – Initially objected on the grounds of lack of information to assess whether or not the impact upon the trunk road would be significant. Following the submission of a Transport Assessment including details of the modifications required at the Rosskeen junction with the A9, Transport Scotland have confirmed that they have no objection subject to conditions ensuring the provision of the modification of the junction and the provision of a Transport Management Plan and a Construction Transport Management Plan.
- 5.6 **SEPA** : Not objecting – Initially objected due to a lack of information regarding flood risk, consentability and drainage. Following the submission of further information SEPA have confirmed that their objection is withdrawn subject to a condition regarding revising the Drainage design and layout to comply with the current SUDS Manual. SEPA are content that this is achievable, however, require the details to be shown within an updated plan/Drainage Impact Assessment.
- 5.7 **Invergordon Community Council** : Objection summarised as follows;
- Site at risk of flooding from Rosskeen Burn.
 - There is potential risk of contamination of Rosskeen Burn.
 - The increase in traffic movements along the access road in both directions will amount to an unacceptable level of noise for the inhabitants of nearby properties. In addition glare from headlights and odour will result in a neighbour nuisance.

- The road system around the proposed development is totally unsuitable for the amount of heavy traffic which would be needed to keep the plant in operation. There is major concern about road safety for the public using these roads because of the proposed huge increase in tractors, trailers, lorries etc.
- If this application was given permission to proceed Invergordon Community Council wish to attach certain conditions;
 - i.) Upgrading of Rosskeen junction with A9
 - ii.) Upgrading of Shore Road/Rosskeen Bridge Junction
 - iii.) Improvements to the minor road from Rosskeen Free Church to the shore Road
 - iv.) Hours of operation to be set to more reasonable periods of time – e.g. 10 am to 8pm and not during the night.

6. DEVELOPMENT PLAN POLICY

The following policies are relevant to the assessment of the application

6.1 Highland Wide Local Development Plan 2012

28	Sustainable Design
61	Landscape
64	Flood Risk
66	Surface Water Drainage
67	Renewable Energy Developments

6.2 Inner Moray Firth Local Development Plan

No policies or allocations relevant to the proposal are included in the Proposed Plan.

6.3 Highland Council Supplementary Planning Policy Guidance

Flood Risk and Drainage Impact Assessment (Jan 2013)
 Highland Historic Environment Strategy (Jan 2013)
 Highland Renewable Energy Strategy and Planning Guidelines (May 2006)
 Managing Waste in New Developments (March 2013)
 Physical Constraints (March 2013)
 Public Art Strategy (March 2013)
 Standards for Archaeological Work (March 2012)
 Sustainable Design Guide (Jan 2013) Trees, Woodlands and Development (Jan 2013)

7. OTHER MATERIAL CONSIDERATIONS

Scottish Government Planning Policy

- 7.1 Scottish Planning Policy (SPP) contains principal policies on Sustainability and Placemaking, and subject policies on A Successful, Sustainable Place; A Low Carbon Place; A Natural, Resilient Place; and A Connected Place. It also highlights that the Development Plan continues to be the starting point of decision making on planning applications. The content of the SPP is a material consideration that carries significant weight, although it is for the decision maker to determine the appropriate weight to be afforded to it in each case.

Other relevant Scottish Government Policy and Guidance

- 7.2
- National Planning Framework for Scotland 3
 - PAN 56 – Planning and Noise
 - PAN 58 – Environmental Impact Assessment
 - PAN 60 – Planning for Natural Heritage
 - 2020 Routemap for Renewable Energy

8. PLANNING APPRAISAL

- 8.1 Section 25 of the Town and Country Planning (Scotland) Act 1997 requires planning applications to be determined in accordance with the development plan unless material considerations indicate otherwise.

- 8.2 This means that the application requires to be assessed against all policies of the Development Plan relevant to the application, all national and local policy guidance and all other material considerations relevant to the application.

8.3 Development Plan

- 8.4 The Development Plan comprises the adopted Highland wide Local Development Plan (HwLDP), the Inner Moray Firth Local Development Plan (IMFLDP) and statutorily adopted supplementary guidance. There are no site specific policies affecting this application site within the IMFLDP. The principal HwLDP policy on which the application needs to be determined is Policy 67 - Renewable Energy. The other HwLDP policies listed at 6.2 of this report are also relevant and the application must be assessed against these.

- 8.5 Policy 67 sets out that renewable energy development should be well related to the source of the primary renewable resource needed for operation, the contribution of the proposed development in meeting renewable energy targets and positive / negative effects on the local and national economy as well as all other relevant policies of the development plan and other relevant guidance. In that context the Council will support proposals where it is satisfied they are located, sited and designed such as they will not be significantly detrimental overall individually or cumulatively with other developments having regard to 11 specified criteria. Such

an approach is consistent with the concept of Sustainable Design (Policy 28) to achieve the right development in the right place; it is not to allow development at any cost. If the Council is satisfied that there will be no significant adverse impact then the application will accord with the Development Plan.

8.6 **National Policy**

8.7 There is strong support for renewable energy development in national policy. The Scottish Government has a target of 50% of Scotland's electricity demand generated from renewable resources by 2015 and 100% of demand by 2020. These targets are not a cap.

8.8 Notwithstanding the overarching context of support, SPP recognises that the need for energy and the need to protect and enhance Scotland's natural and historic environment must be regarded as compatible goals. The planning system has a significant role in securing appropriate protection to the natural and historic environment without unreasonably restricting the potential for renewable energy. National policies highlight potential areas of conflict but also advise that detrimental effects can often be mitigated or effective planning conditions can be used to overcome potential objections to development.

8.9 The Council continues to respond positively to the Government's renewable energy agenda, however the contribution made both nationally and locally from anaerobic digestion is limited. Nationally anaerobic digestion is a growing sector with an installed capacity of 67MW at the end of Quarter 2, 2016. Highland anaerobic digestion energy projects in operation/under construction or approved as of end of Quarter 2, 2016 have a capacity of 2MW.

8.10 Notwithstanding any significant impacts that this proposal may have upon the landscape resource, amenity and heritage of the area, the development could be seen to be compatible with Scottish Government policy and guidance and increase its overall contribution to the Government, UK and European energy targets. Furthermore, the development will have an economic benefit to the area and includes the creation of 5 new jobs

8.11 **Traffic Impacts**

8.12 A key consideration in the determination of this application is the impact upon the local road network and the A9 trunk road.

8.13 There is an existing steady flow of traffic at harvest time delivering crop from field to farm and from farm to farm. This application represents a re-arrangement of this existing traffic with agricultural traffic converging on the AD plant during harvests. A traffic routing plan has been provided which has been designed to avoid right turning onto the A9 from the Rosskeen junction and right turning into the Rosskeen junction from the A9. The routing plan also avoids the use of the Tomich junction in either travelling to or from the site. Traffic flows will be from one farm at a time, with vehicles arriving and leaving in a one way system. This plan shows that it is feasible to operate traffic to and from the plant without resulting in significant road

safety issues. The routing plan is contained at the end of the report. The routes proposed are to be enforced through the provision of an Operational Traffic Management Plan, which can be secured by condition.

- 8.14 Experience from other digester plants such as Wester Kerrowgair Plant (Dalcross, Inverness), suggests that 14 tonne average loads can be expected by tractor and trailer, and evidence of this has been provided. The applicant has stated that the harvesting machine has an output of 60 tonnes per hour. This equates to an hourly maximum of 4 to 5 loads giving a daily total of 64 vehicles. The figure of 4-5 loads per hour is a maximum figure or “worst case scenario” based on harvest operations running as smoothly as they possibly could. This level of maximum intensity will exist for approximately 6 weeks during the peak harvest period, which will vary from year to year due to weather. The outgoing digestate vehicle movements from the plant will be spread over the full year with an average volume of around 1.3 tractor and trailer movement and 0.7 HGV movements per day.
- 8.15 Initially Transport Scotland had objected when the application was lodged due to the lack of information, however this was withdrawn following submission of further information contained within the Transport Assessment and clarification of points raised. Modification of the Rosskeen A9 junction is required, specifically the road is required to be widened by 1.2 to 2.5m over a stretch of approx. 82 m, and this shall be secured via a condition attaching to the consent.
- 8.16 Transport Planning had also objected to the application originally due to the lack of information. They too following submission of further information have confirmed that they have no objection to the development in terms of impact on the local road network both in terms of road safety or the structural integrity of the road. The U1474 is the public road connecting the A9 Rosskeen junction with the Invergordon shore road (B817), and a new access and road is to be created off the U1474 into the proposed site for the digester plant. This new stretch of access roads runs north of the existing access into the site before re-joining the existing track. This removes any conflict between existing residential and business traffic using the existing access into the site and the traffic generated from the proposed development. The new access proposed meets with standard specifications in terms of visibility, surfacing and gradients.
- 8.17 Swept path analysis at the U1474/B817 has been provided and shows that minor improvements to this junction with the Shore Road are required, specifically, road widening of 1m over 15m at the Coastal junction, and this improvement shall be a condition of the consent.
- 8.18 The swept path analysis at the horizontal bend around the old church grounds at the south end of the U1474 approaching Shore Road confirms that it is possible for a tractor and trailer to negotiate the bend safely. Although visibility is restricted around the bend it is not judged to be a significant road safety concern due to the very low speeds at this location. However, it is not possible for heavy vehicles to pass at this location and traffic management and detailed routing for supplier farms is required which avoids two way traffic at this section of the road.

8.19 The Council's structures section confirms that they have no concerns regarding the proposal and the impact upon the minor bridge over the Achnagarron Burn.

8.20 Improvements to the public road at Rosskeen are required and can be secured by condition. These include:

- construction of new and upgrading of existing passing places;
- edge strengthening;
- patching where isolated areas show weakness; and
- a wearing course overlay across the entire length of the road to be utilised.

8.21 Although, the level of traffic is intensive over the harvest period (anticipated 4-5 loads per hour), subject to the improvements sought to the local and trunk road network the impacts of the development can be mitigated and are considered acceptable.

8.22 **Residential Amenity**

8.23 A prominent point of concern within the representations received is the impact of the development upon the amenity of neighbours, particularly in terms of the potential noise and odour. The impact of the development upon the amenity of neighbours is a key issue in the determination of this planning application.

8.24 The applicant submitted a detailed odour assessment which concludes that odour from the development will be negligible at any neighbouring residential property. The applicant has confirmed that no manure, slurry or food waste will be used as a feedstock and a condition prohibiting the use of the aforementioned shall be attached to any permission which may be granted. Other operational AD plants have shown that only a very minimal odour is noticeable when standing adjacent to the silage storage clamps.

8.25 A noise impact assessment has been carried out and reviewed by Environmental Health. The main noise sources have been identified as the mixer motors, the CHP plant and vehicle movements. The layout of the site was revised in order to relocate the main noise sources at the eastern half of the site, as distant from residential neighbours as possible (original site layout provided at the end of this report). The preferred noise criteria for daytime noise is for external Rating levels to be no more than 5dB above the background. The assessment demonstrates that the predicted levels can meet this criteria. The criteria for night time noise is compliance with either an external limit of no more than 5dB above background or an internal limit of Noise Rating Curve 20. The assessment demonstrates that due to the very low night time background levels the external criteria may not be met however, predicted levels will meet the NR 20 criteria. Environmental Health have confirmed that they are satisfied that the assessment has demonstrated that noise levels can meet the necessary criteria and recommend a condition controlling noise limits from the development.

8.26 The impact upon the surrounding residential properties in terms of noise from the vehicular movements will be greatest during the harvest period where deliveries to the site will be at a rate of between 4 and 5 per hour. Initially the route of access to the plan ran parallel with the existing access in front of Achnitoul Cottages. There

was concern about the impact of traffic passing along this route and modifications were sought to the route. A revised plan was submitted relocating the access and access road 60m to the north to run to the rear of these properties. It is acknowledged during the harvest period the level of traffic movement will be high and will have an impact on the amenity of residents for a limited period but the re-routing as detailed should mitigate against this impact. Outwith peak harvest time the impact surrounding residential properties will be negligible. The access track is to be hard surfaced, which will remove the risk of vehicles generating excessive levels of dust. Overall given the mitigation measures regarding noise and the fact that peak traffic movements are restricted to the harvest period, it is not considered that the development would lead to a significant adverse impact upon the amenity of surrounding properties which could justify refusal of the planning permission.

8.27 **Visual Impact and Design**

8.28 The surrounding area is agricultural in character, with scattered farm buildings, commercial uses and residential properties. A mature belt of trees screens the site from the golf course to the south-east. Views to the site from the wider area are limited by existing natural and built features in the landscape. Given the scale of the plant and its position in the landscape it is anticipated that it will be visible from a relatively small proportion of the area. With that said, the areas where it will be visible have concentrations of sensitive receptors including residents and recreational users of the outdoors.

8.29 The plant has an agricultural appearance, which is not at odds with the character of the area where large agricultural and commercial buildings can be found within and adjacent to agricultural fields. The tanks are finished in green to allow for assimilation into the landscape.

8.30 To aid assimilation of the plant into the landscape, further landscaping is to be provided around the perimeter of the site and details of this shall be secured by condition.

8.33 Given the above mitigation and the design and location of the development, it is not considered that there will be a significant visual impact.

8.5 **Other Considerations – not material**

Many of the objections raise the issue of impact upon property value. This is not a material planning consideration.

9. **CONCLUSION**

9.1 The proposed development raises significant concerns, in particular, three key issues: the impact upon the public road network, the impact upon the amenity of neighbours and the visual impact of the development. A substantial body of representation has been received which expresses the aforementioned concerns. These matters were addressed during the determination of the application following

the submission of further information. Measures to mitigate against the impacts have been identified and incorporated into the development post submission. On this basis it is considered that the proposal can be supported subject to conditions.

- 9.2 All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

10. RECOMMENDATION

Action required before decision issued N

Subject to the above, it is recommended the application be **Granted** subject to the following conditions :

1. No development shall commence until details of the colours and finishes for all buildings, fixed plant, and machinery have been submitted to, and approved in writing by, the Planning Authority. Thereafter, the development shall be implemented in accordance with the agreed details.

Reason : In interests of visual amenity and to ensure that the development effectively assimilates into the landscape.

2. No development shall commence until details of the type, dimensions and colour of all boundary treatments shall be submitted to, and approved in writing by, the Planning Authority. Thereafter, the development shall be implemented in accordance with the agreed details.

Reason : In the interest of visual amenity.

3. No development shall commence until details of works to strengthen the U1474 have been submitted to, and approved in writing by, the Planning Authority, in consultation with the Roads Authority. Road Strengthening works shall include edge strengthening, patching where isolated areas show weakness and a wearing course overlay throughout, and any other works deemed necessary by the Planning Authority in consultation with the Roads Authority. Thereafter, the development shall be implemented in accordance with the agreed details.

Reason : In the interests of amenity and road safety.

4. No development shall commence until details of the location and specification of six passing places to be constructed on the U1474, have been submitted to the Planning Authority for approval in writing. Thereafter, the development shall be implemented in accordance with the agreed details.

Reason : In the interests of amenity and road safety.

5. Prior to the 1st use of the development hereby approved the improvements to the junction of the U1474 and the A9 Trunk road as shown on drawing number P431-12B, contained within the approved Transport Assessment reference P431-C1-16-02C, shall be fully constructed and complete. Prior to the commencement of development, exact details of the junction improvements shall be submitted to the Planning Authority for approval in writing. The development shall thereafter proceed in accordance with the approved details.

Reason : In the interests of road safety and amenity.

6. Prior to the 1st use of the development hereby approved the improvements to the junction of the U1474 and the shore road (B817) as shown on drawing number P431-16, contained within the approved Transport Assessment reference P431-C1-16-02C, shall be fully constructed and complete. Prior to the commencement of development, exact details of the junction improvements shall be submitted to the Planning Authority for approval in writing. The development shall thereafter proceed in accordance with the approved details.

Reason : In the interests of road safety and amenity.

7. No development shall commence until an Operational Traffic Management Plan has been submitted to the Planning Authority for approval in writing. Thereafter, the plant shall be operated in accordance with the agreed plan, or any other such amended Operational Traffic Management Plan as may be agreed in writing by the Planning Authority. In particular this Traffic Management Plan shall identify the routes to be followed by individual farms or locational groups of farms and include certain restrictions on movements as follows:

- 1) No traffic to enter, leave or cross the A9 at Tomich junction
- 2) Only Rosskeen Farm traffic to cross the A9 at Rosskeen (U1474) junction
- 3) No right turns into Rosskeen Road (U1474) from the A9
- 4) No right turns from Rosskeen Road (U1474) to the A9
- 5) No routes to cross the A9
- 6) All other routes should have no right turns on or off the A9 except from farm or field accesses
- 7) No two way traffic on U1474 south of the proposed access

The Traffic Management Plan shall be updated yearly to reflect the farms providing crops to the site and be made available for inspection by the Planning Authority or Transport Scotland.

The appropriate routes for each day within the Traffic Management Plan will be examined and signed on a daily basis by drivers to ensure familiarity with the routes.

Reason : In the interests of road safety and amenity.

8. No development shall commence until a Construction Traffic Management Plan has been submitted to the Planning Authority for approval in writing. Thereafter, the plant shall be operated in accordance with the agreed plan.

Reason : In the interests of road safety and amenity.

9. Prior to the first use of the development hereby approved, the new access with the U1474 shall be fully completed and surfaced in bituminous macadam or a similar hard, cohesive material, details of which shall first be submitted to, and approved in writing by, the Planning Authority. Only the approved details shall be employed and thereafter the access shall be maintained to the agreed standard.

Reason : In the interests of road safety and amenity.

10. Visibility splays of 2.4m x 120m (the X dimension and Y dimension respectively) shall be provided in each direction at the junction of the new access with the U1474 public road, and maintained in perpetuity. No surface water from the site shall shed onto the public road and the applicant shall be responsible for the provision and maintenance of any measures necessary to prevent surface water from the public road entering the site.

Reason : In the interests of road safety and amenity.

11. Deliveries to and from the site shall only take place between the hours of 7am and 10pm.

Reason : In the interests of neighbour amenity.

12. All plant, machinery and equipment associated with this development shall be so installed, maintained and operated such that either of the following standards are met: -

- any associated operating noise must not exceed NR 20 when measured or calculated within the bedroom of any noise-sensitive premises with windows open for ventilation purposes.

OR

- the operating noise Rating level must not exceed the Background noise level by more than 5dB(A) including any characteristics penalty. Terms and measurements to be in accordance with BS 4142: 2014 Methods for Rating Industrial and Commercial Sound. Time periods to be taken as daytime 1 hour and night time 15 minutes. Daytime period to be taken as 7am to 11pm.

(For the purposes of this condition, “noise-sensitive premises” includes, but is not necessarily limited to, any building, structure or other development the lawful use of which a) falls within Classes 7 (Hotels and Hostels), 8 (Residential Institutions) or 9 (Houses) of the Town and Country Planning (Use Classes) (Scotland) Order 1997 (as amended), or b) is as a flat or static residential caravan.) Plant, machinery and equipment includes vehicle movements.

Reason : In the interests of neighbour amenity.

13. At no time shall manure, slurry or food waste be stored at the development hereby approved as shown on approved drawing reference G009 A3 REV3.

Reason : In the interests of neighbour amenity and to ensure unacceptable odour does not result from this development.

14. No development shall commence until details of a scheme of soft landscaping works have been submitted to, and approved in writing by, the Planning Authority. Details of the scheme shall include:

- i. A plan showing existing landscaping features and vegetation to be retained;
- ii. All soft landscaping and planting works, including plans and schedules showing the location, species and size of each individual tree and/or shrub and planting densities; and
- iii. A programme for preparation, completion and subsequent on-going maintenance and protection of all landscaping works.

Landscaping works shall be carried out in accordance with the approved scheme. All planting, seeding or turfing as may be comprised in the approved details shall be carried out in the first planting and seeding seasons following the commencement of development, unless otherwise stated in the approved scheme.

Any trees or plants which within a period of five years from the completion of the development die, for whatever reason are removed or damaged shall be replaced in the next planting season with others of the same size and species.

Reason : In order to assimilate the development into its setting; in the interests of visual amenity.

15. No development or work (including site clearance) shall commence until a programme of work for the evaluation, preservation and recording of any archaeological and historic features affected by the proposed development/work, including a timetable for investigation, all in accordance with the attached specification, has been submitted to, and approved in writing by, the Planning Authority. The approved programme shall be implemented in accordance with the agreed timetable for investigation.

Reason : In order to protect the archaeological and historic interest of the site.

16. No development shall commence until the Drainage Impact Assessment is updated and submitted to the Planning Authority for approval in writing. The updated Drainage Impact Assessment must demonstrate that the SUDS proposals meet the treatment requirements outlined in the revised SUDS Manual (CIRIA SUDS Manual C753). Specifically the following shall be provided; 1) the filter drain around the tanks is replaced by a lined swale, which will provide additional treatment 2) each of the SUDS features is designed in line with the requirements of the SUDS Manual and 3) the soakaway is located at least 10m from the top of the banks of the watercourse. Thereafter, only the approved details shall be implemented and all surface water drainage provision shall be completed prior to the first occupation of any of the development.

Reason : To ensure that surface water drainage is provided timeously and complies with the principles of SUDS; in order to protect the water environment

17. A 6m buffer strip between the bank of the burn and the site is required to be kept free from development.

Reason : To ensure an acceptable buffer is maintained for watercourse maintenance and to safeguard the site against flood risk.

18. Prior to the first use of the development hereby approved a scheme to ensure that all vehicles leaving the site have been sufficiently cleaned to ensure that mud, dirt and any other material is not transferred onto the public road network, shall be submitted to the Planning Authority for approval in writing. Thereafter the development shall be implemented in accordance with the approved details.

Reason : In the interests of amenity and road safety.

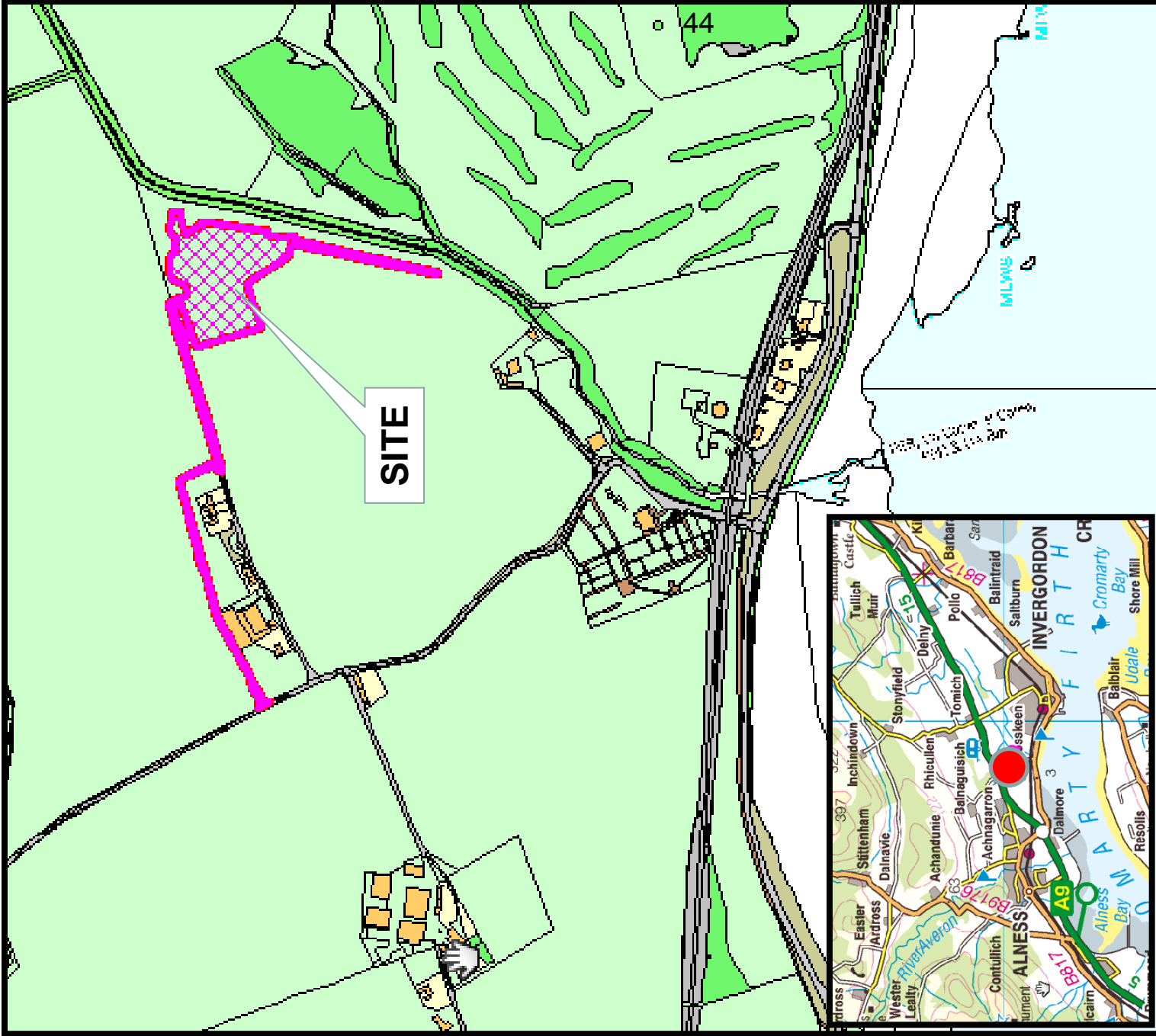
19. Prior to the first use of the development hereby approved full details of any external lighting to be used within the site and/or along its boundaries and/or access shall be submitted to the Planning Authority for approval in writing. Such details shall include the location, type, angle of direction and wattage of each light which shall be positioned and angled to prevent and direct illumination, glare or light spillage outwith the site boundary. Thereafter the development shall be approved in accordance with the approved details.

Reason : In order to ensure that any lighting installed within the application site does not spill beyond the intended target area and does not impact adversely upon the amenity of adjacent residential properties.

20. Should the anaerobic digester plant hereby approved not supply gas for a continuous period of 12 months, it shall be deemed to have ceased to be required and, unless otherwise agreed in writing by the Planning Authority, shall be removed from the site, along with all associated plant and equipment.

Reason : To ensure that any development which has ceased to serve its intended purpose is removed from the site, in the interests of visual amenity.

Signature: Dafydd Jones
Designation: Area Planning Manager - North
Author: Rebecca Hindson
Background Papers: Documents referred to in report and in case file.
Relevant Plans: Plan 1 – Location Plan
Plan 2 – Location Plan
Plan 3 – Site layout Plan
Plan 4 – Access Layout Plan
Plan 5 – Elevations
Plan 6 – Vehicle Movements Plan
Plan 7 – Vehicle Routing Table
Plan 8 – Original Site Layout Plan

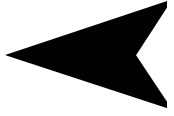


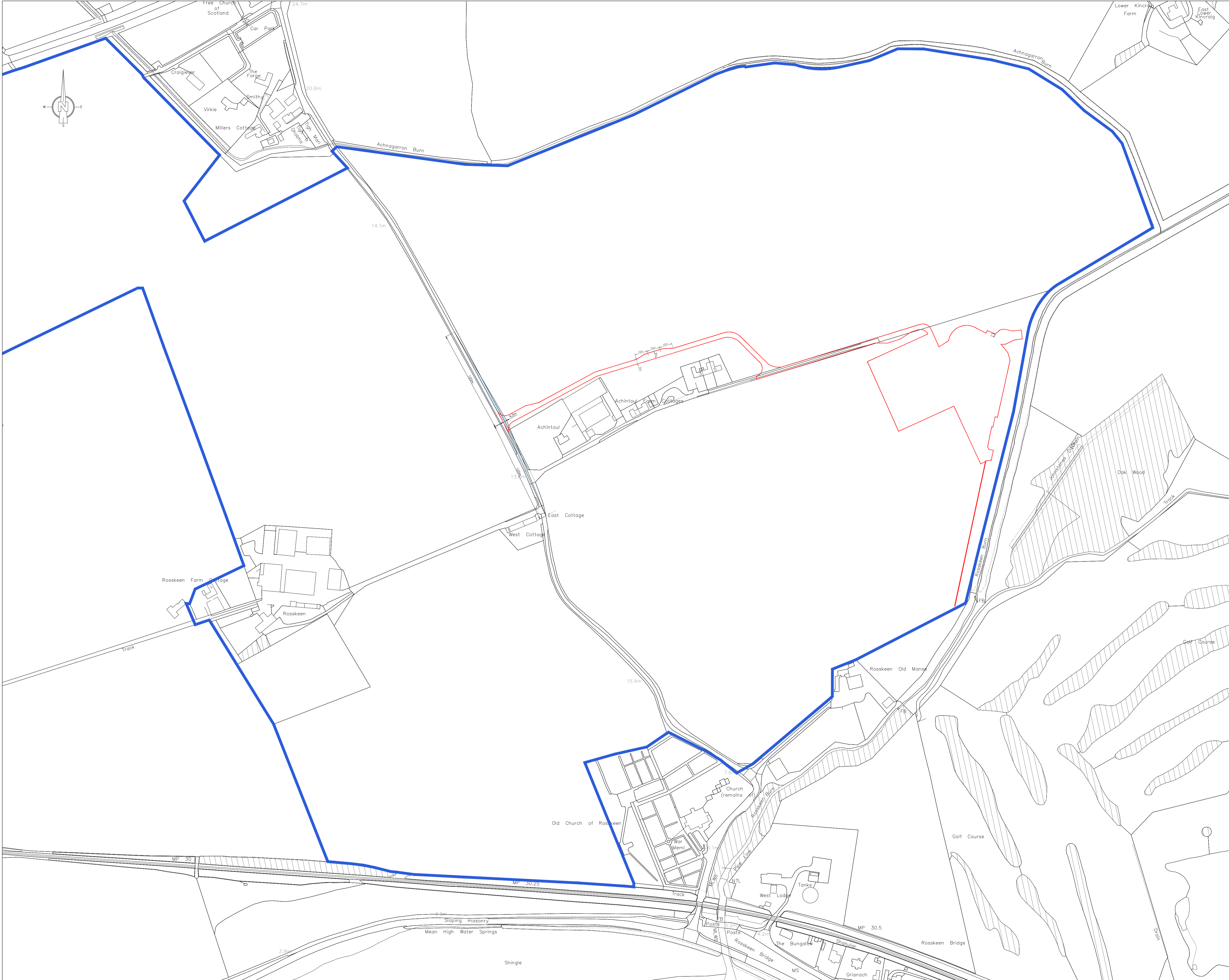
Planning & Development
Service


16/02104/FUL


Erection of agricultural anaerobic digester plant
at Rosskeen Farms, Invergordon

N





 Planning Boundary 1.9ha

 Ownership Boundary

Rev	Revision Note	Revision Date	Drawn by	Checked by
R3	Access updated	18.11.16	CR	DM
R2	For Planning	27.07.16	CR	DM
R1	For Planning	06.05.16	CR	DM

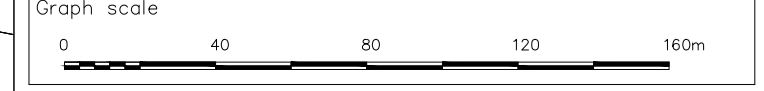
Project
 ROSSKEEN FARM
 INVERGORDON
 IV18 0PL

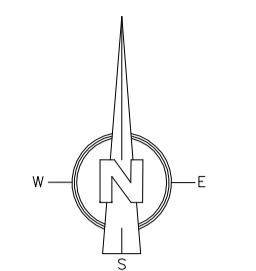
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 G009A1 Location Plan



Drawing status
 For Planning

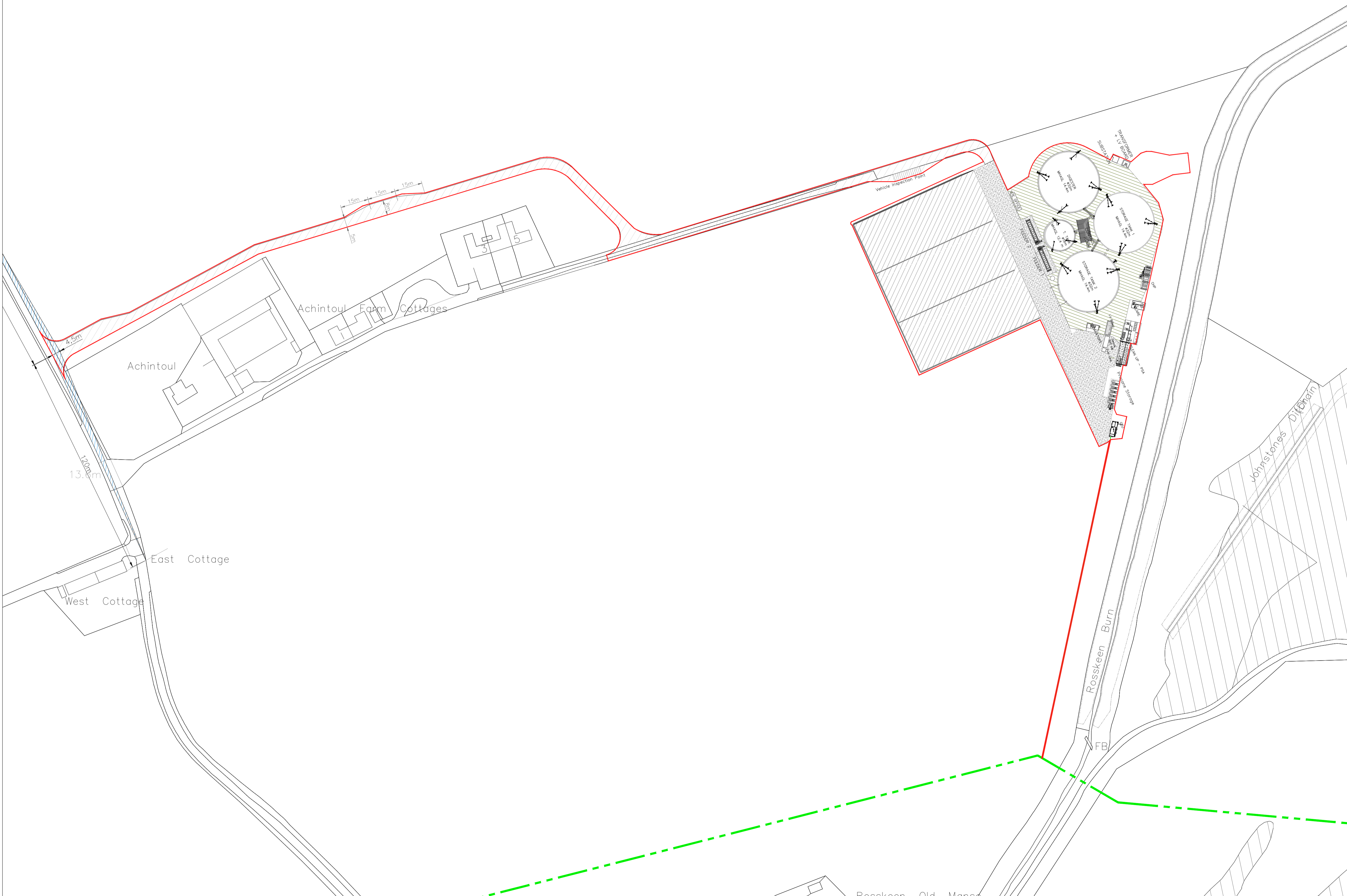
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Date	May 2016	Scale	1:2000





1:1000

- Planning Boundary 1.9ha
- Proposed gas line
- - - IP Mains
- Asphalt
- Agricultural mix
- Concrete
- Grade C30
- Stone track
- 6F2 + type 1
- Access Track



Rev	Revision Note	Revision Date	Drawn by	Checked by
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R2	For Planning	28.07.16	CR	DM
R1	For Planning	06.05.16	CR	DM

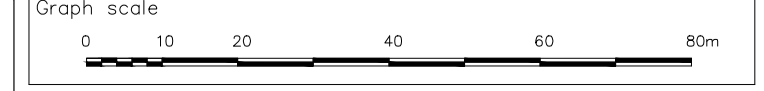
Project
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 INVERGORDON
 IV18 0PL

Drawing
 G009A3 Site Layout Plan



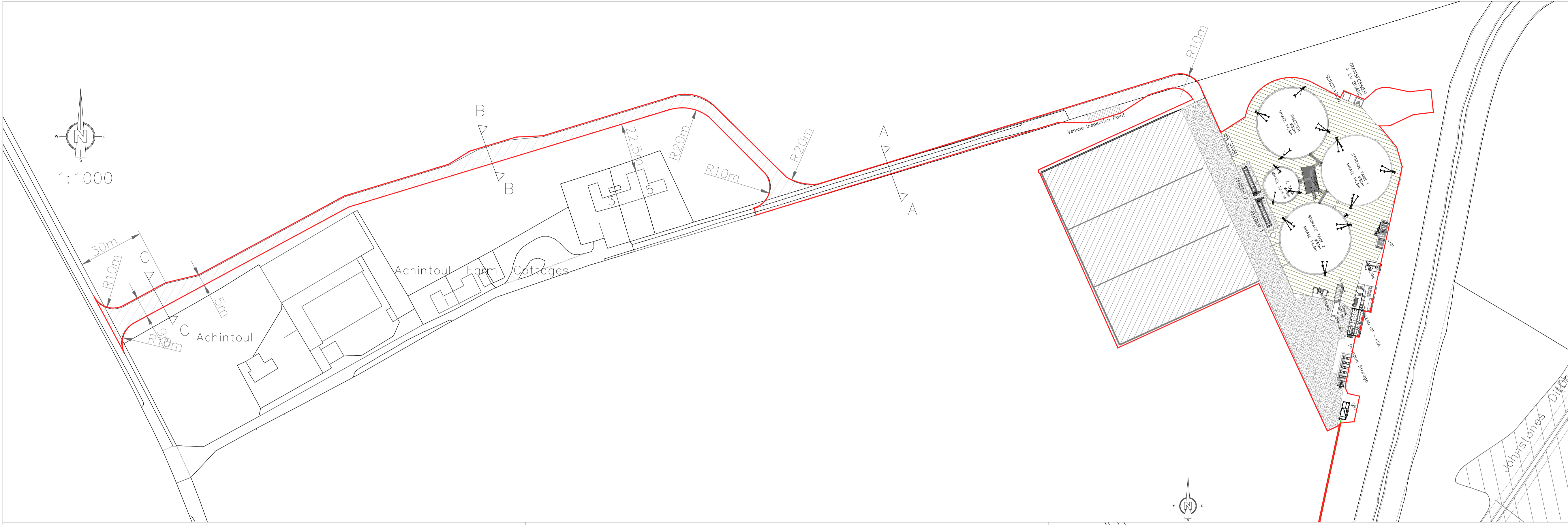
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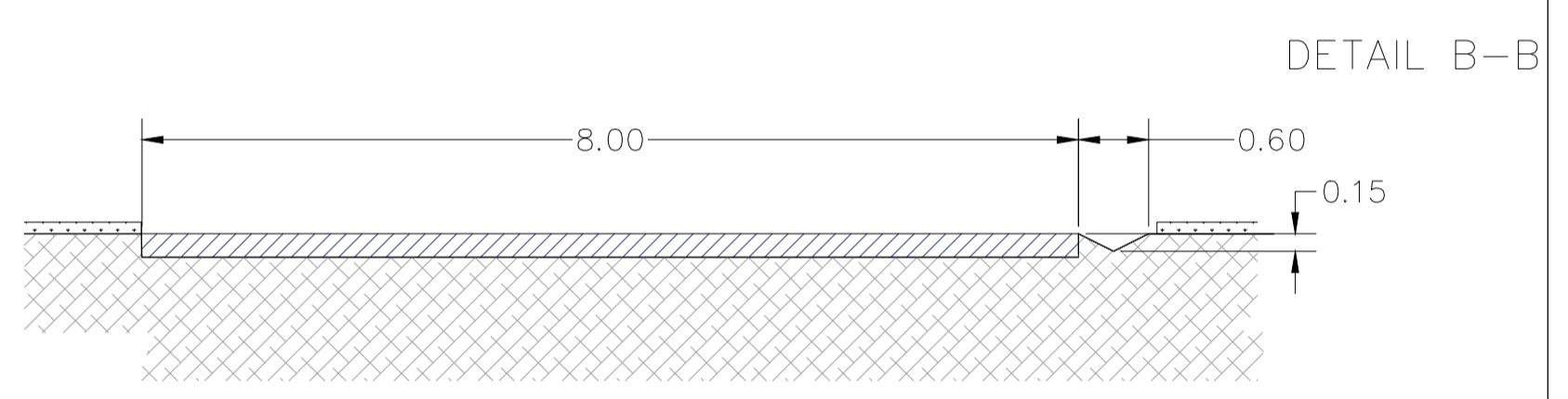
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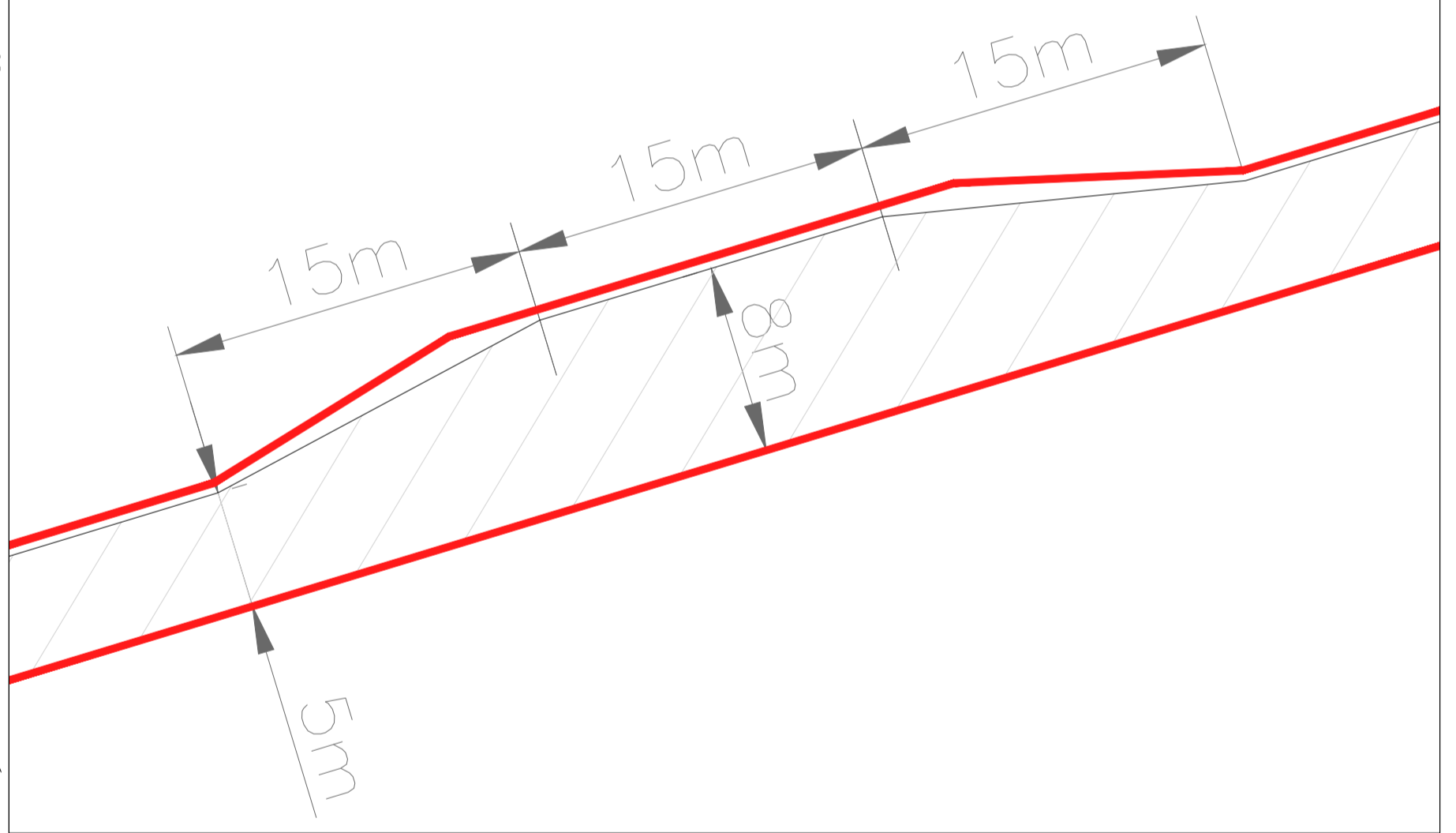
- Planning Boundary 1.9ha
- Asphalt
- Agricultural mix
- Concrete Grade C30
- Stone track 6F2 + type 1
- Access Track

SECTION DETAILS
s 1:60

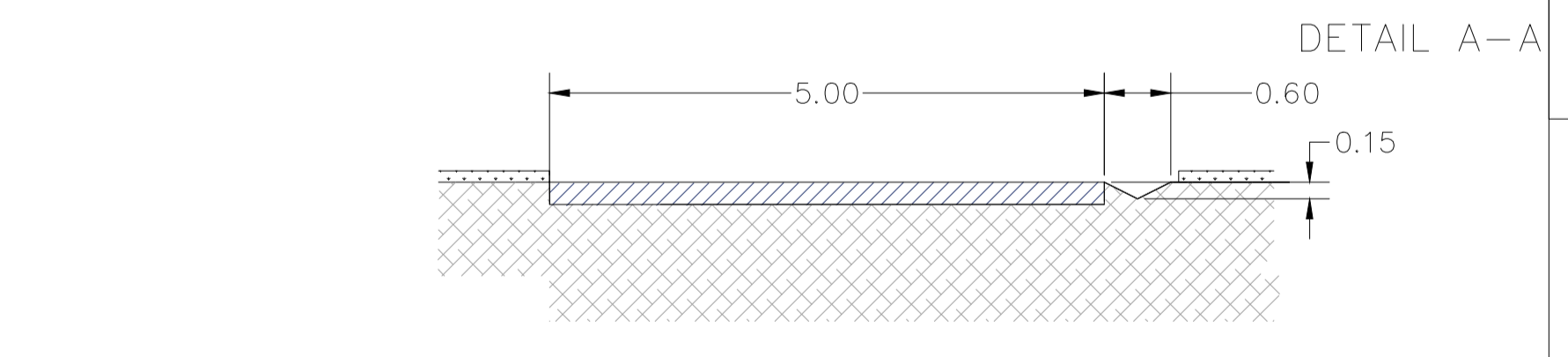


DETAIL B-B

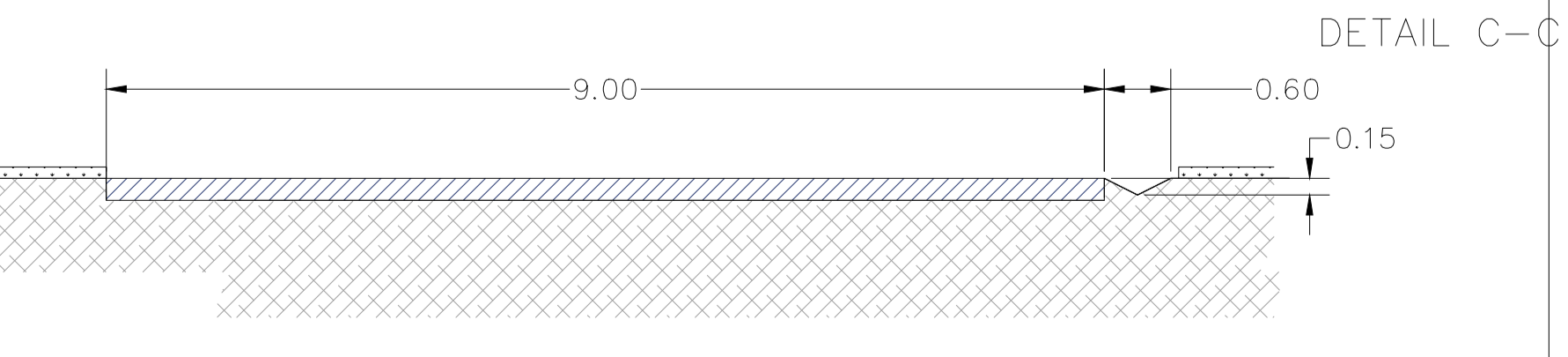
PASSING BAY
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VISIBILITY SPLAY
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DETAIL A-A



DETAIL C-C

R2	Access updated	18.11.16	CR	DM
R1	For Planning	28.07.16	CR	DM
Rev	Revision Note	Revision Date	Drawn by	Checked by

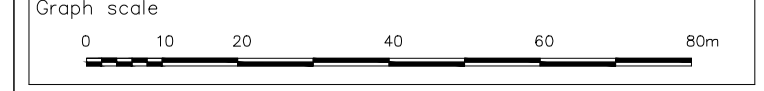
Project
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IV18 0PL

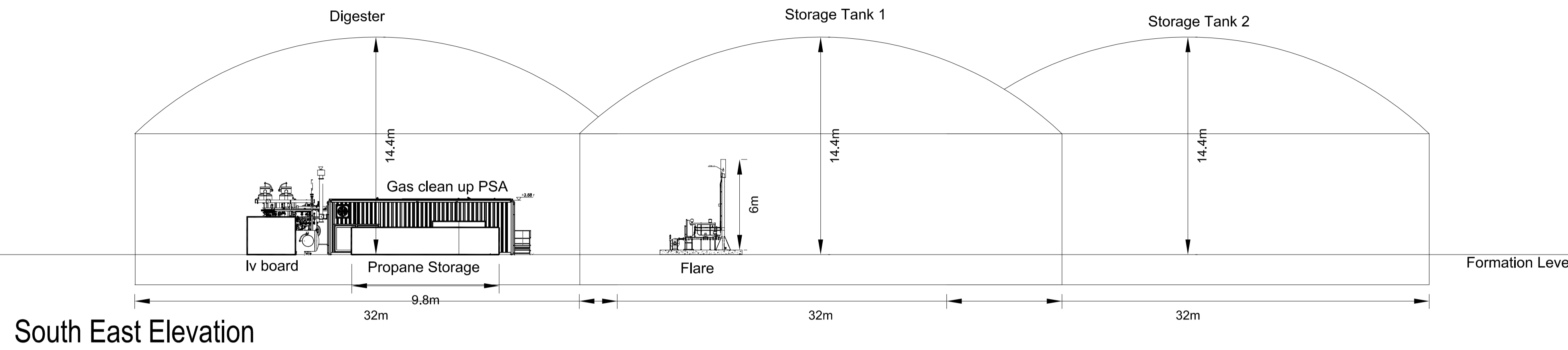
Drawing
G009A2 Site Access



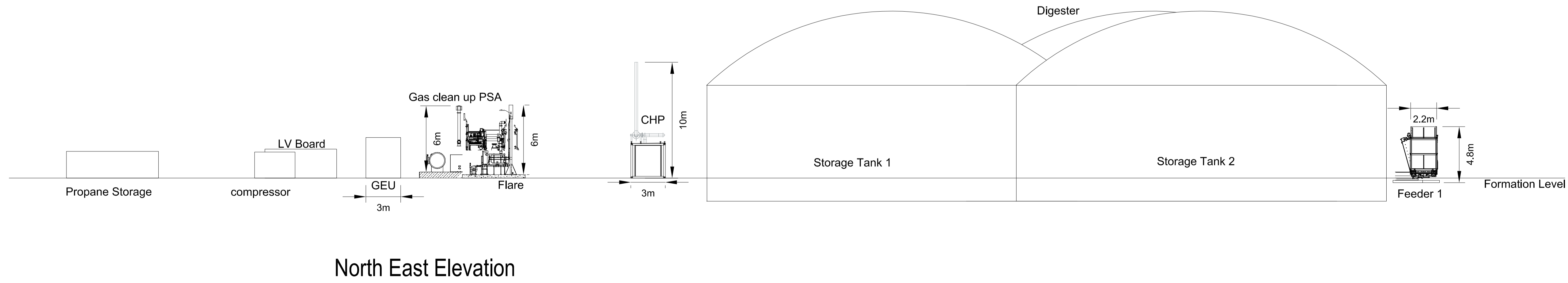
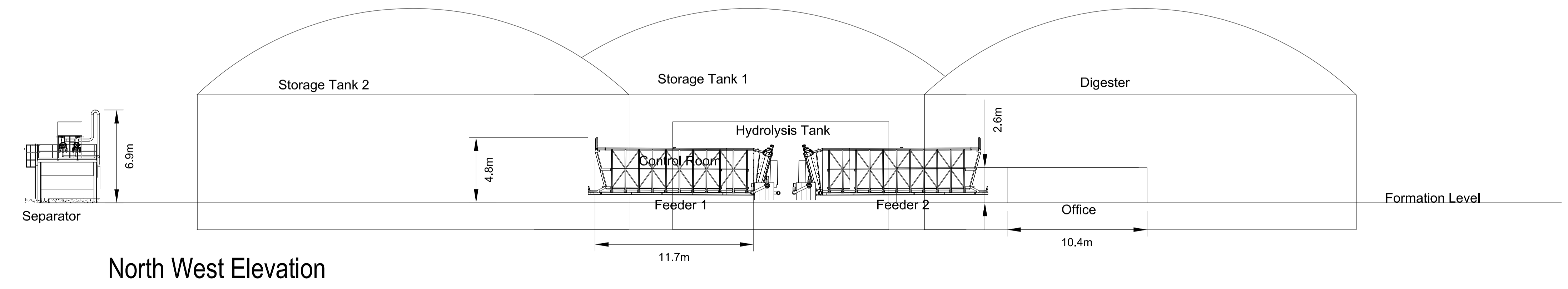
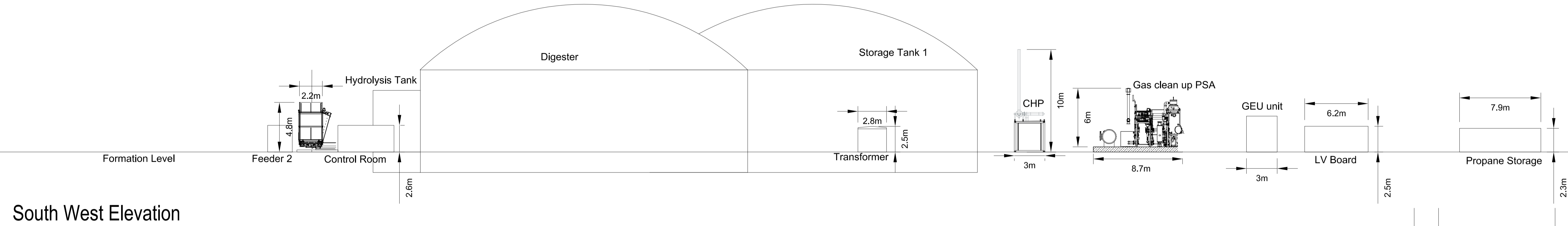
Drawing status
For Planning

R1	R2	Checked by
	CR	DM
Date	July 2016	Scale @ A1
		1:1000





ITEM	MATERIAL	COLOUR
Substation	Glass reinforced plastic (GRP)	12B29 Juniper Green
Transformer	Glass reinforced plastic (GRP)	12B29 Juniper Green
CHP	Galvanised Steel parts	RAL 6005 Moss Green
Flare	Galvanised Steel	
Control Room	Steel	Moss Green
Office	Steel	Moss Green
Propane Storage	Steel	Moss Green
Gas Clean up PSA	Steel	Moss Green
GEU	Concrete panel building	
Separator	Galvanised Steel and concrete frame	
Feeder	Galvanised Steel	Red
Clamps	Concrete walls	
Digester	reinforced concrete, clad with corrugated aluminium sheeting	12B29 Juniper Green
Storage Tank 1	reinforced concrete, clad with corrugated aluminium sheeting	12B29 Juniper Green
Storage Tank 2	reinforced concrete, clad with corrugated aluminium sheeting	12B29 Juniper Green
Hydrolysis Tank	reinforced concrete, clad with corrugated aluminium sheeting	12B29 Juniper Green
Digester, Storage tank 1 & 2 dome	plastic membrane	RAL 6005 Moss Green

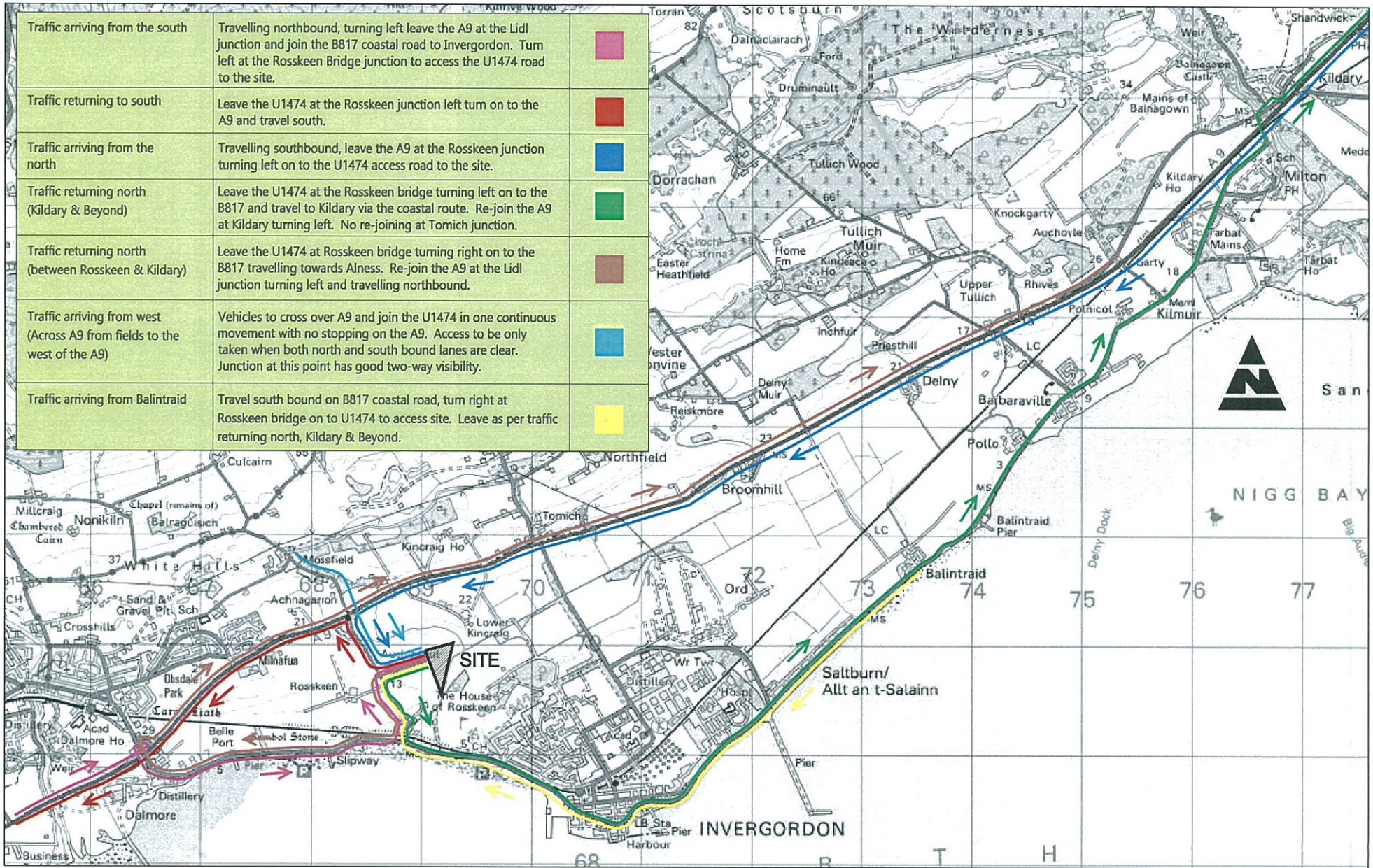


Preliminary		06.05.2016	CR	DM
No.	Revision Note	Revision Date	Drawn by	Checked by
ROSSKEEN FARM INVERGORDON IV18 OPL				
Drawing G009 A7: Elevations Plan				
Client Qila Energy				
Drawing status Issued for planning				
R1				
Drawn by	CR	Checked by	DM	
Date		Scale @ A1		
Graph scale 0 2 4 8 12 16m				

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ROSSKEEN BIODIGESTER PLANT



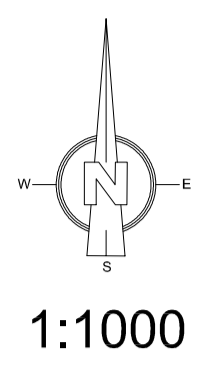
Vehicle Routing & Traffic Movement Methodology

Vehicles will come from one farm at a time due to use of a single harvester for the local farming community. The vehicle routing will be as per the following methodology. No plant traffic will be permitted to use the Tomich Junction in either travelling to or from the site. There will be no right turning on to the A9 from the Rosskeen junction. Traffic arriving from the west may only cross the A9 when both north and south bound lanes are clear and the movement can be undertaken in one continuous flow without stopping on the trunk road.

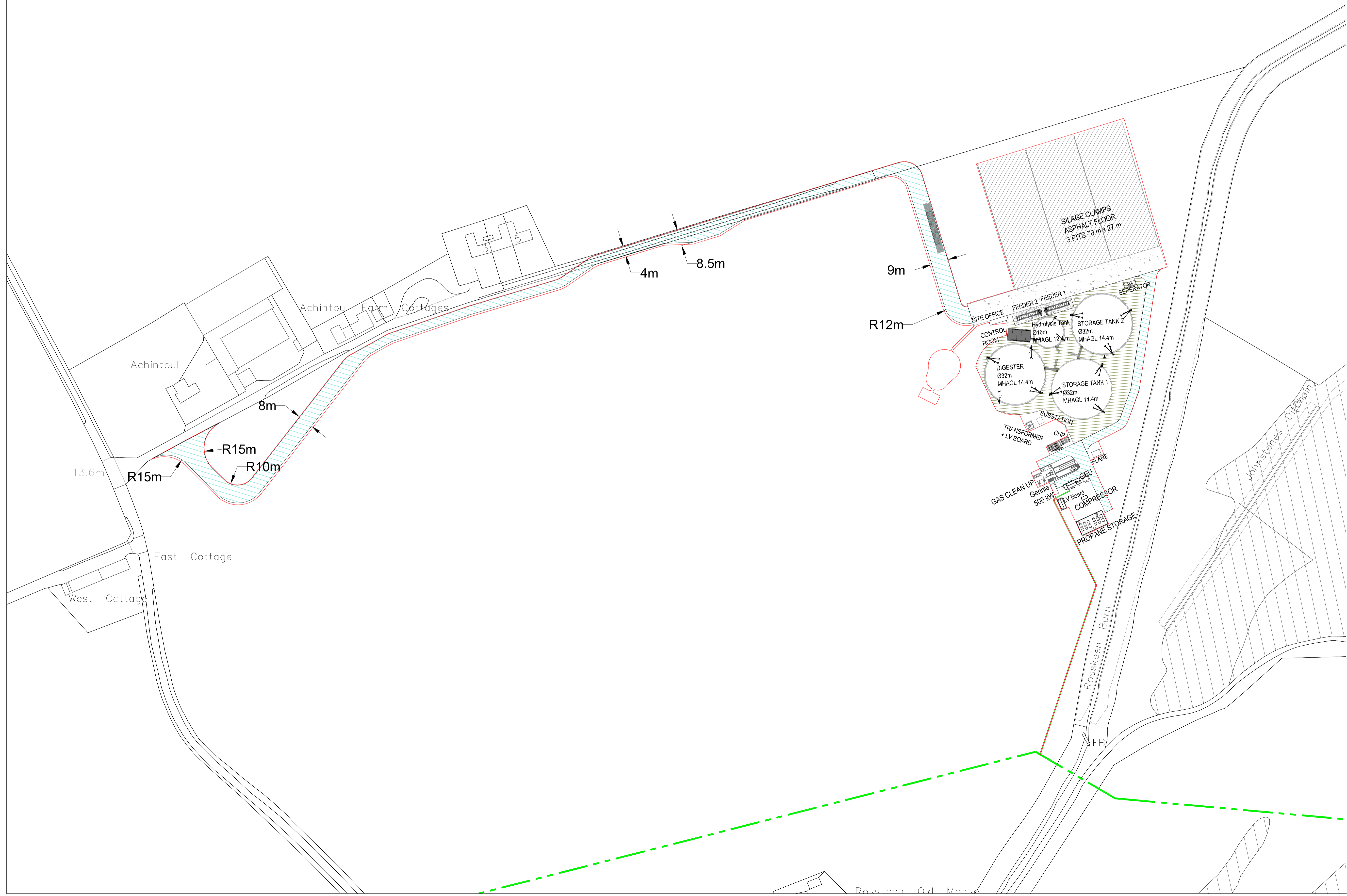
Farm Cluster	Description of Travel
Traffic arriving from the south	Travelling northbound, turning left leave the A9 at the Lidl junction and join the B817 coastal road to Invergordon. Turn left at the Rosskeen Bridge junction to access the U1474 road to the site.
Traffic returning to south	Leave the U1474 at the Rosskeen junction turning left on to the A9 and travel south.
Traffic arriving from the north	Travelling southbound, leave the A9 at the Rosskeen junction turning left on to the U1474 access road to the site.
Traffic returning north (Kildary & Beyond)	Leave the U1474 at the Rosskeen bridge turning left on to the B817 and travel to Kildary via the coastal route. Re-join the A9 at Kildary turning left. No re-joining at Tomich junction.
Traffic returning north (between Rosskeen & Kildary)	Leave the U1474 at Rosskeen bridge turning right on to the B817 travelling towards Alness. Re-join the A9 at the Lidl junction turning left and travelling northbound.
Traffic arriving from west (Across A9 from fields to the west of the A9)	Vehicles to cross over A9 and join the U1474 in one continuous movement with no stopping on the A9. Access to be only taken when both north and south bound lanes are clear. Junction at this point has good two-way visibility.
Traffic arriving from Balintraid	Travel south on B817 coastal road, turn right at Rosskeen bridge on to U1474 to access site. Leave as per traffic returning north, Kildary & Beyond.

Site Signage

At the entrance to the site, signs displaying and reminding the vehicle drivers of the routing will be posted. Signs will also contain emergency contact details for the site.



- Planning Boundary 1.9ha
- Proposed gas line
- - - IP Mains
- Asphalt**
Agricultural mix
- Concrete**
Grade C30
- Stone track**
6F2 + type 1
- Access Track**



Rev	Revision Note	Revision Date	Drawn by	Checked by
R1	For Planning	06.05.16	CR	DM

Project
ROSSKEEN FARM
 INVERGORDON
 IV18 0PL

Drawing
G009A3 Site Layout Plan

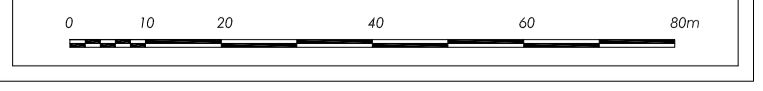


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Date
 May 2016

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