

Agenda Item	6.6
Report No	PLS/071/18

HIGHLAND COUNCIL

Committee: South Planning Applications Committee
Date: 18 September 2018
Report Title: 18/03272/FUL: The Highland Council
Land Between Dores Road and Torvean, Dores Road, Inverness
Report By: Area Planning Manager – South/Major Developments

Purpose/Executive Summary

Description: Amended Design for Inverness West Link Stage 2 - construction of roundabout with new single carriageway road and swing bridge connection to Queens Park Roundabout, bridge control building, vehicular access to Caledonian Canal and realignments of General Booth Road and A82 plus provision of car park, associated in-canal infrastructure, drainage, earthworks, fencing, landscaping, new access tracks/paths, street lighting etc.

Ward: 13 – Inverness West

Development category: Local

Reason referred to Committee: Manager's Discretion

All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

Recommendation

Members are asked to agree the recommendation to **Grant** planning permission as set out in section 11 of the report.

1. PROPOSED DEVELOPMENT

1.1 This application relates to the delivery of Stage 2 of the Inverness West Link from Queens Park Roundabout, crossing of the Caledonian Canal and linking in to a new roundabout at the end of a realigned General Booths Road. This will complete the Inverness Southern Distributor Road (SDR), allowing completion of the City's peripheral neighbourhoods without an unacceptable impact on City centre congestion. This part of the IWL had previously been granted planning permission under planning permission reference: 13/03825/FUL with subsequent amendments to conditions made via a series of applications under S42 of the Town and Country Planning Act. The planning history is contained in Section 3 of this report.

1.2 A number of changes to the previously approved layout and design are proposed through this application. These are summarised as:

- Modification of the Torvean Roundabout from a 4 arm roundabout to a 5 arm roundabout;
- Modification to the accesses to the canal towpath (east and west of the canal) and provision of access to the land at the existing holes 5-8 of the golf course for a future land use;
- Provision of a car park (to be used as a construction compound for the duration of the works);
- Raising the level of the realigned General Booth Road;
- Re-designed surface water drainage system;
- Revisions to the provision of bus stops;
- Details of the design of the bridge control tower;
- Details of the design of the new swing bridge across the canal;
- Details of the canal infrastructure (in-canal and canal side).

1.3 The application is supported by:

- Arboricultural Impact Assessment
- Design and Access Statement – Addendum;
- Design Statement for the Control Tower.
- Economic Assessment for Planning Permission 13/03825/FUL (the original Inverness West Link scheme)
- Environmental Statement for Planning Permission 13/03825/FUL (the original Inverness West Link scheme)
- Environmental Statement Addendum;
- Supporting Statement:
- Transport Assessment for Planning Permission 13/03825/FUL (the original Inverness West Link scheme)
- Tree Survey

1.4 The applicant has made the following variations to the application since submission to address matters raised by consultees:

- Increase in tree planting across the site;
- Provision of a continuous footway/cycleway across the access into the land at existing holes 5-8 of the golf course;

- Changes to the proposed scheme for bus stop provision.

1.5 Further to the variations listed above, the applicant has further committed to the following additional mitigation:

- Provision of a lights controlled crossing where the road crosses the western bank of the Caledonian Canal;
- Provision of a fully detailed access management plan;
- Provision of a full arboricultural method statement prior to construction;
- Provision of an access management plan which will safeguard access along the canal towpath (or via a reasonable diversion) throughout the period of construction
- Inclusion in the contract of the times agreed with Scottish Canals for closure of the canal;
- Detail of the setback from the road of the unbound surface within the car park;
- Provision of a scheme of road signage to meet with the requirements of the Council.

2. SITE DESCRIPTION

- 2.1 The application site comprises areas of the existing Torvean Golf Course (holes 1-4; 8, 15 and 9), an area of the Caledonian Canal and the canal tow path. The application will also affect General Booth Road and the A82(T) through re-alignment works.
- 2.2 The site lies wholly within the catchment of the River Ness.
- 2.3 There are no natural heritage designations covering the site. However, Torvean Landforms Special Scientific Interest (SSSI) lies very close to the proposed scheme. This geomorphological feature contains a range of fluvioglacial landforms (deposited by meltwater) comprising kame terraces, eskers and kettleholes.
- 2.4 The development site is not covered by any international, national, regional or local landscape designation. The site lies within the *Suburban Fringe* and *River Ness and Canal* Landscape Character Types (LCTs) identified in the Inverness District Landscape Character Assessment (LCA) (SNH, 1999).
- 2.5 The Caledonian Canal Scheduled Monument (SM-6499) will be crossed by the new swing bridge. The Tomnahurich Bridge Keeper's Cottage sits adjacent to the Tomnahurich Swing Bridge and is Category B Listed. It will not be affected by the proposed modifications to the scheme. Tomnahurich Cemetery, adjacent to the north of the development, is listed in the national Inventory of Historic Gardens and Designed Landscapes. There are a number of sites of archaeological interest near to the study area.
- 2.6 The hotel, caravan park and housing to north of existing A82(T) are considered noise sensitive properties.

3. PLANNING HISTORY

- | | | | |
|-----|------------|---|--------------------|
| 3.1 | 14.04.2014 | 13/03825/FUL: Construction of 3.2km new single carriageway road, 5 No. roundabouts, a new swing bridge over the Caledonian Canal and a new bridge over the River Ness, with associated drainage, earthworks, fencing, landscaping, new access tracks / paths, street lighting etc | Permission Granted |
| 3.2 | 30.06.2015 | 15/01809/S42: Application under Section 42 to develop land without compliance with conditions 15, 16, 18 and 31 of 13/03825/FUL | Permission Granted |
| 3.3 | 18.12.2015 | 15/03915/S42: Application under Section 42 to develop land without compliance with conditions 2, 3 and 4 of 15/01809/S42 | Permission Granted |
| 3.3 | 03.08.2018 | 18/02843/S42: Application under Section 42 to develop land without compliance with conditions 6 of 15/03915/S42 | Permission Granted |

4. PUBLIC PARTICIPATION

- 4.1 Advertised: Environmental Impact Assessment and Unknown Neighbour
Date Advertised: 03 August 2018 (Inverness Courier and Edinburgh Gazette)
Representation deadline: 01 September 2018
- Timeous representations: 0
- Late representations: 0

5. CONSULTATIONS

- 5.1 **Ballifeary Community Council** did not respond to the consultation
- 5.2 **THC Access Officer** does not object to the application. A number of access matters are highlighted. These can be split into matters concerning construction phase and operational phase. He suggests that these matters can be addressed by an Access Management Plan which can be secured by condition. The construction phase impacts are:
- Temporary diversion of the Great Glen Way for 6 months – consultation with SNH underway;
 - Temporary Diversion of the canal towpath on the east side of the canal;
 - Temporary diversion of the canal towpath on the west side of the canal for 18 months for an undefined length and at an undefined location;
 - Closure of part of the Great Glen Canoe trail for 6 months.

The operational impacts are:

- Permanent diversion of the canal tow path for 160m which increases the distance by approximately 20m beyond the existing route;
- Removal of 60m of canal tow path with a diversion to an uncontrolled road crossing;
- Have an impact on land over which the public has access rights including affecting the between the A82 and Torvean Golf Course;
- Provides additional paths on the local network.

- 5.3 **THC Development Plans Team** has requested a further controlled crossing of the new road to ensure the diversion of the tow path provides an appropriate active travel link. Comment is passed on the need for public art provision and whether the centre of the roundabout is the most appropriate location.
- 5.4 **THC Environmental Health** does not object to the application. It requests conditions to address construction noise and vibration, dust management and bringing forward of previous conditions in relation to operational noise arising from the road.
- 5.5 **THC Flood Risk Management Team** does not object. It requests a condition to secure details of the final surface water drainage scheme.
- 5.6 **THC Forestry Officer** does not object to the application. He raises concerns regarding the potential conflicts between trees and construction, and tree protection measures. Further matters of compensatory planting were raised but these have now been addressed through provision of clarification and an updated landscape drawing.
- 5.7 **THC Transport Planning Team** do not object to the application. It recommends that the cycle/footway on the south side of the route on the 5th arm is extended across the mouth of the proposed maintenance access across the spur of the access. The gravel finish of the road of the parking area needs to be set back a minimum of 6m from the adopted carriageway. Confirmation was requested that the lifting of General Booth Road will not create visibility problems. A scheme for the provision of bus stops was requested in consultation with local operators to address operational requirements. It requires the CEMD to address any conflict between construction access and school hours. Clarification has also been sought in relation to the directional signage arrangements.
- 5.8 **Historic Environment Scotland** do not object to the application. It does not consider the changes to the scheme are material in relation to the associated Scheduled Monuments Consent.
- 5.9 **Scottish Environment Protection Agency** do not object to the application. It does not require a CEMD for matters within their remit.
- 5.10 **Scottish Natural Heritage** do not object to the application.
- 5.11 **Scottish Water** do not object to the application.

5.12 **Transport Scotland** do not object to the application. It requests a condition to ensure the scheme is delivered in accordance with the details shown on the "Design Details and Longsection" drawing.

6. DEVELOPMENT PLAN POLICY

The following policies are relevant to the assessment of the application

6.1 Highland Wide Local Development Plan 2012

08 – Ness-side and Charleston
28 - Sustainable Design
29 - Design Quality & Place-making
30 - Physical Constraints
34 - Settlement Development Areas
51 - Trees and Development
55 - Peat and Soils
56 - Travel
57 - Natural, Built & Cultural Heritage
58 - Protected Species
59 - Other important Species
60 - Other Importance Habitats
61 - Landscape
62 - Geodiversity
63 - Water Environment
64 - Flood Risk
65 - Waste Water Treatment
66 - Surface Water Drainage
70 - Waste Management Facilities
72 - Pollution
73 - Air Quality
74 - Green Networks
75 - Open Space
77 - Public Access
78 - Long Distance Routes

6.2 Inner Moray Firth Local Development Plan (2015)

Allocation IN24 – Torvean and Ness-side

6.3 Supplementary Guidance

Flood Risk & Drainage Impact Assessment (Jan 2013)
Green Networks (Jan 2013)
Highland Historic Environment Strategy (Jan 2013)
Highland's Statutorily Protected Species (March 2013)
Managing Waste in New Developments (March 2013)
Physical Constraints (March 2013)
Public Art Strategy (March 2013)
Standards for Archaeological Work (March 2012)
Sustainable Design Guide (Jan 2013)

Trees, Woodlands and Development (Jan 2013)

Torvean and Ness-side Development Brief (November 2013)

7. OTHER MATERIAL CONSIDERATIONS

7.1 Highland Council Supplementary Planning Policy Guidance

Construction Environmental Management Process for Large Scale Projects
(August 2010)

7.2 Scottish Government Planning Policy and Guidance

- National Planning Framework for Scotland 3 (NPF3)
- Scottish Planning Policy (2014)
- PAN 44 – Fitting New Development into the Landscape
- PAN 60 – Planning for Natural Heritage
- PAN 61 – Planning and Sustainable Urban Drainage Systems
- PAN 65 – Planning and Open Space
- PAN 69 – Planning and Building Standards Advice on Flooding
- PAN 75 – Planning for Transport
- PAN 1/2011 – Planning and Noise
- PAN 2/2011 – Planning and Archaeology
- Circular 1/2018 – Environmental Impact Assessment

8. PLANNING APPRAISAL

8.1 Section 25 of the Town and Country Planning (Scotland) Act 1997 requires planning applications to be determined in accordance with the development plan unless material considerations indicate otherwise.

Determining Issues

8.2 This means that the application requires to be assessed against all policies of the Development Plan relevant to the application, all national and local policy guidance and all other material considerations relevant to the application.

Planning Considerations

8.3 The principle of the development has been established. This is an application to make design changes to the second phase (Stage 2) of the Inverness West Link. While being a fresh application the modifications do not fundamentally change the scheme in terms of its function as a distributor road. In order to address the determining issues therefore, consideration needs to be given to the extent to which the proposal continues to comply with development plan policy and take into consideration any other material considerations.

Development plan/other planning policy

8.4 Development Plan Policy has changed since the time of the determination of the original application through the adoption of the Inner Moray Firth Local Development Plan. This continued to safeguard the land for and seek delivery of

the Inverness West Link. The key issue therefore is whether the modifications to the design would make the development incompatible with the Development Plan.

- 8.5 At the time of consideration of the original application, the Torvean and Ness-side Development Brief had been adopted as a statutory part of the Development Plan. This envisaged a fifth arm from the roundabout to serve any future development at the canal-side and to access the proposed park land to be formed on the area vacated by Torvean Golf Course (Map 6 – Indicative Torvean Masterplan). Whilst the fifth arm and associated access road is on a slightly different alignment than proposed in the indicative masterplan, the principle of the fifth arm is well supported by policy and would facilitate the delivery of the indicative masterplan as it is currently adopted, or any future land uses in the area which may be identified through the review of the Torvean and Ness-side Development Brief.
- 8.6 As Stage 2 of the Inverness West Link progressed into the detailed design stage, further work was progressed to address the matters raised through the original application (i.e. design of the swing bridge and control towers) and associated applications (i.e. vertical alignment of General Booth Road. The remainder of this report will address each of the proposed amendments in turn.

Modification to the accesses to the canal towpath (east and west of the canal) and provision of access to the land at the existing holes 5-8 of the golf course for a future land use

- 8.7 The access to the canal towpath on the original scheme was proposed from a small ramped access coming off the IWL adjacent to the Swing Bridge on the western bank of the canal. This was to provide access for the Rowing Club and other canal users.
- 8.8 The original scheme whilst serving the required purpose could have been enhanced. The scheme as now proposed ensures a road of an improved width, gradient and alignment to facilitate access to the canal tow path. The road will also be able to serve additional development in this area and provide access to the proposed parkland.
- 8.9 Transport Planning had raised concerns over the form of the access to the proposed parkland and the pedestrian connectivity along this route. Transport Planning preferred the continuation of the shared cycleway footway across the access to the parkland as this will be a lesser used vehicular route but likely to be well used for active travel. The applicant has agreed to make the junction between the access road to the canal tow path and the access into the proposed parkland prioritised for active travel users. A detail has been provided to the satisfaction of Transport Planning.
- 8.10 Both the Council's Access Officer and the Development Plans Team requested improved crossing facilities of the IWL to ensure that the severing of the canal towpath on the western bank by the new swing bridge. The applicant had initially shown a dropped curb with an uncontrolled crossing where the towpath rises up to meet the IWL. Following consideration of the matter, the applicant has agreed to put a controlled crossing at this location to facilitate safer crossing of the IWL at this point. This can be secured by condition.

- 8.11 The wider issues of access provision and changes to the access around the area have been carefully considered by the Council's Access Officer. He notes that there are likely to be a number of modifications to access routes in the area but they can be dealt with via the submission of an Access Management Plan. This can be secured by condition and will be required to address access matters for both the construction and operational phases. It is accepted that there will be temporary access restrictions and diversions during construction. Overall, it is considered that access for recreation and active travel will be enhanced as a result of the proposals due to the new linkages that will be created.
- 8.12 The access to the eastern bank is through an improved access down direct from the IWL. While still important this access to the canal towpath would not serve any additional development and is not needed for access to the Inverness Rowing Club clubhouse. The access down will still allow the rowing club access for servicing their events and for Scottish Canals to maintain the canal infrastructure.

Provision of a car park (to be used as a construction compound for the duration of the works)

- 8.13 A car park will be formed between the new access road to the canal and the IWL. The car park will be formed from crushed stone / gravel and will be accessed via the new access road to the canal. The location of the car park will mean that it serves existing canal users, any future uses on the canal side and the proposed parkland.
- 8.14 The interaction between the adopted road and any unbound surface requires to be managed to ensure the unbound stone will not be brought onto the carriageway. Transport Planning have requested that a condition requiring a minimum of 6m bound surface from the edge of the adopted road into the car park to prevent this. This condition can be applied and further a Construction Traffic Management Plan will be secured by condition to ensure this is not an issue during the construction period as this area will be used as a construction compound.

Raising the level of the realigned General Booth Road

- 8.15 During the detailed flood modelling undertaken in the design of the reconfigured Torvean Golf Course, it was identified that if the realignment to General Booth Road was to be taken forward on the original vertical alignment then it would be at risk of flooding in a 1 in 200 year (plus climate change) flood event. To address this matter it is now proposed to lift the level of the re-aligned General Booth Road by an average of approximately 1.5m along its length. For the avoidance of doubt the horizontal alignment has not been modified. The Council's Flood Risk Management Team is content that the change will remove the risk of flooding to the road.
- 8.16 The raising of the road led to some concern from Transport Planning that this may affect visibility out of the junction into the new golf club house. The applicant has confirmed that the junction into the new golf club house was built with the revised vertical realignment in mind and confirmed that the visibility will not be affected. This has been accepted by Transport Planning.

Re-designed Surface Water Drainage System

- 8.17 The area between the new Torvean Roundabout, the new swing bridge and the existing swing bridge will contain the surface water drainage system for Stage 2 of the IWL. This will be formed from an attenuation area. The submitted landscaping scheme shows that this will be delivered to a high standard of landscaping, with areas for seating and shallow slopes creating a well connected greenspace which functions as a surface water drainage system as well. This will also have benefits for biodiversity.
- 8.18 The Flood Risk Management Team is content with the proposals however it requires information on the final detailed design to be secured prior to work commencing. This can be achieved by condition.
- 8.19 The Council's Access Officer welcomes the improved access to the area however has a concern over the finish of the paths, where a bound porous surface is preferred but the applicant proposes a unbound porous surface. There is no guidance on the matter and there are benefits to both approaches. However the use of an unbound surface has a higher lifetime cost due to more regular maintenance being required. The matter remains unresolved between the applicant and the Access Officer. It is proposed that this detail is minor and can be resolved at a later date. A scheme for the construction detail of the paths in this area can be secured by condition.

Revisions to the provisions of bus stops

- 8.20 The original scheme proposed two new bus stops on the re-aligned General Booth Road and two new bus stops between the existing and new swing bridges. As part of this application the applicant proposed to remove all but one of these bus stops.
- 8.22 Concern over the bus stop provision was raised by Transport Planning. It was considered that there was not a clear justification for the change and it would mean that buses would not get the benefit from the new bridge arrangements. Following discussions with the applicant and the bus operators a revised proposal has been brought forward. This includes:
- The bus bays shown on General Booth Road in the original application are to be reinstated in the planning application. They will be staggered and not have raised kerbs. Bus shelters will be provided;
 - The two existing in carriageway stops on the A82, which will be between the swing bridges, will be removed when the new stops on the realigned General Booth Road are operational;
 - The proposed bus bay which was to replace the West bound, in carriageway, stop on the A82 has been deleted from the proposals;
- 8.23 This means that no additional stops on the A82 or on the new leg between the proposed Torvean Roundabout and the proposed swing bridge are required. This approach is satisfactory to Transport Planning and is supported by the bus operators in the area. The proposed new scheme will ensure that bus users get the same benefits from the new bridge arrangement as other motorised users will, meeting with the aspirations of the original scheme and also ensuring conformity

with the Torvean and Ness-side Development Brief.

Design of the Bridge Control Tower

- 8.24 The original West Link application contained an outline design for a bridge control tower. It was thought at the time that further design work was required on the building. The Bridge Control Tower will, in time, operate both the existing Tomnahurich Swing Bridge and the new swing bridge. As this is the case there are a number of operational requirements such as: sufficient height to be able to view both bridges; large areas of glazing to facilitate visibility; areas within the building to house canal equipment; and welfare facilities.
- 8.25 The building proposed meets the operational requirements. The architects for the building were asked at a pre-application stage to consider the design of the surrounding canal infrastructure and the location and visibility of the building by receptors (i.e. people walking along the canal towpath, using the proposed parkland, and travelling past on the road). In doing so they have brought forward a design which is a modern twist on a traditional canal swing bridge control building. Utilising white and black materials matching with the traditional canal side infrastructure, the simple design of the building will be enhanced by design features which have been negotiated during the application and pre-application processes. These include:
- Extended window reveal to make a feature out of the glazing on the upper floor and to break up the mass of the building;
 - Provision of doors / louvres on each side of the building at ground floor level to break up the mass of the building;
 - Provision of glazing on the rear elevation to ensure the back wall is broken up but is also to provide an element of overlooking to the parkland at the rear;
 - Provision of a capping to the top of the wall in a contrasting colour to mirror the featured capping / banding on traditional canal buildings.
- 8.26 These modifications are thought to have enhanced the design of the building. The detailing of such features will be important therefore 1:20 scale drawings of how the features will be brought forward can be secured by condition.

Design of the new swing bridge across the canal

- 8.27 As with the bridge control tower, there were some reservations over the design of the new swing bridge at the time of considering the original application. As this was the case the matter of design of the swing bridge was not given approval. This application brings forward a modified design which no longer mirrors the design of the existing Tomnahurich Swing Bridge.
- 8.28 The proposed design is more modern and will be in contrast to the existing bridge. It follows more closely the design of the new bridge over the River Ness installed as part of IWL Stage 1. Historic Environment Scotland has not objected and do not consider that the change in design has a material affect on the Scheduled Monument Consent which is in place for the delivery of the IWL. The design of the bridge is acceptable.

Details of the canal infrastructure

8.29 A further matter reserved at the time of the original application was the location and design of the in-canal and canal-side infrastructure. This was due to the requirements of canal users now being finalised at the time. Following further discussions between parties this has now been resolved. As part of the IWL Stage 2 works the following infrastructure will be installed and be in addition to the retained and relocated infrastructure:

- 4 new pontoons;
- A wharf;
- A jetty to the south of the swing bridge;
- A jetty to the north of the existing swing bridge.

8.30 This meets with the aspirations in the Torvean and Ness-side Development Brief to enhance the canal infrastructure.

Other material considerations

8.31 In bringing forward the revisions to the design of Stage 2 a number of modifications have been made to the landscaping proposals. These have been further refined through the application process. A number of points of clarity are required prior to commencement of construction including, details of the masonry finish to the underpasses, detail of stone walls, planting schedule, finalisation of the arboricultural method statement and tree protection plans. These matters can be secured by condition.

8.32 The construction of IWL Stage 2 is anticipated to commence in Spring 2019 following completion of the reconfigured Torvean Golf Course and due to be completed in December 2020. The impacts of construction will require to be managed and a Construction Environment Management Document and a Construction Traffic Management Plan can be secured by condition. These will be able to address amenity impacts during construction including those highlighted by consultees. Further a planning monitoring officer, ecological clerk of works and landscaping clerk of works will be required.

8.33 Given the nature of the proposal and location at a key city gateway, the provision of public art should be included in the scheme.

8.34 Having reviewed the findings of the original assessments undertaken, the Environmental Statement Addendum submitted with this application and consultation responses, it is not considered that the proposed modifications to the scheme will have a material affect on their findings.

9. CONCLUSION

9.1 The proposed modifications to the design for Stage 2 of the Inverness West Link accord with the principles of the original Inverness West Link project approval, the supporting studies and the Development Plan. Clarification on a number of minor matters remain to be required but as with any scheme these can be secured by condition. It is of note that there have been no representations on the application.

- 9.2 Overall, it is considered that the amendments to the design of IWL Stage 2 are positive ones that will facilitate the delivery of development in the area and allow for the completion of the last section of the Southern Distributor Road.
- 9.3 All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

10. IMPLICATIONS

- 10.1 Resource: Not applicable
- 10.2 Legal: Not applicable
- 10.3 Community (Equality, Poverty and Rural): Not applicable
- 10.4 Climate Change/Carbon Clever: Not applicable
- 10.5 Risk: Not applicable
- 10.6 Gaelic: Not applicable

11. RECOMMENDATION

Action required before decision issued N

Subject to the above, it is recommended that planning permission be **GRANTED**, subject to the following:

Conditions and Reasons

- 1 Planning Permission is hereby granted for a length of single carriageway road with a roundabout, re-alignment of General Booth Road, a swing bridge over the Caledonian Canal and associated development defined as Stage 2 of the Inverness West Link and as shown on Drawing No. PL04 Revision 9 Design Details and Longsection)

Reason: In order to clarify the terms of the permission and to ensure that the standard of infrastructure modification proposed to the trunk road complies with the current standards, and that safety and free flow of the traffic on the trunk road is not diminished.

- 2 No development shall commence on Stage 2 until a scheme, including work programme, to ensure that a fully operational 18 hole golf course and ancillary facilities, including club house, parking area and practice facilities (the Torvean Golf Course) are available at all times both during and after construction of this stage of the road have been submitted to, and agreed in writing, by the Planning Authority in consultation with SportScotland. The agreed scheme shall be implemented.

Reason: To ensure that an appropriate 18 hole golf course is available at all times during and after construction of Stage 2 of the road link.

- 3 No development shall commence on Stage 2 until confirmation that an appropriate and binding agreement between Transport Scotland and Scottish Canals regarding the sequence and operation of both canal swing bridges, that takes into consideration the needs of Inverness Rowing Club, has been secured.

Reason: To ensure that the new swing bridge will be capable of being opened when required by Inverness Rowing Club for rowing regattas.

- 4 For the avoidance of doubt the design and location of the replacement Jacobite Cruises Building shown on drawing HRS7126 PL31 is not approved.

Any revised design and location, and a schedule for implementation being no later than 18 months after the first opening of Stage 2 of the Inverness West Link, shall be submitted to, and approved in writing by the Planning Authority no later than 30 September 2019. Thereafter only the approved designs shall be constructed in line with the approved schedule for implementation

Reason: To ensure that the building is in a suitable location in terms of operational need and the design is of an appropriate quality to this important gateway into Inverness.

- 5 No development shall commence on Stage 2 until details of the natural stone finish of the entrance walls, including parapet, string courses, reveals and returns, leading into the proposed underpass at Torvean Roundabout have been submitted to, and agreed in writing by, the Planning Authority. Only the agreed detailing and materials shall be implemented

Reason: To ensure a high standard of design that is compatible with the setting of the Caledonian Canal and the general aspiration for improvements to the public realm in this location.

- 6 All walling identified within on the approved plans as landscape design and mitigation shall be constructed of natural stone details which shall be submitted to, and agreed in writing by, the Planning Authority prior to commencement of the stage to which it relates. The approved details shall be implemented and thereafter maintained to the satisfaction of the Planning Authority.

Reason: To ensure a high standard of design and finish in the interest of visual amenity.

- 7 No development shall start on Stage 2 until a Construction Environmental Management Document is submitted to, and agreed in writing by, the Planning Authority. The Document shall include:

An updated Schedule of Mitigation (SM) including all mitigation

proposed in support of the planning application, other relevant agreed mitigation (e.g. as required by agencies) as may be set out in other relevant planning conditions.

- a. Processes to control / action changes from the agreed Schedule of Mitigation.
- b. Processes for informing neighbouring residents, potentially with involvement of Community Councils, of the work programme and likely significant events such as temporary road/path closures, heavy loads and working hours.
- c. The following site specific Construction and Environmental Management Plans (CEMP);
 - i. pollution prevention plan
 - ii. drainage and surface water management plan
 - iii. chemical pollution plan
 - iv. waste management plan
 - v. species protection plans, specifically with regard to protection of Bat, Otter and Red Squirrel
 - vi. Noise and vibration mitigation plan based on guidance with Parts 1 & 2 of BS5228: "Code of Practice for Noise and Vibration Control on Construction and Open Sites" including assessment and mitigation for any piling requirements
 - vii. Dust mitigation plan based on the GLA/London Council guidance. Details of the appointment of an appropriately qualified

Environmental Clerk of Works with roles and responsibilities which shall include but not necessarily be limited to:

- i. Providing training to the developer and contractors on their responsibilities to ensure that work is carried out in strict accordance with environmental protection requirements;
- ii. Monitoring compliance with all environmental and nature conservation mitigation works and working practices approved under this consent;
- iii. Advising the developer on adequate protection for environmental and nature conservation interests within, and adjacent to, the application site;
- iv. Directing the placement of the development (including any micro-siting, if permitted by the terms of this consent) and the avoidance of sensitive features; and
- v. The power to call a halt to development on site where environmental considerations warrant such action. Details of any other methods of monitoring, auditing, reporting and communication of environmental management on site and with the client, Planning

Authority and other relevant parties. Statement of any additional persons responsible for 'stopping the job / activity' if in potential breach of a mitigation or legislation occurs. Unless otherwise agreed in writing by the Planning Authority the development shall proceed in accordance with the agreed Document.

Thereafter the approved document shall be implemented.

Reason: To protect the environment from the construction and operation of the development.

8 No development shall commence on each stage until a Construction Traffic Management Plan (CTMP) has been submitted to, and agreed in writing by, the Planning Authority in consultation with the Roads Authorities. The CTMP shall include details relating to:

- Traffic management measures including accommodation works to manage construction traffic
- Measures to minimise traffic impacts on existing users
- Measures to accommodate pedestrians, cyclists and bus users
- Details of temporary signage
- Details of construction vehicle routing

The measures identified within CTMP and agreed shall be implemented.

Reason: To protect non motorised users and maintain the safety and free flow of the road network.

9 No development shall commence on each stage until the following road drainage details have been submitted to, and agreed in writing by, the Planning Authority:

- Infiltration basins, including inlet/outlet structures, cross sections, fencing and signage;
- An operation and maintenance manual detailing the frequency and type of maintenance measures for all proposed SUDS infrastructure;
- The design of the roadside ditches.

The agreed details, and maintenance as appropriate, shall thereafter be implemented to the satisfaction of the Planning Authority.

Reason: In order to ensure that the design and maintenance of the SUDS infrastructure is appropriate.

- 10 No development shall commence on each stage until a Construction Traffic Noise Management Plan is submitted to, and approved in writing by, the Planning Authority. The Construction Traffic Noise Management Plan shall include a schedule of monitoring of impacts on sensitive receptors, details of working hours, any mitigation related to construction traffic noise set out in any Construction Traffic Management Plan which is required under Condition 8 of this planning permission and any mitigation related to construction traffic noise set out in the Construction Environment Management Document required under Condition 7 of this planning permission. The approved mitigation/management controls shall be implemented in accordance with the agreed programme, unless otherwise agreed in writing by the planning authority.

Reason: To protect the amenity of sensitive receptors from traffic noise during construction.

- 11 All bridge deck expansions joints shall satisfy the requirements of Standard BD33 'Expansion Joints for Use in Highway Bridge Decks', details of which shall be submitted to, and approved in writing by, the Planning Authority prior to the commencement of development on the swing bridge.

No later than the last day of April following the first opening of the of the swing bridge, baseline measurements of traffic noise by means of a noise survey shall be undertaken at nearby noise sensitive premises. The noise survey shall detail the traffic flows across the bridge joint, over 10 minute measuring periods with measuring periods set to commence on the hour and at 10 minute consecutive increments thereafter for a period of 1 hour at both the AM and PM peak. Measurements shall be calculated at the nearest noise sensitive receptors to be determined and agreed in writing by the Planning Authority in advance of commencement of the survey. This baseline noise survey shall be submitted to the Planning Authority no later than three months prior to the first anniversary of the opening of the swing bridge. Following this, in the event of a complaint and/or at the request of the Planning Authority, the developer shall assess, at its own expense and using a suitably qualified consultant(s), the level of noise emissions from the bridge joints using the same methodology employed in the baseline noise survey.

Where noise emissions are found to exceed those contained within the baseline noise survey, then the developer shall identify as part of their assessment appropriate mitigation measures including, but not limited to, the replacement of the bridge joint.

The assessment and any proposed mitigation, including the timescale for implementation, shall be submitted to, and approved in writing by, the Planning Authority within one month of such a request being made under this condition, unless otherwise agreed in writing by the Planning Authority. The agreed mitigation measures shall thereafter be implemented in full.

Reason: To protect the amenity of sensitive receptors from traffic noise during construction.

- 12 No later than six months after the first anniversary of the opening of Stage 2, a Noise Monitoring Report, that shall compare the predicted traffic noise contained within the ES against actual traffic noise on the nearest sensitive receptors to the development and identify any further mitigation deemed necessary to protect noise sensitive receptors, shall be submitted to, and approved in writing by, the Planning Authority. Any mitigation identified shall be undertaken in accordance with recommendations of the Report no later than 12 months of approval, unless otherwise agreed in writing by the Planning Authority.

Reason: To protect the amenity of sensitive receptors from traffic noise that was not predicted.

- 13 No development shall commence on Stage 2 until a scheme for monitoring both motorised and non-motorised travel patterns within the road network during the first year of operation has been submitted to, and agreed in writing by, the Planning Authority. Within six months of the first anniversary of the opening of Stage 2, a report that compares the predicted effects contained with the Transport Assessment submitted in support of this application and actual affects shall be submitted, and agreed in writing by the Planning Authority.

Reason: In order to monitor the effectiveness of the proposed development.

- 14 No development shall commence on Stage 2, until pre-commencement surveys to locate the presence or absence of otter, bats and squirrel is undertaken and a report of survey has been submitted to, and approved in writing by, the Planning Authority. The survey shall inform any mitigation measures identified in the Species Protection Plan required as part of the Construction Environmental Management Document/Plan(s) approved under Condition 7.

Reason: To protect nature conservation interests from construction activities.

- 15 No development shall commence on each stage until a Badger Protection Plan has been submitted to, and agreed in writing, by the Planning Authority in consultation with Scottish Natural Heritage. The agreed plan shall be implemented.

Reason: To protect the badger interest of the site.

- 16 All tree works are to be carried out by a suitably qualified and experienced arboriculturalist in accordance with BS:3998(2010) Tree Work - Recommendations.

Reason: To ensure the tree works are carried out to a high standard.

- 17 Prior to any site excavation or groundworks within Stage 2 a Tree Protection Plan, Arboricultural Method Statement and a Scheme of Supervision (all in accordance with BS5837:2012 - Trees in Relation to Design, Demolition and Construction) are to be submitted to and subsequently approved in writing by the planning authority. All retained trees are to be protected against construction damage using protective barriers located beyond the Root Protection Area (in accordance with BS5837:2012 Trees in Relation to Design, Demolition & Construction). Barriers are to remain in place throughout the construction period and must not be moved or removed without the prior written approval of the Planning Authority.

Reason: To ensure the protection of retained trees during construction and thereafter.

- 18 A suitably qualified Arboricultural consultant shall be employed at the applicant's expense to ensure that the Tree Protection Plan and Arboricultural Method Statement are implemented to the agreed standard. Stages requiring supervision as per the approved Scheme of Supervision under Condition 17 are to be agreed with the Planning Authority prior to the commencement of work on each Stage. Certificates of compliance shall be submitted for approval following completion.

Reason: To ensure the protection of retained trees throughout the construction period.

- 19 No development shall commence on Stage 2 until a detailed Landscape Plan, to include details of type/location and size of species relevant to any required compensatory tree planting, and programme for its maintenance has been submitted to, and approved in writing by, the Planning Authority. The Landscape Plan shall be implemented in full during the first planting season following commencement of development or as otherwise may be agreed in writing by the Planning Authority.

Reason: In the interests of amenity and to secure appropriate compensatory planting.

- 20 A suitably qualified landscape consultant shall be employed, at the applicant's expense, to ensure that the Landscape Plan is implemented and thereafter maintained to the agreed standard. Stages requiring supervision are to be agreed with the Planning Authority prior to the commencement of work on Stage 2. Certificates of compliance shall be submitted for approval following completion.

Reason: In the interests of amenity and ensuring that appropriate mitigation treatments are secured.

- 21 No development shall commence on Stage 2 until a detailed Access Management Plan for public access across the site (as existing, during construction and following completion) has been submitted to, and approved in writing by, the Planning Authority. The plan shall include details showing:
- i. All existing access points, paths, core paths, tracks, rights of way and other routes (whether on land or inland water), and any areas currently outwith or excluded from statutory access rights under Part One of the Land Reform (Scotland) Act 2003, within and adjacent to the application site;
 - ii. Any areas proposed for exclusion from statutory access rights, for reasons of privacy, disturbance or effect on curtilage related to proposed buildings or structures;
 - iii. All paths, tracks and other routes for use by walkers, riders, cyclists and any other relevant outdoor access enhancement i.e. car park (including construction specifications, signage, information leaflets, proposals for on-going maintenance etc.);
 - iv. Any diversion of paths, tracks or other routes (whether on land or inland water), temporary or permanent, proposed as part of the development (including details of mitigation measures, diversion works, duration and signage). The approved Access Management Plan, and any associated works, shall be implemented in full prior to the first occupation of the development or as otherwise may be agreed within the approved plan;
 - v. Full details of the canal closures proposed;
 - vi. Clarification on the type, construction and finish of all paths proposed within the development.

Reason: To safeguard and maximise the opportunities for continued public access in and around the development site in accordance with Policy 77 of the HwLDP.

- 22 No development shall commence on Stage 2 until a programme for the evaluation, preservation and recording of any archaeological and historic features affected by the proposed development, including a timetable for investigation, all in accordance with the attached specification, has been submitted to, and agreed in writing by, the Planning Authority. The agreed proposals shall be implemented in accordance with the agreed timetable for investigation.

Reason: In order to protect the archaeological and historic interest of the site.

- 23 The proposed controlled crossing point of Stage 2 to the west of the Swing Bridge shall be constructed to a layout, type and method of construction that shall be submitted to and agreed by the Planning Authority prior to the commencement of development. The approved details shall thereafter be implemented prior to first opening of the road to traffic.

Reason: To facilitate a safe active travel crossing of the proposed road.

- 24 The proposed access to the parkland area for maintenance shall be formed with active travel priority and designed to the standards set out for commercial access in Table 7.4 of the Roads and Transportation Guidelines for New Developments (or any superseding document prevailing at the time) to the shall be submitted to and agreed by the Planning Authority prior to the commencement of development. The approved details shall thereafter be implemented prior to first opening of the road to traffic.

Reason: To facilitate a safe active travel crossing of the proposed road.

- 25 For the avoidance of doubt, the cohesive finishing material shall extend from the adopted road into the proposed car park for a distance of at least 6m back from the nearside edge of the public road.

Reason: In the interests of road safety.

- 26 No development shall commence until the Planning Authority has approved the terms of appointment and the identity of the appointee by and at the cost of the Developer of an independent and suitably qualified consultant to assist the Planning Authority in the monitoring of compliance with conditions attached to this deemed planning permission during the period from commencement of Development to the date of completion of the development.

Reason: To enable the Development to be suitably monitored during the construction phase to ensure compliance with the permission issued.

- 27 No development or work shall commence until a detailed specification for all proposed external materials and finishes for the control building, plant building and bridge parapets (including trade names and samples where necessary) has been submitted to, and approved in writing by, the Planning Authority. Thereafter, development and work shall progress in accordance with these approved details.

Reason: In the interests of visual amenity.

- 28 No development shall commence on site until a scheme for the inclusion of public art within the development, including types and locations of artworks and the management and maintenance thereof, has been submitted to, and approved in writing by, the Planning Authority. The approved scheme shall be implemented prior to occupation of the development and maintained in perpetuity.

Reason: To ensure the delivery of a development with a unique identity which facilitates the creation of place.

- 29 No development shall commence until a community liaison group is established by the developer, in collaboration with The Highland Council and

local Community Councils. The group shall act as a vehicle for the community to be kept informed of project progress and, in particular, should allow advanced dialogue on the provision of all transport-related mitigation measures. This should also ensure that local events and tourist seasons are considered and appropriate measures to co-ordinate deliveries and work with these and any other major projects in the area to ensure no conflict between construction traffic and the increased traffic generated by such events / seasons / developments. The liaison group, or element of any combined liaison group relating to this development, shall be maintained until the development has been completed and is fully operational.

Reason: To assist with the provision of mitigation measures to minimise the potential hazard to road users, including pedestrians travelling on the road networks.

REASON FOR DECISION

All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

TIME LIMIT FOR THE IMPLEMENTATION OF THIS PLANNING PERMISSION

In accordance with Section 58 of the Town and Country Planning (Scotland) Act 1997 (as amended), the development to which this planning permission relates must commence within THREE YEARS of the date of this decision notice. If development has not commenced within this period, then this planning permission shall lapse.

FOOTNOTE TO APPLICANT

Initiation and Completion Notices

The Town and Country Planning (Scotland) Act 1997 (as amended) requires all developers to submit notices to the Planning Authority prior to, and upon completion of, development. These are in addition to any other similar requirements (such as Building Warrant completion notices) and failure to comply represents a breach of planning control and may result in formal enforcement action.

1. The developer must submit a Notice of Initiation of Development in accordance with Section 27A of the Act to the Planning Authority prior to work commencing on site.
2. On completion of the development, the developer must submit a Notice of Completion in accordance with Section 27B of the Act to the Planning Authority.

Copies of the notices referred to are attached to this decision notice for your convenience.

Accordance with Approved Plans & Conditions

You are advised that development must progress in accordance with the plans approved under, and any conditions attached to, this permission. You must not deviate from this permission without consent from the Planning Authority (irrespective of any changes that may separately be requested at the Building Warrant stage or by any other Statutory Authority). Any pre-conditions (those requiring certain works, submissions etc. prior to commencement of development) must be fulfilled prior to work starting on site. Failure to adhere to this permission and meet the requirements of all conditions may invalidate your permission or result in formal enforcement action

Flood Risk

It is important to note that the granting of planning permission does not imply there is an unconditional absence of flood risk relating to (or emanating from) the application site. As per Scottish Planning Policy (paragraph 259), planning permission does not remove the liability position of developers or owners in relation to flood risk.

Scottish Water

You are advised that a supply and connection to Scottish Water infrastructure is dependent on sufficient spare capacity at the time of the application for connection to Scottish Water. The granting of planning permission does not guarantee a connection. Any enquiries with regards to sewerage connection and/or water supply should be directed to Scottish Water on 0845 601 8855.

Local Roads Authority Consent

In addition to planning permission, you may require one or more separate consents (such as road construction consent, dropped kerb consent, a road openings permit, occupation of the road permit etc.) from the Area Roads Team prior to work commencing. These consents may require additional work and/or introduce additional specifications and you are therefore advised to contact your local Area Roads office for further guidance at the earliest opportunity.

Failure to comply with access, parking and drainage infrastructure requirements may endanger road users, affect the safety and free-flow of traffic and is likely to result in enforcement action being taken against you under both the Town and Country Planning (Scotland) Act 1997 and the Roads (Scotland) Act 1984.

Further information on the Council's roads standards can be found at: <http://www.highland.gov.uk/yourenvironment/roadsandtransport>

Application forms and guidance notes for access-related consents can be downloaded from:

http://www.highland.gov.uk/info/20005/roads_and_pavements/101/permits_for_working_on_public_roads/2

Mud & Debris on Road

Please note that it is an offence under Section 95 of the Roads (Scotland) Act 1984 to allow mud or any other material to be deposited, and thereafter remain, on a public road from any vehicle or development site. You must, therefore, put in place a strategy for dealing with any material deposited on the public road network and maintain this until development is complete.

Construction Hours and Noise-Generating Activities: You are advised that construction work associated with the approved development (incl. the loading/unloading of delivery vehicles, plant or other machinery), for which noise is audible at the boundary of the application site, should not normally take place outwith the hours of 08:00 and 19:00 Monday to Friday, 08:00 and 13:00 on Saturdays or at any time on a Sunday or Bank Holiday in Scotland, as prescribed in Schedule 1 of the Banking and Financial Dealings Act 1971 (as amended).

Work falling outwith these hours which gives rise to amenity concerns, or noise at any time which exceeds acceptable levels, may result in the service of a notice under Section 60 of the Control of Pollution Act 1974 (as amended). Breaching a Section 60 notice constitutes an offence and is likely to result in court action.

If you wish formal consent to work at specific times or on specific days, you may apply to the Council's Environmental Health Officer under Section 61 of the 1974 Act. Any such application should be submitted after you have obtained your Building Warrant, if required, and will be considered on its merits. Any decision taken will reflect the nature of the development, the site's location and the proximity of noise sensitive premises. Please contact env.health@highland.gov.uk for more information.

Protected Species – Halting of Work

You are advised that work on site must stop immediately, and Scottish Natural Heritage must be contacted, if evidence of any protected species or nesting/breeding sites, not previously detected during the course of the application and provided for in this permission, are found on site. For the avoidance of doubt, it is an offence to deliberately or recklessly kill, injure or disturb protected species or to damage or destroy the breeding site of a protected species. These sites are protected even if the animal is not there at the time of discovery. Further information regarding protected species and developer responsibilities is available from SNH: www.snh.gov.uk/protecting-scotlands-nature/protected-species

Signature: Nicola Drummond
Designation: Area Planning Manager – South/Major Developments
Author: Simon Hindson
Background Papers: Documents referred to in report and in case file.
Relevant Plans: Plan 1 PL02 Rev 2 – Location Plan
Plan 2 - PL01 REV 4 – Site Layout Plan
Plan 3 - PL03 REV 4 – Temporary Compounds
Plan 4 - PL04 Rev 9 – Design Details and Longsection
Plan 5 - PL27 REV 2 – Underpass Design Details

Plan 6 - PL29 REV 2 – CCTV General Arrangement Plan

Plan 7 - PL32 REV 01 – Swing Bridge Plant Building

Plan 8 - PL40 REV 4 – Canal Infrastructure Plan

Plan 9 - PL41 REV 2 – Pontoon Details Plan

Plan 10 - PL42 REV 3 – Proposed Jetty Arrangement Plan

Plan 11 - PL43 REV 2 – Proposed Wharf Cross Section

Plan 12 - PL33 Rev 0 – Cycle Path Details

Plan 13 - HRS7126-CS-02-3000-DR-L-1002 REV P2 – Environmental Mitigation Plan

Plan 14 - HRS7126-CS-S2-7000-DR-S-0001 REV P2 – Swing Bridge General Arrangement Plan

Plan 15 - HRS7126-CS-S2-7000-DR-S-0002 REV P2 – General Arrangement Plan

Plan 16 - HRS7126-CS-02-3000-DR-L-001 REV P01 – Tree Planting Sheet 1

Plan 17 - HRS7126-CS-02-3000-DR-L-002 REV P01 – Tree Planting Sheet 2

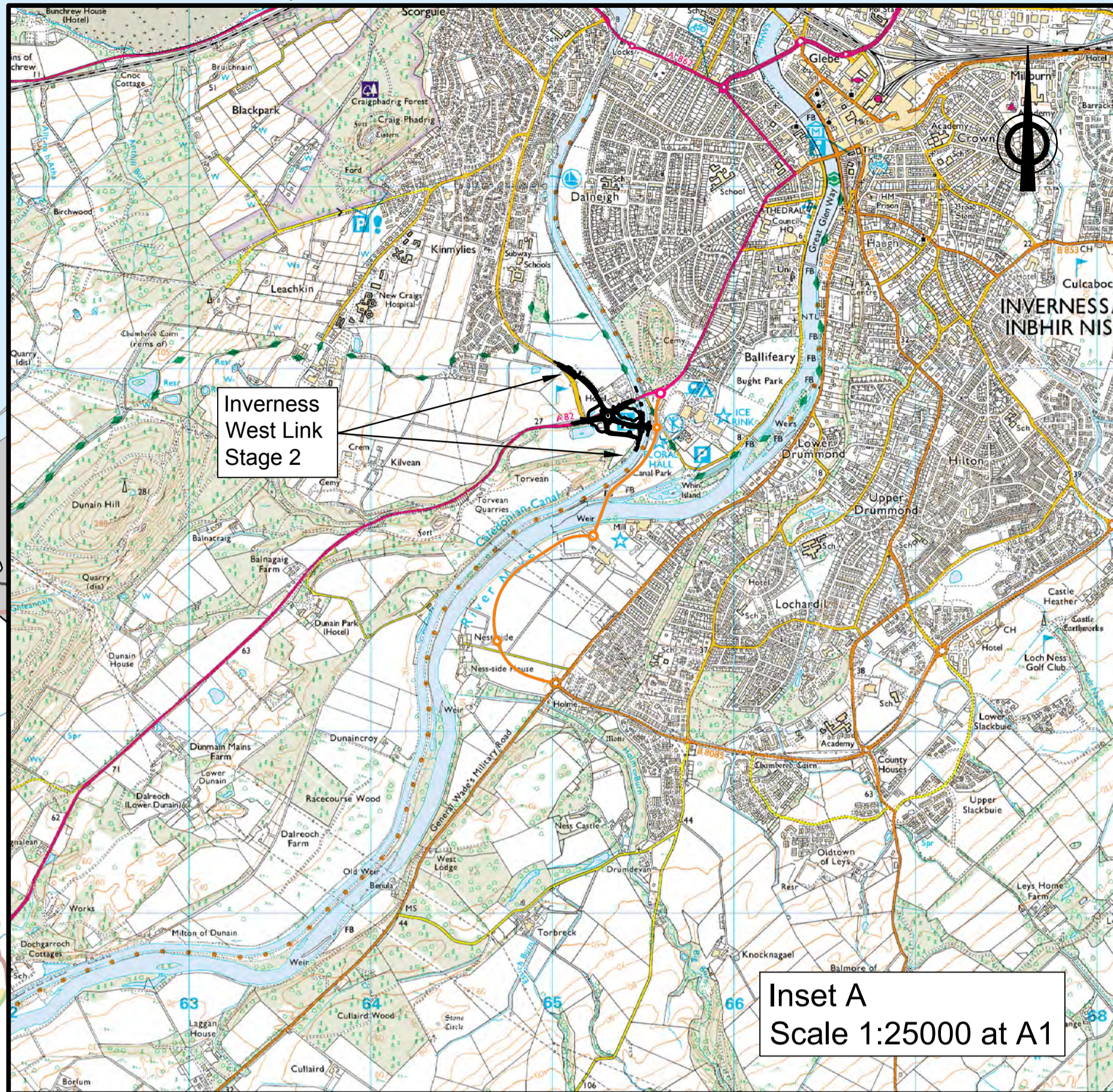
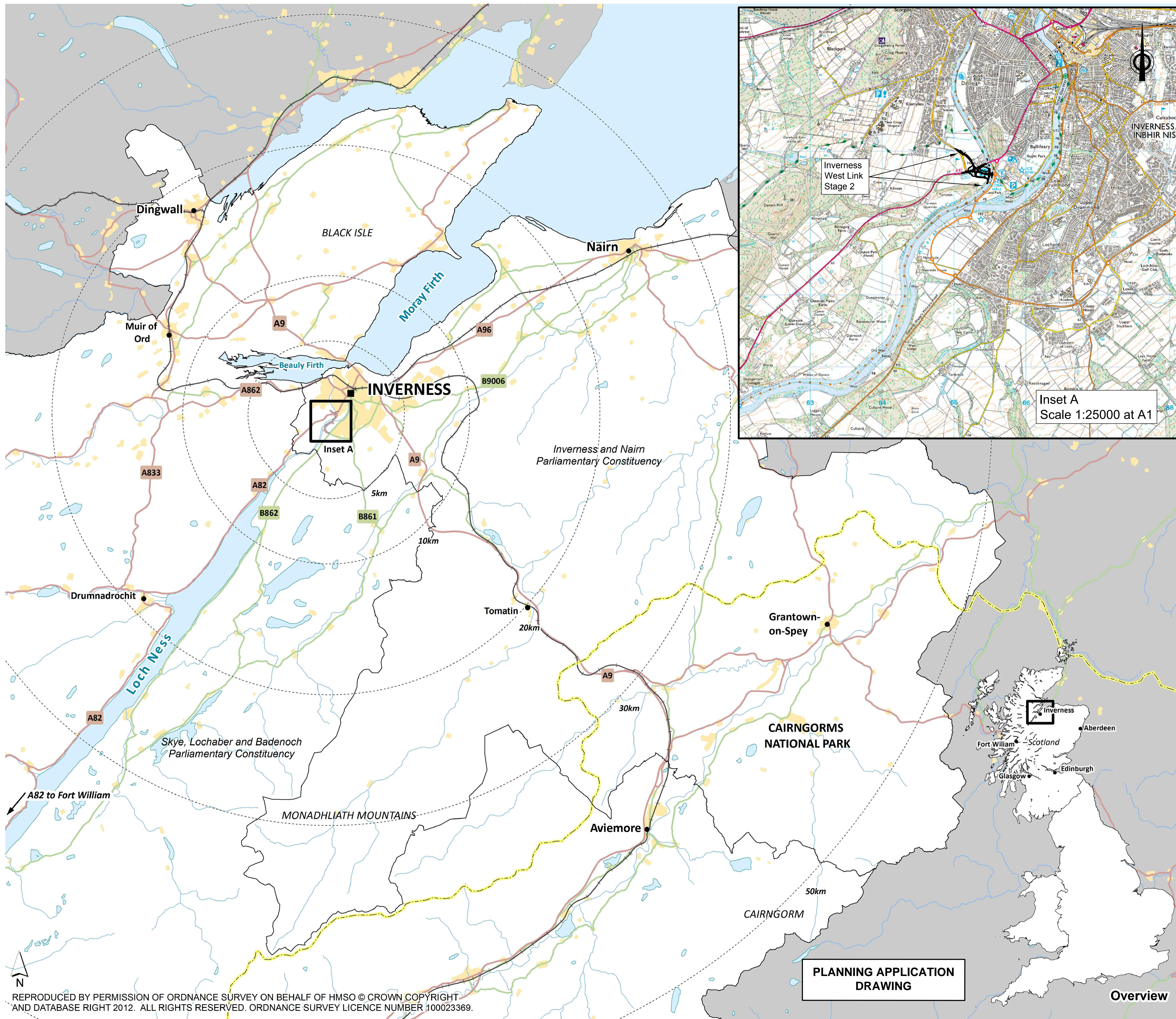
Plan 18 - HRS7126-CS-02-3000-DR-L-003 REV P01 – Tree Planting Sheet 3

Plan 19 - DI7048-ZZ-XX-DR-A-2401 Rev G – Floor Plan

Plan 20 - DI7048-ZZ-XX-DR-A-3401 Rev G – Elevation Plan

Plan 21 - DI17048-THC-ZZ-XX-DR-A-4401 Rev E – Section Plan (Control Tower)

Plan 22 - DI17048-ZZ-XX-DR-A-1401 Rev E – Site Layout Plan (Control Tower)



COPYRIGHT, DESIGNS and PATENTS ACT 1988:
 This drawing may not be reproduced, copied or used in any way without the written permission of the Director of Transport, Environmental & Community & Services.
 © THE HIGHLAND COUNCIL

Notes
 Reproduced from the Ordnance Survey mapping with the permission of the Controller of Her Majesty's Stationary Office © Crown Copyright. Unauthorised reproduction infringes Crown copyright and may lead to prosecution or civil proceedings.
 The Highland Council 2014 100023369

SCHEME TITLE AMENDED	JJA	20.06.2018	r2
FIVE ARM TORVEAN ROUNDABOUT	JJA	22.05.2018	r1

Revision Details	By	Date	Surf's
------------------	----	------	--------



DEVELOPMENT & INFRASTRUCTURE

PROJECT DESIGN UNIT
 Osprey House
 Ainess Point Business Park
 Ainess, IV17 0UP

Phone : 01349 886741
 Fax : 01349 886749
 E-Mail : bryan.stout@highland.gov.uk

Project **The Highland Council (Inverness West Link Stage 2 Modifications) Planning Application**

Title **Location Plan**

Scale (at A1) **As Shown**

Drawn	JJA	Date	04/10/2013
Checked	PB	Date	04/10/2013

Project No: **HRS7126**

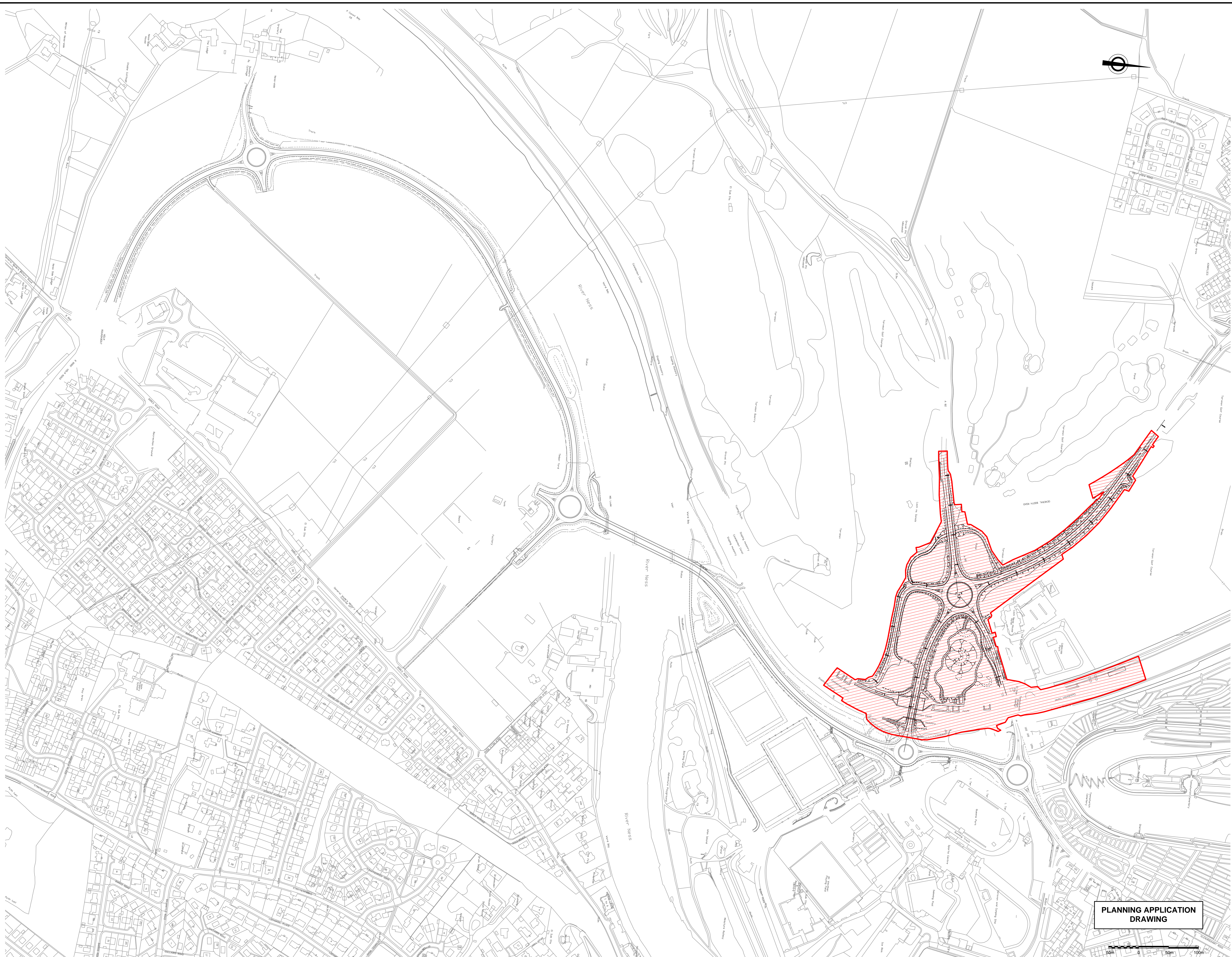
Drawing No: **PL02** Rev: **2**

REPRODUCED BY PERMISSION OF ORDNANCE SURVEY ON BEHALF OF HMSO © CROWN COPYRIGHT AND DATABASE RIGHT 2012. ALL RIGHTS RESERVED. ORDNANCE SURVEY LICENCE NUMBER 100023369.

PLANNING APPLICATION DRAWING

Overview

AS: Inverness West Link Road/2 - Drawings/1 - Working/URS1206_P101_4 - Red Line Boundary.dwg: 19/07/2018



COPYRIGHT, DESIGN and PATENTS ACT 1988:
This drawing may not be reproduced, stored or used in any way without the written permission of the Director of Transport, Environmental & Community Services.
© THE HIGHLAND COUNCIL

Notes
Reproduced from the Ordnance Survey mapping with the permission of the Controller of Her Majesty's Stationary Office © Crown Copyright. Unauthorised reproduction infringes crown copyright and may lead to prosecution or civil proceedings.
The Highland Council 2014 100023569

KEY
[Red hatched box] EXTENT OF PLANNING BOUNDARY
[Red hatched box] AREA OF RED LINE BOUNDARY 91021 m²

SCALEBAR ADDED	WS 19 07 2018	r4
CANAL JETTY MOVED NORTHWARDS.	JJA 11 07 2018	r3
RED LINE BOUNDARY REDUCED TO COVER STAGE 2 WORKS ONLY.	JJA 20 06 2018	r2
RED LINE BOUNDARY EXTENDED TO INCLUDE LAND REQUIRED FOR ACCESS TO CANAL FROM SW ARM OF REVISED ROUNDABOUT AND RAISED GENERAL BOOTH ROAD.	JJA 14 05 2018	r1
Revision Details	By	Date

CAPITA
Property and Infrastructure
Inverness Business Park, Cullis, Cullis, Cullis
www.capita.co.uk
Capita Property and Infrastructure Ltd.

The Highland Council
Comhairle na Gàidhealtachd

DEVELOPMENT & INFRASTRUCTURE

PROJECT DESIGN UNIT
Osprey House
Alness Point Business Park
Alness, IV17 0UP

Phone : 01349 886741
Fax : 01349 886749
E-Mail : bryon.stuart@highland.gov.uk

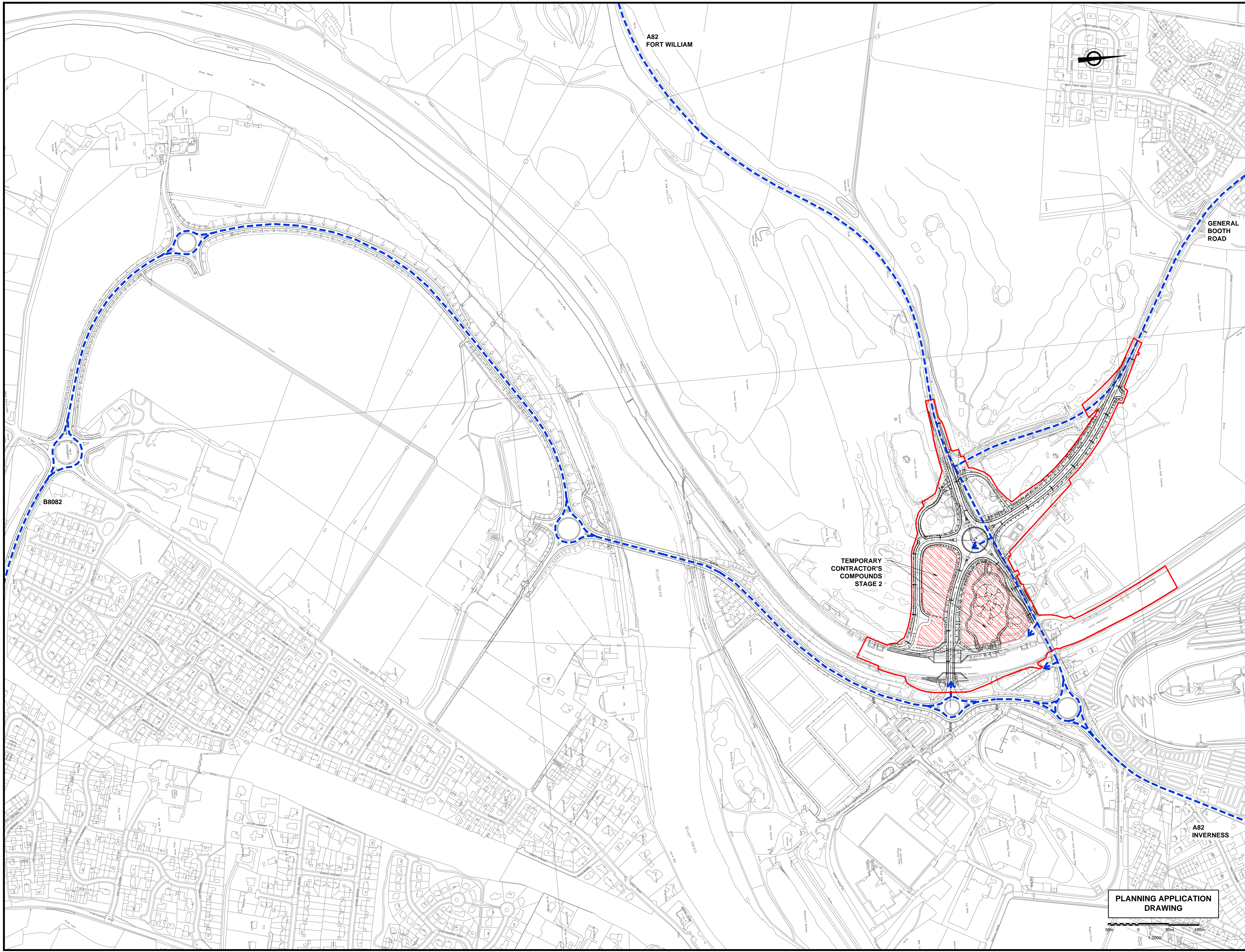
Project: The Highland Council
(Inverness West Link
Stage 2 Modifications)
Planning Application

Title
Site Plan
(Red Line Boundary)

**PLANNING APPLICATION
DRAWING**




Scale (at A0) 1:2000

Drawn	RG	Date	16/06/2013
Checked	PB	Date	16/06/2013
Project No:			



COPYRIGHT, DESIGNS and PATENTS ACT 1986:
 This drawing may not be reproduced, copied or used in any way without the written permission of the Director of Transport, Environmental & Community Services.
 © THE HIGHLAND COUNCIL

Notes
 Reproduced from the Ordnance Survey mapping with the permission of the Controller of Her Majesty's Stationary Office © Crown Copyright.
 Unauthorised reproduction infringes Crown copyright and may lead to prosecution or civil proceedings.
 The Highland Council 2013 100023369

- KEY**
-  PLANNING RED LINE BOUNDARY
 -  TEMPORARY COMPOUNDS AND STOCKPILES
 -  SITE ACCESS ROUTE

- NOTES**
1. INDICATIVE ARRANGEMENT ONLY. LOCATION OF ADDITIONAL CONTRACTOR'S COMPOUNDS AND STOCKPILE AREAS SUBJECT TO AGREEMENT BETWEEN CONTRACTOR AND LANDOWNERS.
 2. MATERIALS ARE EXPECTED TO BE DELIVERED FROM QUARRY ON DEMAND FOR DIRECT PLACEMENT, SO NO STOCKPILING OTHER THAN TOPSOIL IS ANTICIPATED.
 3. ADVANCED SIGNING PROVISION NOT SHOWN.

SCALEBAR ADDED WS 19 07 2018 r4

CANAL JETTY MOVED NORTHWARDS JJA 11 07 2018 r3

GENERAL BOOTH ROAD ADDED AS ALLOWABLE SITE ACCESS ROUTE JJA 20 06 2018 r2

REVISED FOR STAGE 2 WORKS JJA 14 05 2018 r1

Revision	Details	By	Date	Surf's

CAPITA
 Property and Infrastructure
 Kingsway Business Park, Carlisle, Cumbria, CA4 6JL
 01273 872000
 www.capitaproperty.co.uk
 Capita Property and Infrastructure Ltd

The Highland Council
 Comhairle na Gàidhealtachd

DEVELOPMENT & INFRASTRUCTURE

PROJECT DESIGN UNIT
 Osprey House
 Alness Point Business Park
 Alness, IV17 0UP

Phone : 01349 886741
 Fax : 01349 886749
 E-Mail : bryan.stout@highland.gov.uk

Project
 The Highland Council
 (Inverness West Link
 Stage 2 Modifications)
 Planning Application

Title
 Temporary
 Compounds

Scale (at A0)
 1:2000

Drawn	RG	Date	01/10/2013
Checked	PB	Date	01/10/2013

Project No:
 HRS7126

Drawing No: PL03 Rev: 4

**PLANNING APPLICATION
 DRAWING**



Notes

Reproduced from the Ordnance Survey mapping with the permission of the Controller of Her Majesty's Stationary Office. Crown Copyright. Unauthorised reproduction, storage, retrieval, copying, or use in any form is prohibited.
© Crown Copyright 2014. Licence number: G100023569

- DESIGN PARAMETERS:**
 - TORVEAN ROUNDABOUT
 - CIRCULATORY CARRIAGEWAY WIDTH 6.4m
 - ENTRY WIDTH TO 7m
 - CYCLEWAY WIDTH 3m
 - VERGE WIDTH 1.0m
 - ARC LINKS AND GENERAL BOOTH ROAD**
 - DESIGN SPEED 50km
 - CARRIAGEWAY WIDTH 7.3m
 - CYCLEWAY WIDTH 3m
 - VERGE WIDTH 1.0 to 1.5m
- ROAD LEVELS SHOWN MAY BE VARIED BY +/- 0.3m.
- ONLY TRAFFIC SIGNS LARGER THAN 2m x 2m SHOWN ON DRAWING
- ROAD DRAINAGE COMPRISES A SYSTEM OF POSITIVE RUNOFF COLLECTION DISCHARGING TO GROUND VIA A TWO STAGE SUDS TREATMENT TRAIN. RUNOFF WILL BE COLLECTED USING KERBS AND GULLIES OR COMBINED KERBS DRAINAGE UNITS. THE FIRST STAGE OF SUDS TREATMENT WILL CONSIST OF EITHER LINED DRAINAGE DITCHES OR GRAVEL FILTER CHAMBERS AND THE SECOND STAGE WILL BE PROVIDED BY INFILTRATION BASINS. WHERE VARIABLE BASINS WILL INCORPORATE HIGH LEVEL OVERFLOWS TO ADJACENT WATERCOURSE. ALL SUCH OVERFLOWS WILL INCORPORATE FLOW CONTROL DEVICES TO LIMIT DISCHARGE RATES TO GREENFIELD EQUIVALENT.
- STREET LIGHTING FOR THE INVERNESS WEST LINK COMPRISES OF A SYSTEM OF 10m HIGH LIGHTING COLUMNS AT APPROXIMATELY 40m SPACINGS. COLUMN SPACING MAY BE CLOSER THAN 40m WHERE NECESSARY TO LIGHT INDIVIDUAL FEATURES SUCH AS BUS STOPS, JUNCTIONS, ETC.
STREET LIGHTING FOR THE PROPOSED FOOTWAY/CYCLEWAY UNDERPASS LINKS WILL BE BY 5m HIGH COLUMNS AT APPROXIMATELY 20m SPACINGS. LIGHTINGS WILL BE L.E.D. DIRECTIONAL UNITS THAT MINIMISE ENERGY CONSUMPTION AND LIGHT SPILLAGE.

KEY

- PROPOSED ROAD
- PROPOSED FENCE
- PROPOSED SHARED USE CYCLEWAY / FOOTWAY
- PROPOSED FOOTWAY
- EXISTING CORE PATH TO REMAIN.
- REDUNDANT AREA OF EXISTING ASPHALT TO BE LANDSCAPED.
- CROSS SECTION DRAWING LOCATION (DRAWING NUMBER IN BUBBLE)

Revision Details		By	Date	Surf
BUS STOPS MOVED FROM A82 TO GENERAL BOOTH ROAD. FID CROSSING WEST OF NEW SWING BRIDGE CHANGED FROM UNCONTROLLED TO SIGNAL CONTROLLED TOUJAN.				
JJA	31/08/19			r9
SCALE BARS ADDED.				
WS	19/07/18			r8
BACKGROUND MAPPING UPDATED TO SHOW ADDITIONAL EXISTING CANAL JETTES.				
JJA	11/07/17			r7
NEW CANAL JETTY MOVED NORTHWARDS.				
JJA	11/07/16			r6
SCHEME TITLE AMENDED.				
JJA	20/06/16			r5
FILLING STATION ENTRANCES RETAINED. CHANGES TO CANAL INFRASTRUCTURE. OVERFLOW CAR PARKING AREA. PARK RUN NOTE ADDED.				
JJA	20/06/16			r4
JJA	20/12/15			r3
TORVEAN TO JACOBITE CRUISES LINK AMENDED.				
JJA	05/12/15			r2
JJA	06/11/17			r1
STAGE 1 WORKS SHOWN AS BUILT. JACOBITE CRUISES AND CANAL TOW PATH ACCESS ROAD AMENDED WITH 5TH ARM FROM TORVEAN ROUNDABOUT. GEN. BOOTH ROAD BUS LAYBY REMOVED. GEN. BOOTH ROAD BUS LAYBY MOVED. BUS LAYBY ADDED TO A82. EASTERN TOWPATH TRUNK ROAD FID CROSSING MOVED TO UNDERPASS.				

CAPITA
Property and Infrastructure
Angus Street, Falkirk, Scotland, G11 6EQ
01753 210060
www.capita.gov.uk
Capita Design and Information Ltd.



DEVELOPMENT & INFRASTRUCTURE
PROJECT DESIGN UNIT
Osprey House
AIness Point Business Park
AIness, N17 0UP

Phone : 01349 886741
Fax : 01349 886749
E-Mail : kayvon.stuart@highland.gov.uk

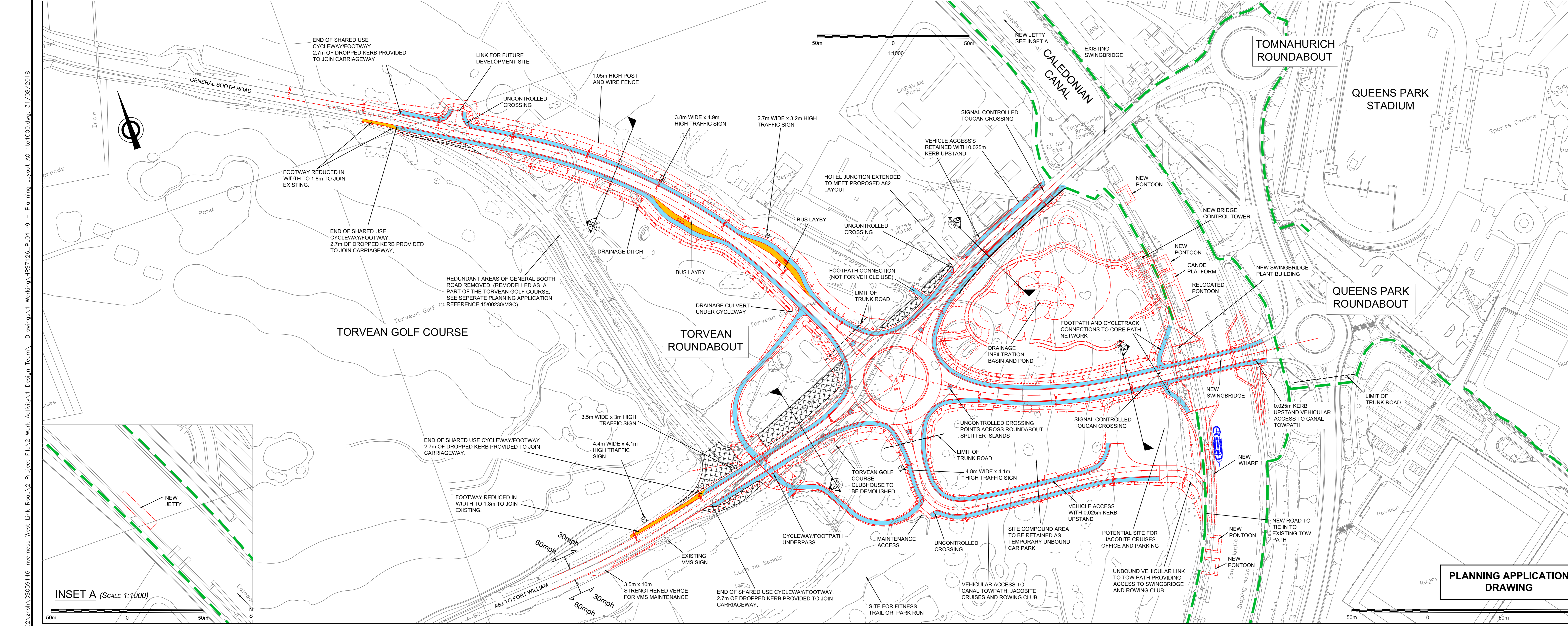
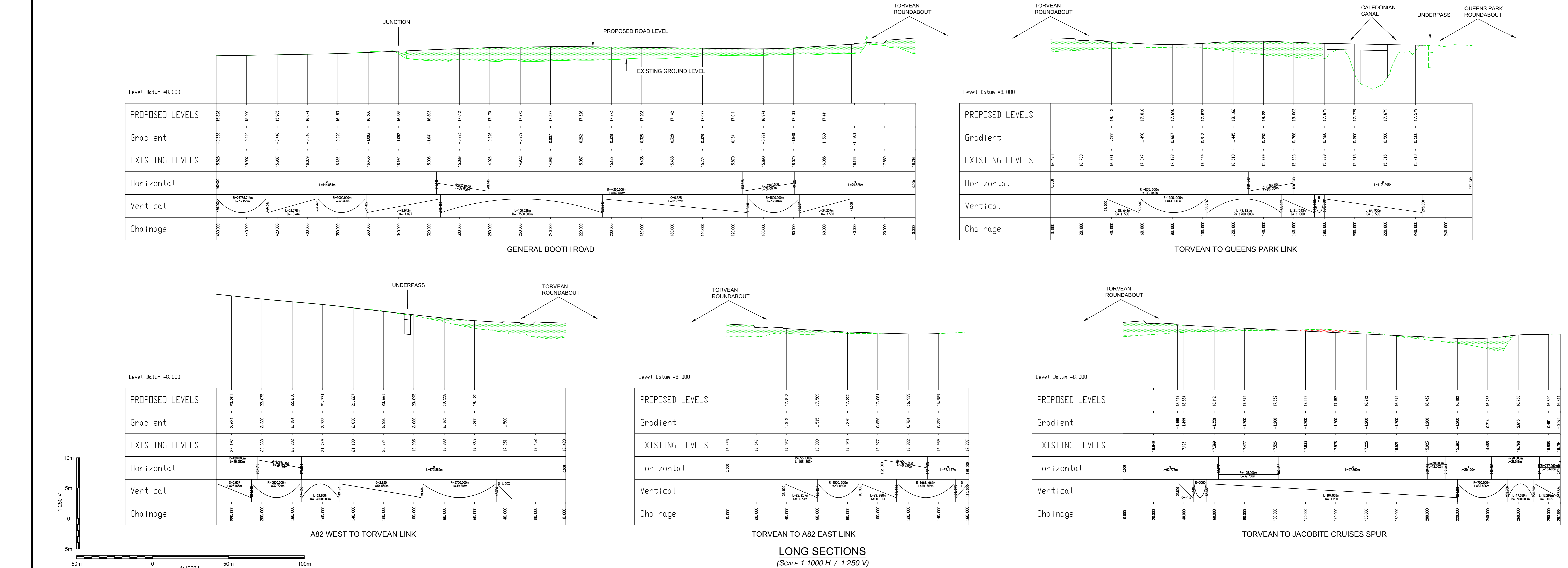
The Highland Council (Inverness West Link Stage 2 Modifications) Planning Application

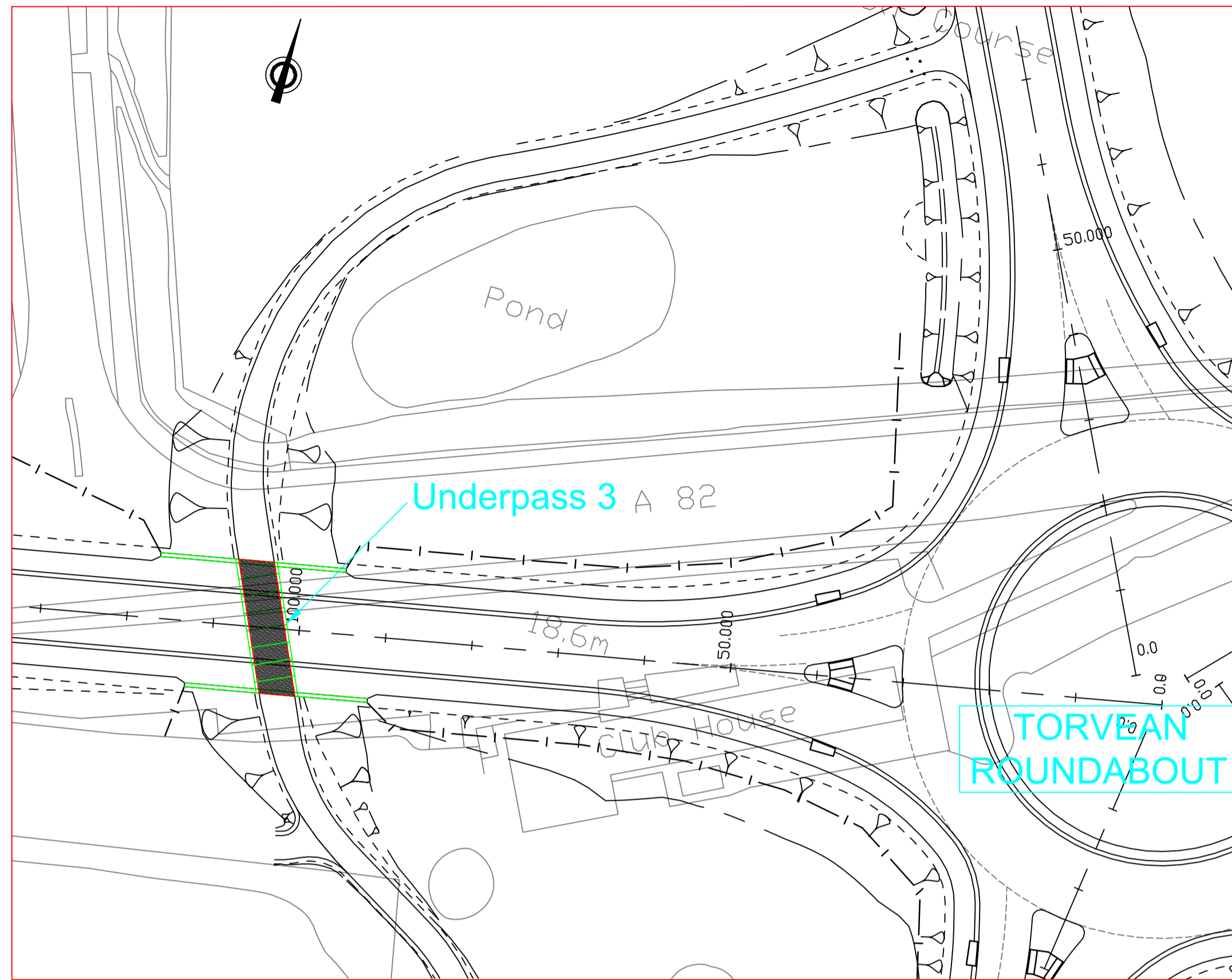
Design Details Plan and Longsection

Scale (of A0)
1:1000 H / 1:250 V

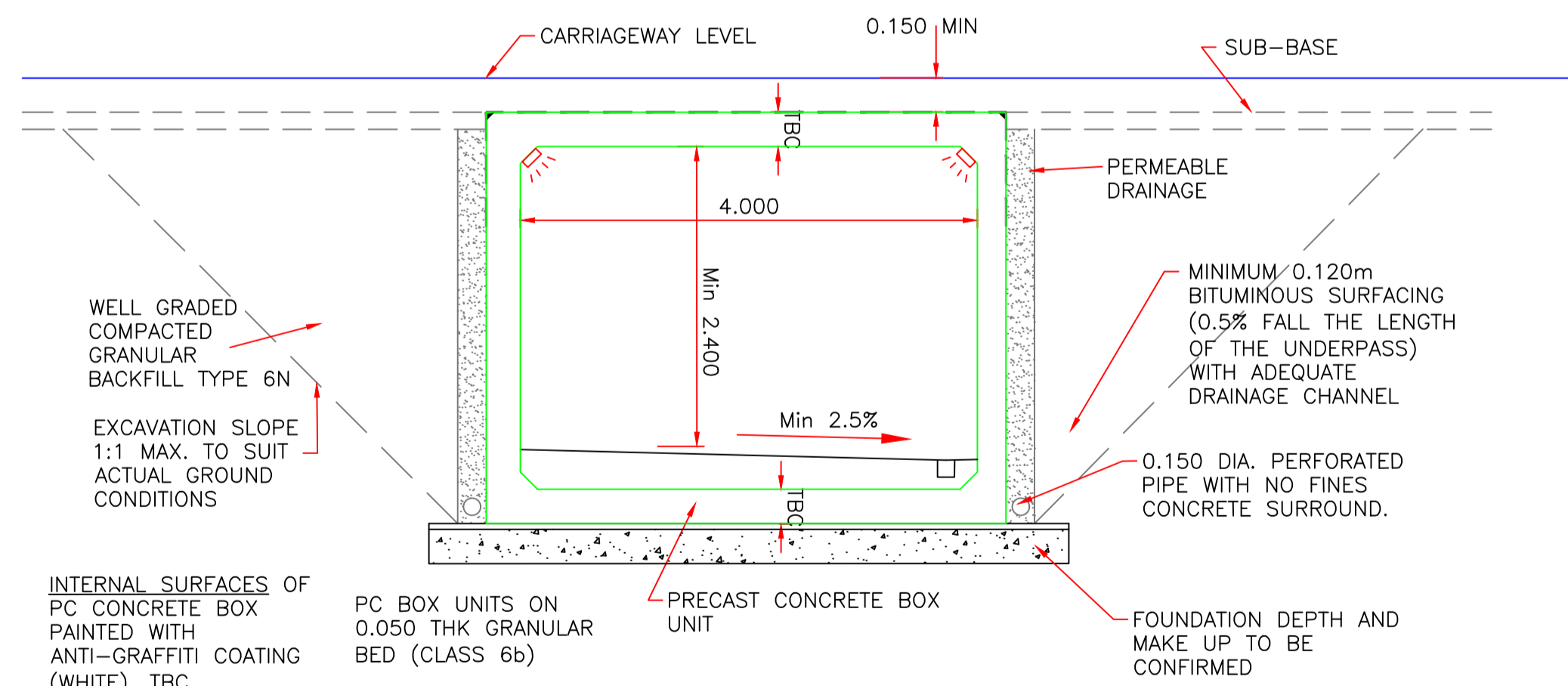
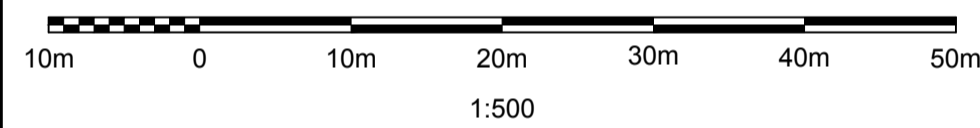
Drawn	JJA	Date	25/09/2013
Checked	PB	Date	25/09/2013

Project No: **HRS7126**
Drawing No: **PL04** Rev: **9**

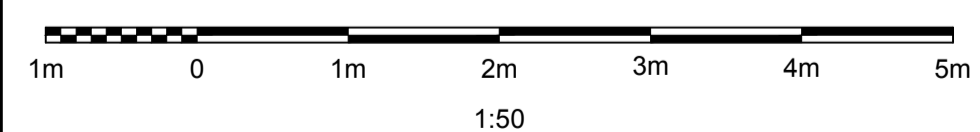




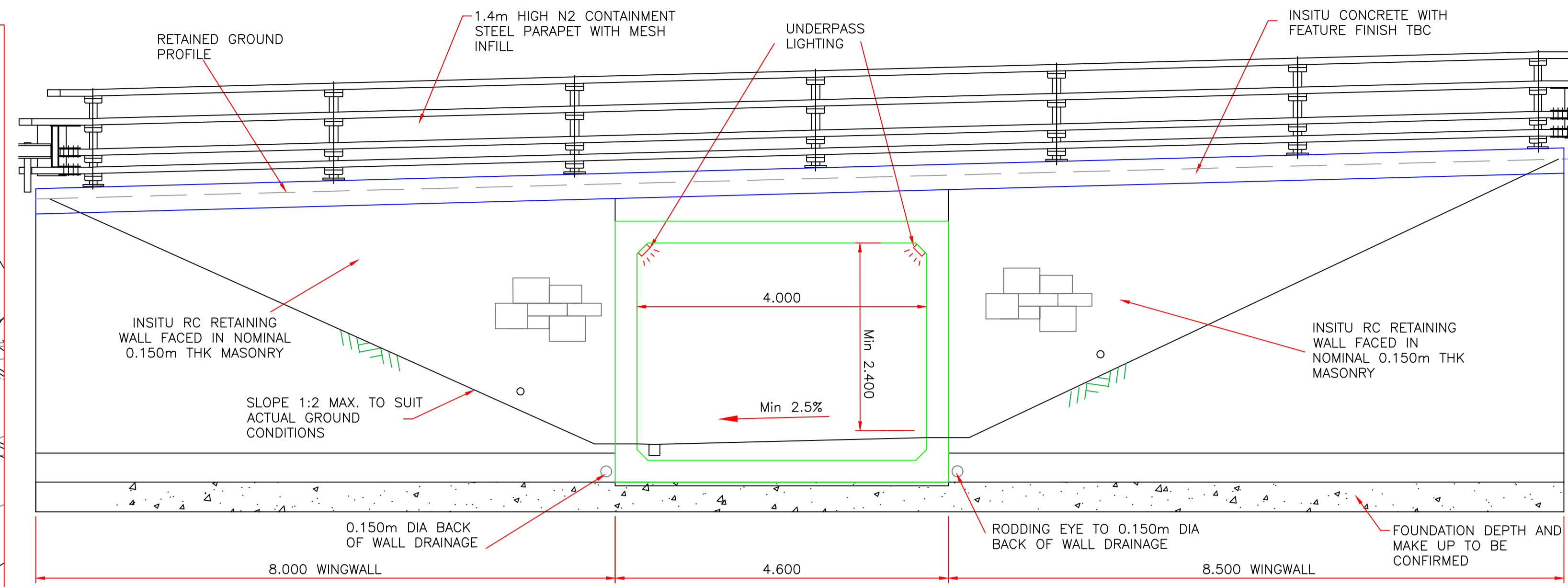
**UNDERPASS 3
LOCATION PLAN**
Scale 1:500



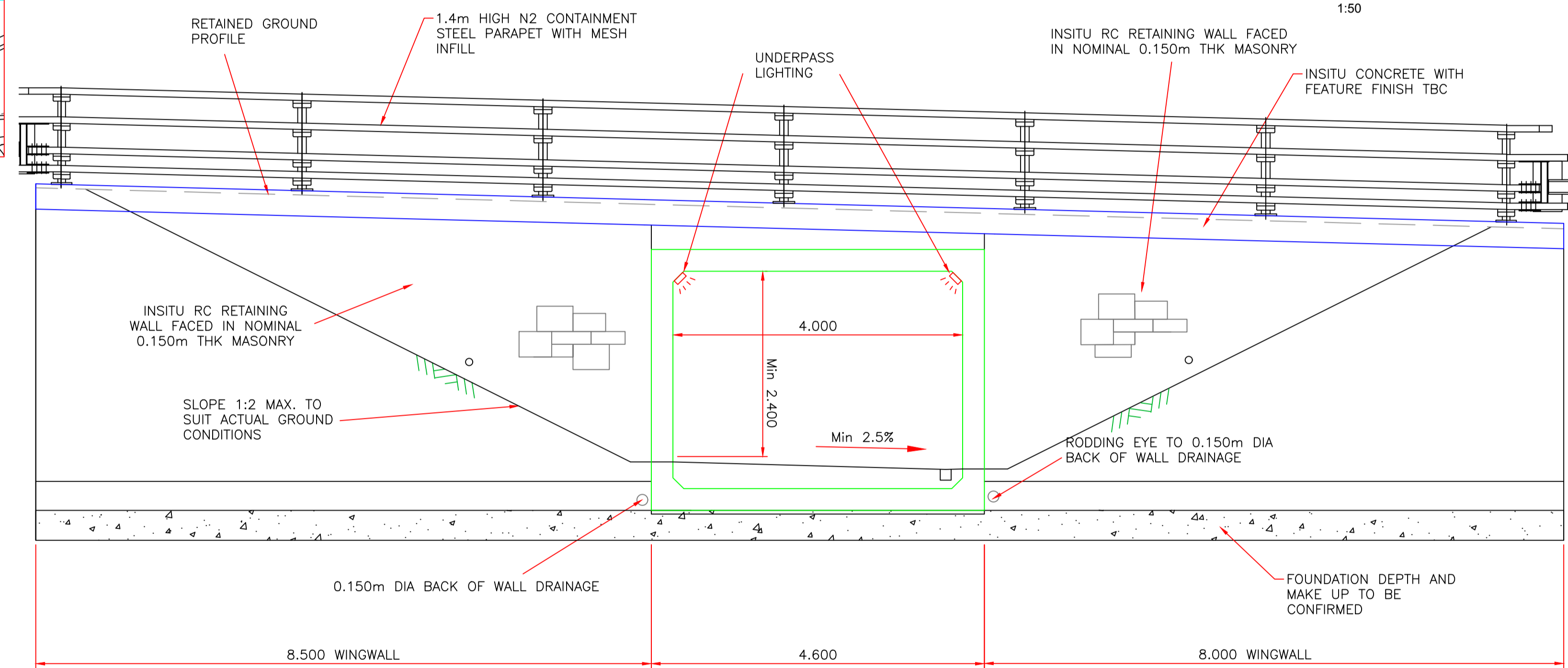
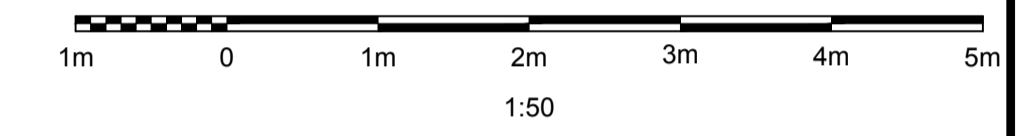
TYPICAL SECTION THROUGH UNDERPASS 3
Scale 1:50



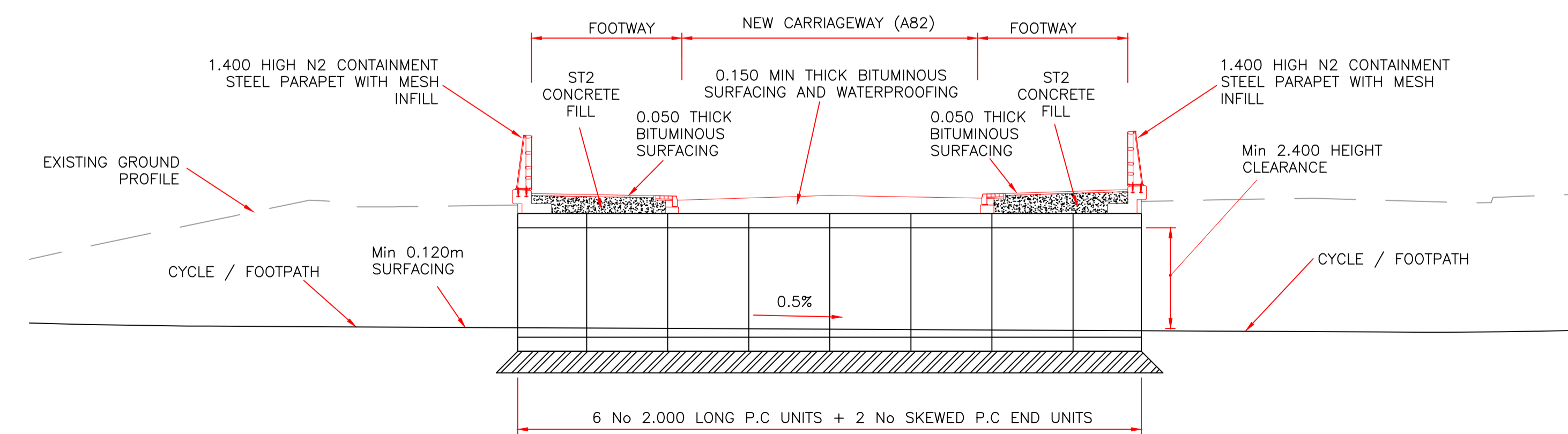
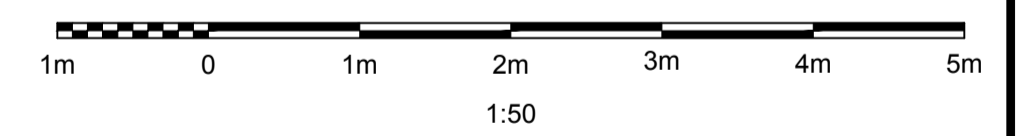
**PLANNING APPLICATION
DRAWING**



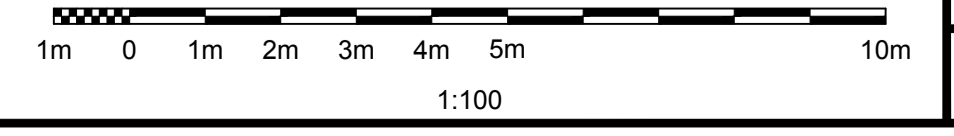
NORTH ENTRANCE ELEVATION OF UNDERPASS 3
Scale 1:50



SOUTH ENTRANCE ELEVATION OF UNDERPASS 3
Scale 1:50



LONG SECTION ALONG CENTRELINE OF UNDERPASS 3 (SOUTH TO NORTH)
SCALE 1:100



COPYRIGHT, DESIGNS and PATENTS ACT 1988:
This drawing may not be reproduced, copied or used in any way without the written permission of the Director of Transport, Environmental & Community Services.
© THE HIGHLAND COUNCIL

Notes

1. CCTV system to be installed to cover the underpasses and approaches (not shown on drawing). CCTV locations and type to be confirmed.
2. Only low level planting to be provided in vicinity of underpass entrances to maximise visibility and improve personal security.
3. Dimensions and general arrangement details may be subject to minor changes at detailed design.

DIMENSIONS CHANGED TO METRES	WS 19/07/2018	r2
SCALEBARS ADDED		
PARAPET CHANGED TO STEEL	JJA 14/05/2018	r1
UNDERPASS WIDTH REDUCED TO 4.0m		

Revision Details	By	Date	Sur/x
------------------	----	------	-------

CAPITA
Property and Infrastructure
Kingmoor Business Park, Carlisle, Cumbria, CA6 4SJ
01228 673889
www.capitaproperty.co.uk
Capita Property and Infrastructure Ltd.

The Highland Council
Comhairle na Gàidhealtachd

DEVELOPMENT & INFRASTRUCTURE
PROJECT DESIGN UNIT
Osprey House
Alness Point Business Park
Alness, IV17 0UP

Phone : 01349 886741
Fax : 01349 886749
E-Mail : bryan.stout@highland.gov.uk

Project
The Highland Council
(Inverness West Link
Stage 2)
Planning Application

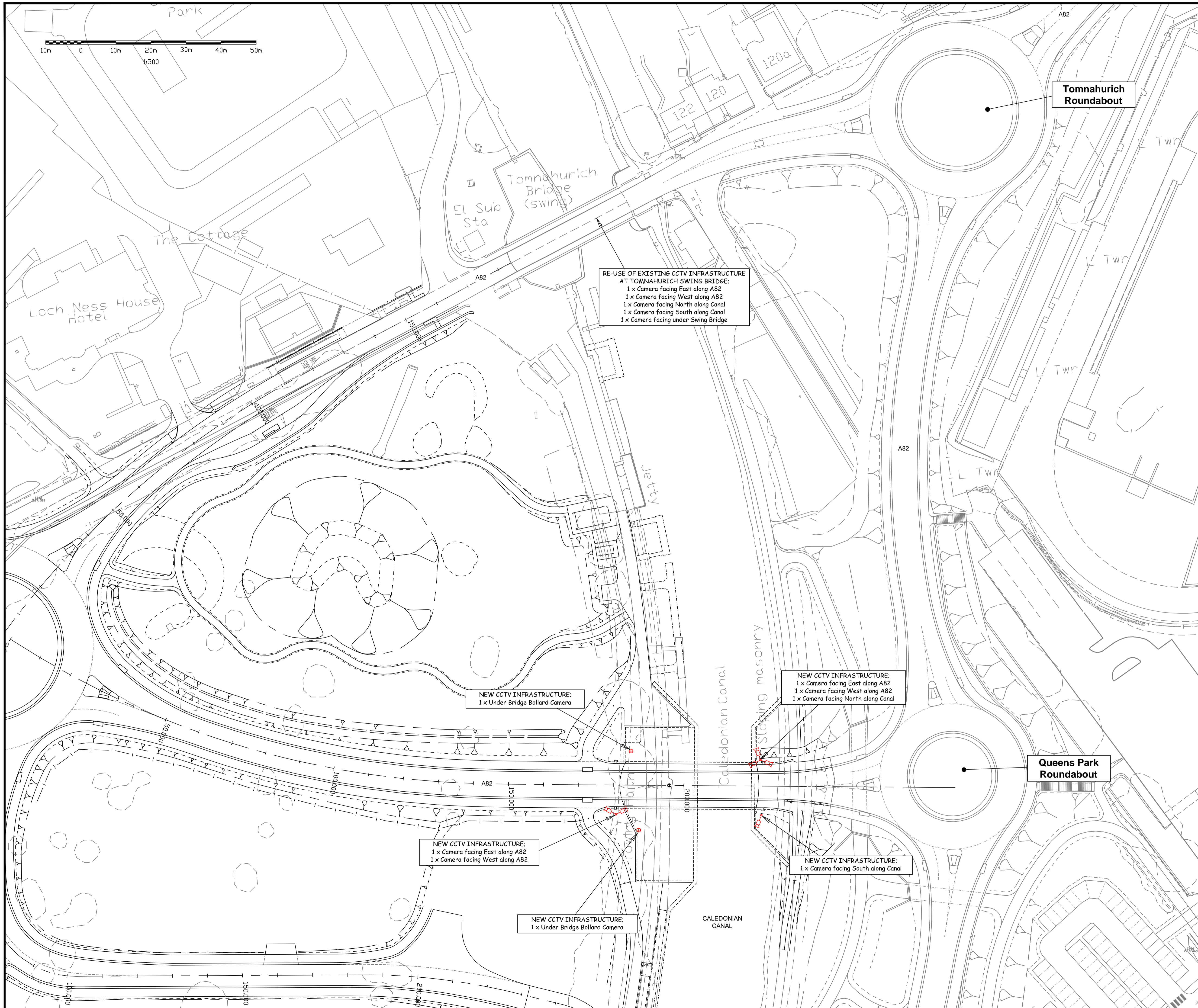
Title
**Underpass 3
Torvean**

Scale (at A1)
As Shown

Drawn	RA	Date	04/09/2013
Checked	TB	Date	04/09/2013

Project No:
HR57126

Drawing No:	PL27	Rev:	2
-------------	------	------	---



COPYRIGHT, DESIGNS and PATENTS ACT 1988:
 This drawing may not be reproduced, copied or used in any way without the written permission of the Director of Transport, Environmental & Community Services.
 © THE HIGHLAND COUNCIL

Notes
 Reproduced from the Ordnance Survey mapping with the permission of the Controller of Her Majesty's Stationary Office © Crown Copyright. Unauthorised reproduction infringes Crown copyright and may lead to prosecution or civil proceedings.
 The Highland Council 2013 100023369

SCALE BAR ADDED	CB	19.07.2018	r2
REVISED FOR STAGE 2 (EXISTING CCTV ON TOMNAHURICH SWING BRIDGE TO BE RE-USED)	CB	11.06.2018	r1
Revision Details	By	Date	Suf'x



DEVELOPMENT & INFRASTRUCTURE

PROJECT DESIGN UNIT
 Osprey House
 Alness Point Business Park
 Alness, IV17 0UP
 Phone : 01349 886741
 Fax : 01349 886749
 E-Mail : bryan.stout@highland.gov.uk

Project **The Highland Council (Inverness West Link Stage 2 Modifications) Planning Application**

Title **Proposed CCTV General Arrangements**

Scale (at A1) **1: 500**

Drawn	MJP	Date	AUG 2013
Checked	PB	Date	AUG 2013

Project No: **HRS7126**

Drawing No: **PL29** Rev: **2**