

Agenda Item	<b>6.11</b>
Report No	<b>PLS/099/19</b>

## HIGHLAND COUNCIL

**Committee:** South Planning Applications Committee

**Date:** 4 December 2019

**Report Title:** 19/03933/FUL and 19/03991/FUL: Vastint Hospitality B.V.  
Former Swimming Pool Site, Glebe Street, Inverness

**Report By:** Area Planning Manager – South

### Purpose/Executive Summary

**Description:** Erection of hotel development with associated landscaping, car parking and ancillary uses

**Ward:** 14 - Inverness Central

**Development category:** Major

**Reason referred to Committee:** Major Development

All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

### Recommendation

Members are asked to agree the recommendation to **Grant** planning permission as set out in section 11 of the report.

## 1. PROPOSED DEVELOPMENT

- 1.1 The application relates to the construction and operation of a 175 bed 4 star rated hotel with associated landscaping, car parking and ancillary uses; including restaurant/café/lounge bar, meeting space and gym. The intended operator is the Marriot AC brand. The applicant has secured additional land, at the north-east corner of the site, for use of car parking and landscaping associated with the main site. This has been submitted as a separate application but has been considered in conjunction with the hotel development as a whole. In addition, the applicant is now believed to have control of the adjacent warehouse property that while not forming part of this proposal could provide options for further leisure and function use associated with the hotel in the future.
- 1.2 The proposed building is in two parts; the main block of a general rectilinear form developed on a L-plan footprint facing the river and A82(T), with a secondary separate block on Glebe Street. The west (river) elevation steps from 3 to 4 then five storeys presenting as a cantilevered gable at the north-west corner adjacent to Friar's Bridge. On the north elevation the building runs parallel to and set back from Friars' Bridge, with the building continuing at five storeys at the return for most of this elevation before dropping to three storeys. On the riverfront the building is set back from the flood wall, to allow for access and maintenance. The Glebe Street block is a three storey, rectangular plan form that contains rooms only.
- 1.3 The main entrance to the hotel, both vehicular and pedestrian, will be from Glebe Street with the front door directly opposite the end of Friar's Street. Vehicular access, for servicing and guest parking, is to be taken from the existing access to the site. There will be a total of 41 car parking spaces. The ground immediately to the west, along the river, and south, along Glebe Street, will be landscaped to form both public and outdoor guest space as part of proposed public realm improvements.
- 1.4 A key project principle of this development is use of the latest cross laminated timber (CLT) technology. The buildings are of modular lightweight construction which means that they can be assembled relatively quickly and therefore cost effectively while having a lighter environmental footprint; reduction of construction waste and better thermal efficiency than more traditional construction.
- 1.5 External cladding materials proposed are a combination of large format reconstructed stone panels with expanses of glazing on ground floor elevations and large format ceramic panels on the elevations above. Windows are aluminium framed within classically spaced and proportioned openings. A variety of window styles will be used with some windows projecting from the building set within aluminium surround features, some recessed and others contained within aluminium clad wall head dormers. Larger glazed areas are incorporated within the 'gable' onto the river. Elevations are terminated by a parapet with pitched roofs finished in standing seam set behind.
- 1.6 The rooftop plant will be hidden behind louvered/mesh screens. Bin stores are located along the north side of building adjacent to Friars' bridge. A new substation is shown to the eastern edge of the carpark. Surface water drainage from the development will be discharged, without attenuation, directly to the River Ness via

an existing outfall. Car park construction will incorporate SuDS principles and have a level of treatment prior to discharge. Foul drainage will connect to the existing public sewer.

1.7 Pre Application Consultation: The applicant held a public consultation event on 29 May 2019 at the Royal Highland Hotel. No formal pre-application advice was sought from the Service.

1.8 Inverness Design Review Panel: The applicant presented its initial proposal for the site to the Design Review Panel on 08 August 2018. The Panel's report is contained within Appendix 2. The Executive Summary states:

*The Panel welcomes the opportunity to comment further on a revised design for this prominent riverfront site, whose development has significant potential to shape the future of the city centre.*

*There is recognition of how far the design has evolved in order to address some of the key issues with the previous scheme. It is considered that the scale and massing is appropriate to the site. Breaking down the mass and creating distinct building forms is welcome and subject to achieving appropriate junction detailing will contribute to the successful modelling of the elevations. The use of a pitch roof form and wall head dormers will assist in reflecting some of the variety/distinctiveness of the surroundings. There remains a question over how the building relates to the river and this could be viewed as a limitation of the use of CLT/modular construction. There are differing views on this and how the development will be able to respond sensitively to the site's historic riverside setting in this regard. The introduction of the building on Glebe Street is a welcome addition.*

*The re-emphasis on connecting ground floor accommodation and activity to the public realm is welcomed and encouraged, along with proposals for tree planting and landscaping.*

1.9 Supporting Information:

- Design and Access Statement
- Transport Statement
- Travel Plan
- Drainage Impact Assessment
- Flood Risk Assessment
- Ground Conditions Report
- Visualisations
- Pre-application Consultation Report

1.10 Variations:

1. Scheme design amended 29.10.2019. Key changes

- Refinement to the elevations at entrances

2. Scheme design amended 12.11.2019. Key changes:

- Increase in footway width on Glebe Street

## 2. SITE DESCRIPTION

- 2.1 The site comprises a relatively level triangular shaped parcel of vacant land that extends from the Shore St roundabout junction at Friar's Bridge towards the River Ness. The site was the location of the former Inverness public baths but has lain vacant for some years.
- 2.2 Friar's Bridge defines the northern boundary of the site. The existing cycle/footway along the River Ness forms the west boundary and Glebe Street its south-east. An existing retail and commercial development directly abuts the development site at its eastern most edge. This has an existing vehicular access onto Glebe Street for delivery of goods and fire escape. Uses within the immediate area are predominantly residential with housing located to the south on Glebe Street, Friar's Street and Douglas Row. This is reflected in the scale of building that is between two and three storeys in height.
- 2.3 The site is located within the Inverness (Riverside) Conservation Area. A number of listed buildings lie within the vicinity; most notably the Category B group of properties on Douglas Row.

## 3. PLANNING HISTORY

- |     |            |  |                             |
|-----|------------|--|-----------------------------|
| 3.1 | 21.01.2010 | Hotel development (08/00353/FULIN)   | Planning Permission Granted |
| 3.2 | 20.01.2014 | 5 storey hotel development with restaurant, bar facilities with associated car parking and access (13/03235/FUL) | Withdrawn                   |
| 3.3 | 11.10.2016 | Erection of 60 residential units along with open space, parking and associated infrastructure (15/02556/FUL)     | Planning Permission Granted |
| 3.4 | 24.09.2018 | Erection of hotel with associated landscaping, car parking and ancillary uses (18/01248/FUL)                     | Planning Permission Refused |
| 3.5 | 01.04.2019 | Erection of hotel with associated landscaping, car parking and ancillary uses (PPA-270-2204)                     | Appeal Dismissed            |

## 4. PUBLIC PARTICIPATION

- 4.1 Advertised: Section 65: Affecting the Setting of Conservation Area  
Section 34: Schedule 3
- Date Advertised: 20.09.2019
- Representation deadline: 11.10.2019
- Timeous representations: 4



Late representations: 0

4.2 Material considerations raised are summarised as follows:

- Lacks interest
- East elevation devoid of windows or other articulation
- Need visualisations from Shore Street/Chapel Street and Glebe Street/Friars Street [*subsequently provided*]
- View from Shore Street along the line of the Friar's Bridge looks particularly stark and the repetitive uniformity of the fenestration is considered unattractive.
- 'Looks like a prison'
- Does not reflect Highland vernacular
- Superficial reference to the existing built fabric of the area
- Steps down crudely
- Lacks articulation to break up mass
- Pitched gable and dormers add to incongruous nature
- Detract from the riverfront and views of the river
- Concern over increased use of Friar's Street and Douglas Row - implication for access and egress (lack of existing enforcement)
- Impact on parking in the vicinity - specifically Friar's Street and the courtyard parking in the street

4.3 Inverness Civic Trust make particular reference to Inverness City Centre Development Brief in which it states that:

*The proposals are considered to fall short of meeting the aspirations and requirements of the Inverness City Centre Development Brief in the following respects:*

*Page 5, Sect. 2.3:*

*'Distinctive and Attractive: Inverness takes pride in being a high quality city centre that celebrates and safeguards its unique combination of natural, cultural and built heritage, in particular its historic river setting.'*

*Page 6, Sect 2.4*

*'Our approach to development is to create and promote a vibrant, high density mix of uses that ...delivers high quality development, including an attractive public realm.'*

*Page 32, Sect 7.8*

*'The priorities for this Brief are to....enable innovative, high quality, contemporary design through the application of placemaking principles to all new development.'*

*In summary, the Trust considers that the current proposals, if approved, would risk becoming the Bridge Street of the future and saddle Inverness (in the eyes of its citizens) with another monstrosity. A better solution is needed.*

4.4 All letters of representation are available for inspection via the Council's eplanning portal which can be accessed through the internet [www.wam.highland.gov.uk/wam](http://www.wam.highland.gov.uk/wam).

## 5. CONSULTATIONS

- 5.1 **Crown and City Centre Community Council** *has considered various proposals for this site over recent years and there has always been tension between the desire to see the site developed against the scale and impact of the built form proposed. The current proposals have evolved in consultation with the planning authority as well as the Design Panel in a due process to reconcile the scale and design of building in the context of Friars Bridge and the residential properties on Douglas Row and Glebe St. We are assuming from the submissions made that these proposals have now reached a stage which can be supported.*

*There are no adverse comments from members of the CC to these revised proposals.*

*We now hope that the process can be concluded and that the site can now proceed to development.*

- 5.2 **Transport Scotland:** No objection subject to conditions relating to the need for Travel Plan, details of external lighting, landscaping along the trunk road, foundation structures and drainage connections.

- 5.3 **SEPA** advise that the site is located on land protected from flooding by the River Ness Flood Alleviation Scheme. It notes that while the design standard was 1 in 100 years plus allowance for climate change and an additional allowance for freeboard since being built it has been demonstrated that the scheme actually exceeds 1 in 200 years including a 600mm allowance for freeboard. SEPA notes the conclusion of the Flood Risk Assessment submitted in support of the application that has demonstrated that the minimum standard 1 in 200 year protection for the site can be achieved. It also notes consideration of climate change.

SEPA has no objection to the proposal on flood risk grounds providing that the existing and proposed site section drawing (KEP-XX-XX-DR-A-5020-0003) is one of the approved plans which show the finished floor levels set at 4.69m AOD.

- 5.4 **Contaminated Land Team** indicate that the site/part of the site has historic uses that may have resulted in land contamination, with some investigation work having been carried out but not yet completed. No objections subject to conditions.

- 5.5 **Historic Environment Team (Archaeology)** advises that proposal lies within an area of archaeological interest and that while construction and demolition of the former swimming pool may have removed some evidence for earlier occupation at the site there remains the potential for buried archaeological features and deposits to survive. Having said that, the risk of surviving buried features is not considered such that full excavation is required but the advice provided is that it is important that the nature and extent of any features are identified and recorded before they are impacted by the development. Site clearance work should therefore be carried out under archaeological supervision – secured through condition.

A number of historic gravestones are currently installed into the existing brick wall of the current carpark that backs onto Friars Bridge. It is not clear at this stage whether they will be impacted. Advice is that additional detail on this element of the application is required, including a method statement setting out how the stones will be extracted.

- 5.6 **Historic Environment Team (Conservation):** No issues in conservation terms. The colour/finish will however need to be confirmed/conditioned for approval.
- 5.7 **Transport Planning Team** advise that, notwithstanding errors in the Transport Statement and lack of clarity on trip generation, there is no objection to the application subject to a number of conditions.

It recommends the following matters need to be addressed through condition before any works begin on site:

- The details of signing at the exit from the hotel to discourage right turning traffic towards Douglas Row.
- A signing scheme to promote access to the site via Friars Lane and Friars Street and discourage right turning traffic into Glebe Street from Chapel Street.
- Details of staff cycle parking for a minimum of 10 bicycles which must be covered and secure to protect bicycles from the elements.
- A swept path analysis for an industry standard refuse vehicle (10.5m long) to ensure that service vehicles can access and egress the site in forward gear.
- Details of the bin storage area to ensure it is sized to meet the guidelines as set out in the Council document 'Managing Waste in New Developments' within an acceptable carry distance of the refuse vehicle.
- A suitably designed scheme to improve pedestrian connectivity at the junction of Chapel Street/Glebe Street to be implemented before occupation of the development since the proposals set out on dwg no.123174/1002 Rev B with the Transport Statement are considered indicative only.
- Details to ensure operations at the adjacent warehouse/foodbank are not prejudiced by the development or during its construction.

The Transport Planning Team ask that the commitment set out to have a Travel Plan would need to be conditioned to explicitly require monitoring results to be submitted to the Council for a minimum of 5 years.

In addition the Transport Planning Team comment that there is a shortfall of 83 spaces for this development and that this needs to be mitigated, as set out within the City Centre Development Brief, through upfront developer contributions of (83 x £3490) £289,670.00 for the purpose of improving pedestrian and cycle routes to the hotel from the train and bus stations.

- 5.8 **Flood Risk Management Team** requests that the finished floor levels are no lower than 4.69m AOD. It is content with the general principle of the proposed drainage, which will utilise an existing outfall to the river. However, it requests, by condition, further details in respect of run-off rates.

In addition a condition to ensure that a minimum buffer strip of 3m from the flood defence wall kept free from development to allow for future maintenance of the flood defence wall is requested. It advises that storage of materials within this area during construction is not to be permitted.

- 5.9 **Access Officer** comments that the redline boundary of the proposed development coincides with Core Path IN19.01 Riverside Path. It is recommended that both the route remain open and free from obstruction or encroachment before and during construction. It is suggested that if necessary this could be achieved by condition. In addition, a suggestion is that should the path become damaged or disturbed that this be rectified within 14 days.

- 5.10 **Environmental Health** advises that the impact of construction noise will be controlled under Section 60 of the Control of Pollution Act 1974 and that as no construction noise assessment has been submitted there is an assumption that construction activity will be between 8am to 7pm Monday to Friday and 8am to 1pm Saturday only.

Subject to conditions regarding noise in relation to heating, ventilation and refrigeration plant and bin storage Environmental Health has no objection.

## 6. **DEVELOPMENT PLAN POLICY**

The following policies are relevant to the assessment of the application

### 6.1 **Highland Wide Local Development Plan 2012**

- 28 - Sustainable Design
- 29 - Design Quality and Place-making
- 30 - Physical Constraints
- 31 - Developer Contributions
- 34 - Settlement Development Areas
- 42 - Previously Used Land
- 43 - Tourism
- 44 - Tourist Accommodation
- 51 - Trees and Development
- 56 - Travel
- 57 - Natural, Built and Cultural Heritage
- 58 - Protected Species
- 59 - Other important Species
- 60 - Other Importance Habitats
- 61 - Landscape
- 63 - Water Environment
- 64 - Flood Risk
- 65 - Waste Water Treatment
- 66 - Surface Water Drainage

70 - Waste Management Facilities  
72 - Pollution  
73 - Air Quality  
74 - Green Networks  
75 - Open Space  
77 - Public Access

**6.2 Inverness Local Plan 2006 (as continued in force 2012)**

N/A

**6.3 Inner Moray Firth Local Development Plan 2015**

Policy 1 - Promoting and Protecting City and Town Centres

Policy 2 - Delivering Development

IN10: Site at Glebe Street

**6.4 Highland Council Supplementary Planning Policy Guidance**

Highland Historic Environment Strategy (Jan 2013)

Inverness City Centre Development Brief (February 2018)

Developer Contributions (March 2013)

Flood Risk and Drainage Impact Assessment (Jan 2013)

Green Networks (Jan 2013)

Highland's Statutorily Protected Species (March 2013)

Construction Environmental Management Process for Large Scale Projects  
(August 2010)

Managing Waste in New Developments (March 2013)

Physical Constraints (March 2013)

Public Art Strategy (March 2013)

Standards for Archaeological Work (March 2012)

Sustainable Design Guide (Jan 2013)

**7. OTHER MATERIAL POLICY CONSIDERATIONS**

7.1 Inverness City Conservation Area Character Appraisal (2004)

7.2 Inverness Conservation Area Management Plan (2015)

7.3 Scottish Government Planning Policy and Guidance

- Scottish Planning Policy (SPP)
- Designing Streets
- Creating Places
- PAN 61 - Sustainable Drainage Systems
- PAN 68 - Design Statements
- PAN 75 - Planning for Transport
- PAN 77 - Designing for Safer Places
- PAN 1/2011 Planning and Noise

## **8. PLANNING APPRAISAL**

- 8.1 Section 25 of the Town and Country Planning (Scotland) Act 1997 requires planning applications to be determined in accordance with the development plan unless material considerations indicate otherwise.
- 8.2 Under Section 59 of the Planning (Listed Buildings and Conservation Areas) (Scotland) Act 1997, in considering whether to grant planning permission for development which affects a listed building or its setting, the Planning Authority must have regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.
- 8.3 Under Section 64 of the Planning (Listed Buildings and Conservation Areas) (Scotland) Act 1997, it is the duty of the Planning Authority to ensure that, within conservation areas, development proposals preserve or enhance the character and appearance of a conservation area.

### **Determining Issues**

- 8.4 This means that the application requires to be assessed against all policies of the Development Plan relevant to the application, all national and local policy guidance and all other material considerations relevant to the application.

### **Planning Considerations**

- 8.5 The key considerations in this case are:
- a) compliance with the development plan and other planning policy
  - b) design quality, the Inverness Conservation Area and impact on the setting of adjacent listed buildings
  - c) amenity of neighbouring residents
  - d) flood risk and drainage (construction and operation)
  - e) contaminated land
  - f) parking and access (incl. public rights of way)
  - g) archaeology
  - h) any other material considerations.

### **Development plan/other planning policy**

- 8.6 Both the Highland wide Local Development Plan (2012) and Inner Moray Firth Local Plan (2015) support proposals that will maintain and/or strengthen the vitality and viability of the City Centre. The former supports use of previously used land. It also supports tourist accommodation where it can be located without adverse impacts on neighbouring uses. The site is identified within the Development Plan for, amongst other uses, a hotel. It is considered that such a use will strengthen the vitality and viability of the City Centre and in principle would be acceptable.

- 8.7 The site is not specifically referenced within the current Inverness City Development Brief (January 2018) and does not form one of the sites with detailed development guidance. However, the design is expected to take into account the placemaking principles set out within the guidance including; requirement for contextual analysis, protecting key views, safeguarding setting of built heritage, height, scale, massing, provision of active frontages, permeability, high quality materials, public realm and open space to name a few.
- 8.8 Subject to the proposal demonstrating a high quality design and having no significant adverse impact on the local character and historic and natural environment, on individual and community residential amenity and existing infrastructure then the proposals would comply with the development plan.

### **Design Quality, the Inverness (Riverside) Conservation Area and setting of listed buildings**

- 8.9 A key characteristic of the Inverness (Riverside) Conservation Area is the relationship of the main streets, which run parallel to the river, the connecting streets, which lead towards the river, and the significant buildings and views along them that combine to provide the City with its unique identity and sense of place. The value of the City's townscape is evident in views into the Old Town, especially from the west bank of the river. The river is central to the identity of the city and to its visual character. Development is expected to take this into account and protect views to and from the river, river bank and/or bridges. The Castle, the Town House, the Town Steeple and the various church spires are key buildings on the Inverness skyline. It is important that new buildings do not detract from their distinctiveness.
- 8.10 The Inverness Old Town Conservation Area Management Plan, approved by the City of Inverness Area Committee in June 2015 and therefore material to the determination of this application, sets a framework for the management of change in the built environment based on a detailed understanding of the character of this important part of the Inverness (Riverside) Conservation Area. It identifies key issues for its sensitive management as well as principles and guidelines that will be applied to future decision making.
- 8.11 It recommends that development guidance is prepared for the Glebe Street site to address matters of scale, height, massing, the context of the prominent site on the riverside, key views, materials and site permeability. Specific development guidance for this site was contained within the previous version of the Inverness City Centre Development Brief, used in previous decision making on this site, but is not contained within the most recent City Centre Development Brief, which instead expects development to be appraised against a number of placemaking principles.
- 8.12 The Inverness Old Town Conservation Area Management Plan states that proposals for Glebe Street *'should make a distinctive and positive contribution at the north of the conservation area, while respecting the hierarchy of key buildings/structures in the Inverness city centre. Large masses of uniform height (as built during the later 20th century in the vicinity of Bridge Street) should be avoided. Some vertical features or emphasis could contribute to the townscape; however, the small scale of the riverside buildings in Douglas Row and on the opposite bank of the Ness at*

*Huntly Street and Friars' Place should be respected. A proposal on this site should have a strength of presence, of the highest design quality and should enhance the relationship of the site to the river, including fully recognising the importance of the riverside pedestrian route leading to Riverside Street, north of Friars' Bridge.'* This statement remains relevant to the consideration of the proposal against the placemaking principles contained within the Inverness City Centre Development Brief.

- 8.13 In summary, what is expected is a building that does not detract from the existing key qualities of the riverfront, that respects the key views and the lower scale buildings adjacent to and on the opposite side of the river, will not be a single large mass of uniform height having an elevational treatment that relates well to the pattern of the surrounding historic fabric and one that utilises high quality materials. Not only however should it relate well to the river frontage but provide an opportunity to create a new street frontage on Glebe Street.
- 8.14 The development as now presented is significantly different from that which was previously refused by the Council and then subsequently dismissed on appeal. In redesigning the scheme, the applicant has taken into account the criticism set out in the Reporters decision, the Council's position and to an extent the comments of third parties on the previous scheme. The building, while still providing the required number of rooms and ensuring that the CLT approach to construction remains possible has addressed the issue of mass through a combination of more pronounced stepping in height, use of roofspace for accommodation, and small set-backs within the elevations to create visual breaks and a sense that the building is made up from several smaller constructions albeit unified by the ground floor and use of materials.
- 8.15 The L-plan form of the principal hotel building has essentially reverted to an earlier iteration placing the building back along Friar's Bridge. This results in a less dominant form on Glebe Street, providing an opportunity for a lower, more domestic scale, annex to be located here instead. A perimeter street block is the result with the hotel entrance framed in views along Friar's Street. While it would have been preferable for the Glebe Street block to have more of an active frontage, from an operational perspective, its entrance needs to address the hotel rather than the street. Overall the proposals considerably improve the streetscape and public realm.
- 8.16 The monolithic form and mass that was a particular criticism of the previous proposal has been addressed through further refinement of building heights, stepping and elevational treatment. While comments made to this application consider the repetitive uniformity of windows on the elevations undesirable, this is a feature of historic buildings within Inverness, which tend to have a strong horizontal emphasis. The inclusion of pitched roofs is a welcome addition and making use of roofspace with wallhead dormer windows assist in reducing the perceived scale of the building and improves its relationship with the properties on Douglas Row in particular.
- 8.17 The positioning of the building has also improved its relationship with Douglas Row. Those parts with greatest height will be located further from Douglas Row. The river frontage is also set further back within the site. The Design Review Panel had



differing views on the success of this latter aspect, considering that it may have been better to front the river more directly but equally it was acknowledged that the chosen approach would mean that the development would not compete as much with Douglas Row in downstream river views. This set back from Douglas Row and the stepped arrangement to the riverfront means that the massing does not adversely impact on the setting of adjacent listed buildings, nor does it compete with other large structures along the riverfront.

- 8.18 Existing key views to Ben Wyvis from the river and Castle Hill will be uninterrupted. The view from the Ness Bridge north towards the Black Isle will be interrupted but the building will not skyline in this view. It is considered that a building of scale is required in this location given its location adjacent to the A82/Friar's Bridge and the way in which this current proposal has approached design has taken a more sensitive approach to the scale of surrounding buildings and the architectural qualities of the conservation area as a whole.
- 8.19 While officers raised initial reservations over the use of modern ceramic materials for the cladding, the proposed product is of high quality and given that the panels will be large format will work well with the scale of the building. The ceramic panel will be used for all elevations. The materials are appropriate to this contemporary design.
- 8.20 Representations made against the proposal by members of the public consider that the proposed design approach is not appropriate for the site. On the other hand the Design Review Panel is more supportive. The planning history of the site is also relevant, with planning permission already granted for substantial contemporary buildings in this space. It is recognised that design is largely a subjective matter. However, having assessed the proposal against the placemaking principles set out within the Inverness City Centre Development Brief (Appendix 3), and taking into account the planning history, it is considered that the proposals would comply with the Brief, subject to achieving high quality landscaping and public realm improvements, including public art, which are matters that can be secured through condition and/or contributions. While introducing change, it is not considered that the proposed development will have a significant adverse impact upon the character or appearance of the conservation area overall or the setting of adjacent listed buildings.

### **Residential amenity**

- 8.21 The proximity of the building, particularly on Glebe Street, to residential properties opposite does not raise any particular concern for the potential for neighbouring residents to be affected by a loss of daylight and sunlight. The distance of the Glebe Street block is sufficient so as not to have any adverse impact on privacy and/or overlooking.
- 8.22 The development provides an opportunity to enhance the streetscape and public realm.

- 8.23 The development will introduce change to the area and with that will be an increase in noise and activity. Environmental Health has no objection to the proposals subject to conditions. The increase in activity through increased footfall may not be welcomed by all but does provide the opportunity for this area to contribute more to the vitality of the City and over time perhaps benefit from increased investment.
- 8.24 The impact of construction on residents needs careful consideration and it is suggested that some form of community consultation by the developer is necessary to ensure that residents are not unduly inconvenienced. This can be sought by condition. The issue of working hours and construction generated noise is a matter for Environmental Health.

### **Flood risk and drainage**

- 8.25 The site lies behind the recently finished River Ness Flood Scheme; above pavement level behind a secondary flood wall. While this has, disappointingly, prevented the development from having direct frontage access to the river itself, the site is protected from flooding. On the basis that finished floor levels are set at 4.69m AOD, which is above the existing ground level, SEPA and the Flood Risk Management Team have no objection to the proposals. The Flood Risk Management Team has asked that an area at least 3m back from the flood wall remain clear at all times to allow for maintenance. These matters can be controlled by condition.
- 8.26 Surface water drainage from the building will be discharged, without attenuation, directly to the River Ness via an existing outfall. The car park will have a level of treatment prior to discharge to the same outfall. The Flood Risk Management Team was concerned that this outfall did not exist but the applicant has confirmed this to be the case and consequently the Flood Risk Management Team is now content that the drainage solution is appropriate subject to clarification on flow rates. This can be confirmed prior to the commencement of development.
- 8.27 Foul drainage will connect to the existing public sewer.

### **Contaminated land**

- 8.28 The Council's Contaminated Land Team has highlighted that the previous use of the site may have resulted in land contamination. While some investigation work has already been carried out this work is yet to be completed. Subject to a condition requiring this matter to be adequately dealt with the Contaminated Land Team has no objection.

### **Parking and access**

- 8.29 Access to the site is proposed to be taken from Academy Street via Friar's Street, rather than encourage a right turn onto Glebe Street from Chapel Street. Drop-off provision will be on-street. This will be separate to and located further east than the existing on-street residents parking bay.

- 8.30 This approach should mean that traffic will be naturally dissuaded from continuing along Glebe Street onto Douglas Row, which has restricted access, since vehicles will be facing the opposite direction. Transport Planning is content with this arrangement, believing it will work well subject to an appropriate scheme of mitigation to include an operational traffic management plan, a scheme for advance directional signage and a scheme to improve pedestrian connectivity at the junction of Chapel Street/Glebe Street.
- 8.31 There are 41 parking spaces. This is a shortfall of 83 spaces. There will therefore be a requirement for these to be compensated through payment of developer contributions towards active travel improvements in accordance with the requirements of the City Centre Development Brief.
- 8.32 Transport Planning request further confirmation on how the adjacent properties will be accessed and serviced throughout construction. This was an issue that was raised by the previous owner of the warehouse property to the east. This property, as well as the portion of the site covered by planning application 19/03991/FUL, is now in the ownership of the applicant. A short term lease on the warehouse remains in place however and it therefore also needs to be considered as part of the operational traffic management plan and construction traffic management plan.
- 8.33 Transport Scotland has no objection subject to a requirement for a Travel Plan and conditions to protect its own infrastructure.
- 8.34 The Council's Access Officer has identified the need to maintain access on the core path that runs along the riverfront and onto Friar's Bridge. This is a matter that could be controlled by condition.

### **Archaeology**

- 8.35 There is potential for the historic gravestones along the existing brick wall of Friar's Bridge to be impacted by the development, particularly through construction. Details for their protection will be required can be secured by condition as can potential for uncovering buried archaeological features.

### **Other material considerations**

- 8.36 The site is situated adjacent to the River Ness which itself has connectivity to the River Moriston Special Area of Conservation (SAC); the qualifying interests of which are Atlantic salmon and freshwater pearl mussel. While there is potential for construction activity to affect the qualifying interests the risk would be low and any activity likely to affect the water environment would be controlled by SEPA in any event. The development is unlikely to have any significant effect on the SAC.
- 8.37 There are no specific proposals for public art as part of this application although the City Centre Development Brief does consider that there is potential to include public art within planned public realm improvements. The application, as revised, does include considerable space that will contribute to public realm and there is potential for public art to be included with this. It is anticipated that these matters can be addressed by condition.

## **Non-material considerations**

8.38 None.

## **Matters to be secured by Section 75 Agreement**

- 8.39
1. Contributions towards active travel within the City in accordance with the City Centre Development Brief equating to £3,490 per space.
  2. Contribution of £8,300 towards the Wayfinding Strategy for Inverness City Centre.
- 8.40 The applicant has four months from the date that the Council's solicitor writes to the Applicant/Applicant's solicitor indicating the terms of the legal agreement, to deliver to the Council a signed legal agreement. Should an agreement not be delivered within four months, the application shall be refused under delegated powers.

## **9. CONCLUSION**

- 9.1 The principle of hotel development is supported by the Development Plan.
- 9.2 The Community Council welcomes the redevelopment of the site and has no adverse comments to these revised proposals. It remains clear however that some have concerns regarding the design of the development with opponents essentially of the belief that this is not the right building for this location, lacking in visual interest with only 'superficial reference' to the historic townscape.
- 9.3 The building does however reflect many of the key features of historic buildings within the City Centre, particularly the classical window proportions and regularity that provides strong horizontal emphasis. The elevational treatment is contemporary and of quality. This is also the case for the materials proposed for the façade and although there were initial reservations over the use of ceramic cladding this will enable a consistent approach to be taken for all elevations and is therefore welcome. The massing of the building has been improved through use of different building heights and introducing vertical breaks in the elevations to create a series of individual buildings connected not only physically but by the uniformity of materials and style.
- 9.4 By doing so it is considered that the building will add visual interest to this edge of the conservation area without impacting significantly on views. The separation from Douglas Row will ensure that the setting of these listed buildings is not adversely impacted and overall as a high quality modern development the proposal will not have an adverse impact on the character of appearance of the conservation area.
- 9.5 The history of the site must be given due consideration. There is an extant permission for residential development that is of contemporary design; the height of which ranges from three to seven stories. A contemporary approach is an entirely appropriate response and one of the key requirements of the Inverness City Centre

Development Brief. It is considered that the development accords with the principles contained within the Brief. The proposal presents an opportunity to considerably improve the public realm of Glebe Street and create an attractive and vibrant riverside development.

- 9.6 There will be no significant adverse impact on community or residential amenity, or existing infrastructure. Indeed, the proposal will improve active travel opportunities within the vicinity. Traffic generated can be appropriately managed to avoid conflict with residential streets and parking.
- 9.7 In summary, the proposal will bring back into use this long standing vacant site within the City, provide a modern quality building that respects the key characteristics of the riverfront of Inverness and will make a positive contribution to the vibrancy of the City.
- 9.8 All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

## **10. IMPLICATIONS**

- 10.1 Resource: Not applicable
- 10.2 Legal: Not applicable
- 10.3 Community (Equality, Poverty and Rural): Not applicable
- 10.4 Climate Change/Carbon Clever: Not applicable
- 10.5 Risk: Not applicable
- 10.6 Gaelic: Not applicable

## **11. RECOMMENDATION**

<b>Action required before decision issued</b>	Y
Notification to Scottish Ministers	N
Conclusion of Section 75 Obligation	Y
Revocation of previous permission	N

**Subject to the above,** it is recommended that

**A.** Planning permission be **GRANTED** for application 19/03933/FUL, subject to the following conditions and reasons:

1. No development or work shall commence until a detailed specification for all proposed external materials and finishes (including trade names and samples where necessary) has been submitted to and approved in writing by the Planning Authority. Thereafter, development and work shall progress in accordance with these approved details.

**Reason:** In the interests of visual amenity.

2. No development shall commence until details of a scheme of hard and soft landscaping works for the site including the area at the south boundary have been submitted to and approved in writing by the Planning Authority. Details of the scheme shall include:
  - i. all earthworks and existing and finished ground levels in relation to an identified fixed datum point;
  - ii. the location and design, including materials, of any existing or proposed walls, fences and gates;
  - iii. all soft landscaping and planting works, including plans and schedules showing the location, species and size of each individual tree and/or shrub and planting densities; and
  - iv. a programme for preparation, completion and subsequent on-going maintenance and protection of all landscaping works.

All landscaping works shall be carried out in accordance with the approved scheme. All planting, seeding or turfing as may be comprised in the approved details shall be carried out in the first planting and seeding seasons following the commencement of development, unless otherwise stated in the approved scheme.

Any trees or plants which within a period of five years from the completion of the development die, for whatever reason are removed or damaged shall be replaced in the next planting season with others of the same size and species.

For the avoidance of doubt, the landscaped area, particularly along Glebe Street, shall include at least one piece of public art and seating for public use.

**Reason:** In order to ensure that the approved landscaping works are properly undertaken on site.

3. Public access to any Core Path within, or adjacent to, the application site shall at no time be obstructed or deterred by construction-related activities, unless otherwise approved in writing by the Council's Access Officer as a temporary measure required for health and safety or operational purposes. Under such circumstances, any temporary obstruction or deterrent shall cover only the smallest area practicable and for the shortest duration

possible, with waymarked diversions provided as necessary. Any disturbance or damage to the route must be repaired to as good or better standard to that at pre-development within 14 days, or such longer period as may be agreed by the Planning Authority, of the event.

**Reason:** In order to safeguard public access during the construction phase of the development.

4. No development shall commence until a scheme to deal with potential contamination within the application site has been submitted to, and agreed in writing by, the Planning Authority. The scheme shall include:
  - i. the nature, extent and type of contamination on site and identification of pollutant linkages and assessment of risk (i.e. a land contamination investigation and risk assessment), the scope and method of which shall be submitted to, and agreed in writing by, the Planning Authority, and undertaken in accordance with Planning Advice Note 33 (2000) and BS10175:2011+A1:2013 Investigation of Potentially Contaminated Sites - Code of Practice;
  - ii. the measures required to treat/remove contamination (remedial strategy) including a method statement, programme of works, and proposed verification plan to ensure that the site is fit for the uses proposed;
  - iii. measures to deal with contamination during construction works;
  - iv. in the event that remedial action is required, a validation report that will validate and verify the completion of the agreed decontamination measures;
  - v. in the event that monitoring is required, monitoring statements shall be submitted to at agreed intervals for such time as is considered appropriate by the Planning Authority.

No development shall commence until written confirmation has been received that the scheme has been implemented, completed and, if required, monitoring measurements are in place, all to the satisfaction of the Planning Authority.

**Reason:** In order to ensure that the site is suitable for redevelopment given the nature of previous uses/processes on the site.

5. For the avoidance of doubt, the ground floor finished floor level of the development hereby granted planning permission shall be no less than 4.69 metres AOD.

**Reason:** In the interests of flood prevention.

6. An area of a minimum buffer width of 3 metres shall be provided between the footprint of development and the existing flood prevention wall and retained in perpetuity in order to provide access for maintenance and repair of the wall, all to the satisfaction of the Planning Authority. No development shall commence until details of the buffer strip has been submitted to, and agreed in writing by, the Planning Authority.

**Reason:** In order to retain access for repair and maintenance of the flood wall.

7. No development or work (including site clearance) shall commence until proposals for an archaeological watching brief to be carried out during site clearance and excavation works, has been submitted to, and approved in writing by, the Planning Authority. Thereafter, the watching brief shall be implemented as approved.

**Reason:** In order to protect the archaeological and historic interest of the site.

8. No work shall commence until a scheme for the protection of the historic gravestones along the north/east boundary of the site from construction activities shall be submitted to, and agreed in writing by, the Planning Authority.

**Reason:** In order to protect the archaeological and historic interest of the site.

9. No development shall commence until full details of the drainage design, including pre and post development runoff rates, are submitted to, and agreed in writing by, the Planning Authority. For the avoidance of doubt there should be no increase in the rate and volume of runoff into the River Ness from the pre-development scenario. Sensitivity testing of the network should include a submerged outfall based upon 1 in 200 year tidal water levels (including climate change) in the River Ness.

**Reason:** In order to ensure that the drainage design is appropriate.

10. No development shall commence until a Construction Environmental Management Document (CEMD), in accordance with The Highland Council's Guidance Note on Construction Environmental Management Process for Large Scale Projects (August 2010) (as amended, revoked or re-enacted; with or without modification), has been submitted to, and approved in writing by, the Planning Authority. The CEMD shall be submitted at least two months prior to the intended start date on site and shall include the following:

- i. Change control procedures to manage/action changes from the approved CEMD and Construction Environmental Management Plans;



- ii. Construction Environmental Management Plans (CEMPs) for the construction phase, covering:
  - a. Pre-commencement species surveys;
  - b. Pollution Prevention and Control;
  - c. Dust Management;
  - d. Construction Noise Assessment and Mitigation Plan in accordance with BS5228 Code of practice for noise and vibration control on construction and open sites - Part 1: Noise;
  - e. Construction Vibration Assessment and Mitigation Plan in accordance with BS5228 Code of practice for noise and vibration control on construction and open sites - Part 1: Noise;
  - f. Site Waste Management;
  - g. Surface and Ground Water Management i.e.;
    - i. Drainage and sediment management measures from all construction areas; and
    - ii. Mechanisms to ensure that construction will not take place during periods of high flow or high rainfall.
  - h. Emergency Response Plans; and
  - i. Other relevant environmental management as may be relevant to the development.
- iii. A statement of responsibility to 'stop the job/activity' if a breach or potential breach of mitigation or legislation occurs; and
- iv. Methods for monitoring, auditing, reporting and the communication of environmental management on site and with client, Planning Authority and other relevant parties.

Thereafter, development shall be carried out in accordance with the approved Schedule of Mitigation, Construction Environmental Management Document and any Construction Environmental Management Plans approved thereunder.

**Reason:** In the interests of the protection of the environment and amenity of residents during construction.

- 11. No development shall commence until a Construction Traffic Management Plan (CTMP) has been submitted to, and approved by, the Planning Authority in consultation with the relevant Roads Authority(s). The CTMP, which shall be implemented as approved during all period of construction, must include:
  - i. A description of all measures to be implemented by the developer in order to manage traffic during the construction phase (incl. routing strategies), with any additional or temporary signage and traffic control undertaken by a recognised suitably qualified traffic management consultant;

- ii. The identification and delivery of all upgrades to the public road network to ensure that it is to a standard capable of accommodating construction related traffic and the operational requirements of the development to the satisfaction of The Highland Council;
- iii. A procedure for the regular monitoring of road conditions and the implementation of any remedial works required during construction periods.
- iv. Details of any upgrading works required at the junction of the site access and the public road. Such works may include suitable drainage measures, improved geometry and construction, measures to protect the public road and the provision and maintenance of appropriate visibility splays.
- vi. Details of appropriate traffic management which shall be established and maintained at the site access for the duration of the construction period. Full details shall be submitted for the prior approval of The Highland Council, as Roads Authority.
- vii. Wheel washing measures to ensure water and debris are prevented from discharging from the site onto the public road;
- viii. Appropriate reinstatement works shall be carried out, as required by Highland Council, at the end of the construction of the development.
- ix. Measures to ensure that construction traffic adheres to agreed routes.

Thereafter the approved Construction Traffic Management Plan shall be implemented in full, unless otherwise approved in writing by the Planning Authority.

**Reason:** In the interest of road safety during construction.

- 12. No development shall commence on site until a scheme for the inclusion of public art within the development, including types and locations of artworks and the management and maintenance thereof, has been submitted to, and approved in writing by, the Planning Authority. The approved scheme shall be implemented prior to occupation of the development and maintained in perpetuity.

**Reason:** To ensure the delivery of a development with a unique identity that facilitates the creation of place.

- 13. The design and installation of any plant, machinery or equipment shall be such that any associated noise complies with NR20 when measured and/or calculated within any nearby living apartment, and no structure borne vibration is perceptible within any nearby living apartment.

**Reason:** In order to safeguard the amenity of neighbouring occupiers.

14. No development shall commence on site until a scheme for the enhancement of the pedestrian refuge island on Glebe St at its junction with Chapel St has been submitted to, and approved in writing by, the Planning Authority. The agreed scheme shall be implemented prior to first occupation of the hotel hereby granted planning permission.

**Reason:** In the interests of pedestrian safety.

15. No part of the development shall be occupied until a comprehensive Travel Plan that sets out proposals for reducing dependency on the private car has been submitted and approved in writing by the planning authority, after consultation with Transport Scotland, as the Trunk Roads Authority. In particular this Travel Plan shall identify measures to be implemented, the system of management, monitoring (for a period of not less than five years), review, reporting and the duration of the plan.

**Reason:** To be consistent with the requirements of Scottish Planning Policy (SPP) and PAN 75 Planning for Transport

16. No part of the development shall be occupied until a comprehensive Operational Traffic Management Plan (OTMP) has been submitted to, and approved by, the Planning Authority in consultation with the relevant Roads Authority(s). The OTMP, which shall be implemented as approved during the operation of the development, must include:
- i. A description of all measures to be implemented by the developer/operator in order to manage traffic during operation of the development hereby granted planning permission, with proposals for any additional signage and traffic control. In particular this should include:
    - A scheme for, and provision of, suitable advance directional signage/signage to promote the use of Friar's Street as the principal access to the development and discourage access directly from Chapel Street;
    - A scheme for, and provision of, suitable signage to prevent, as far as is possible, traffic egressing the site onto Douglas Row;
  - ii. Review of, and potential upgrades of, existing waiting restrictions;
  - iii. Details for covered and secure staff cycle parking (min 10 cycles) and their maintenance;
  - iv. Measures to prevent, guests or delivery vehicles, impacting upon the parking/deliveries area of the adjacent warehouse building.

Thereafter the approved Operational Traffic Management Plan shall be implemented in full, and any agreed measure maintained, unless otherwise approved in writing by the Planning Authority.

**Reason:** In the interest of road safety and to protect, as far as possible, the interests of neighbouring occupiers from as increase in traffic to the area.

17. Details of the lighting within the site shall be submitted for the approval of the Planning Authority, after consultation with Transport Scotland, as the Trunk Roads Authority.

**Reason:** To ensure that there will be no distraction or dazzle to drivers on the trunk road and that the safety of the traffic on the trunk road will not be diminished.

18. Prior to commencement of the development, details of the frontage landscaping treatment along the trunk road boundary shall be submitted to, and approved by, the Planning Authority, after consultation with Transport Scotland.

**Reason:** To ensure that there will be no distraction to drivers on the trunk road and that the safety of the traffic on the trunk road will not be diminished.

19. Details of the piled foundations of permanent structures within the site shall be submitted for the approval of Highland Council, after consultation with Transport Scotland.

**Reason:** To prevent interaction between the piled foundations of the A82 Friar's Bridge, and the foundations of the hotel building and / or any other permanent structures on site.

20. There shall be no drainage connections to the trunk road drainage system.

**Reason:** To ensure that the efficiency of the existing trunk road drainage network is not affected.

21. No development shall commence until a Waste Management Strategy has been submitted to, and approved in writing by, the Planning Authority. This shall detail an approach to sustainable waste management in the operation of all aspects of development and; identify bin collection points and off-street bin stores; set out procedures to ensure that no refuse or recycling materials associated with the development are stored or places for collection on the public highway or pavement, except on day of collection; the proposed collection schedule for refuse and recycling; identify size of and routes for waste collection vehicles, management of these routes and any conflicts between vehicular movements and pedestrians; and details of any required infrastructure. Thereafter the strategy shall be implemented in accordance with the approved details.

**Reason:** To ensure the development is appropriately serviced and the waste management arrangements do not have an adverse impact on the amenity of the area.

22. No development shall commence until a community liaison group is established by the developer, the terms of reference of which shall be submitted to, and approved in writing by, the Planning Authority. The group shall act as a vehicle for the community to be kept informed of project

progress as well as liaising over certain site specific construction matters that may have an impact on the local community in relation to noise from construction activities and construction traffic. The liaison group, or element of any combined liaison group relating to this development, shall be maintained until the development has been completed, unless otherwise agreed in writing with the Planning Authority.

**Reason:** to ensure that the local community is kept apprised of environmental matters relating to the development of the site.

**B.** Planning permission be **GRANTED** for application 19/003991/FUL.

### **REASON FOR DECISION**

It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

### **TIME LIMIT FOR THE IMPLEMENTATION OF THIS PLANNING PERMISSION**

In accordance with Section 58 of the Town and Country Planning (Scotland) Act 1997 (as amended), the development to which this planning permission relates must commence within THREE YEARS of the date of this decision notice. If development has not commenced within this period, then this planning permission shall lapse.

### **FOOTNOTE TO APPLICANT**

#### **Initiation and Completion Notices**

The Town and Country Planning (Scotland) Act 1997 (as amended) requires all developers to submit notices to the Planning Authority prior to, and upon completion of, development. These are in addition to any other similar requirements (such as Building Warrant completion notices) and failure to comply represents a breach of planning control and may result in formal enforcement action.

1. The developer must submit a Notice of Initiation of Development in accordance with Section 27A of the Act to the Planning Authority prior to work commencing on site.
2. On completion of the development, the developer must submit a Notice of Completion in accordance with Section 27B of the Act to the Planning Authority.

Copies of the notices referred to are attached to this decision notice for your convenience.

## **Major Development Site Notice**

Prior to the commencement of this development, the attached Site Notice must be posted in a publicly accessible part of the site and remain in place until the development is complete. This is a statutory requirement of the Town and Country Planning (Scotland) Acts and associated regulations.

## **Accordance with Approved Plans and Conditions**

You are advised that development must progress in accordance with the plans approved under, and any conditions attached to, this permission. You must not deviate from this permission without consent from the Planning Authority (irrespective of any changes that may separately be requested at the Building Warrant stage or by any other Statutory Authority). Any pre-conditions (those requiring certain works, submissions etc. prior to commencement of development) must be fulfilled prior to work starting on site. Failure to adhere to this permission and meet the requirements of all conditions may invalidate your permission or result in formal enforcement action

## **Flood Risk**

It is important to note that the granting of planning permission does not imply there is an unconditional absence of flood risk relating to (or emanating from) the application site. As per Scottish Planning Policy (paragraph 259), planning permission does not remove the liability position of developers or owners in relation to flood risk.

## **Scottish Water**

You are advised that a supply and connection to Scottish Water infrastructure is dependent on sufficient spare capacity at the time of the application for connection to Scottish Water. The granting of planning permission does not guarantee a connection. Any enquiries with regards to sewerage connection and/or water supply should be directed to Scottish Water on 0845 601 8855.

## **Septic Tanks and Soakaways**

Where a private foul drainage solution is proposed, you will require separate consent from the Scottish Environment Protection Agency (SEPA). Planning permission does not guarantee that approval will be given by SEPA and as such you are advised to contact them direct to discuss the matter (01349 862021).

## **Local Roads Authority Consent**

In addition to planning permission, you may require one or more separate consents (such as road construction consent, dropped kerb consent, a road openings permit, occupation of the road permit etc.) from the Area Roads Team prior to work commencing. These consents may require additional work and/or introduce additional specifications and you are therefore advised to contact your local Area Roads office for further guidance at the earliest opportunity.

Failure to comply with access, parking and drainage infrastructure requirements may endanger road users, affect the safety and free-flow of traffic and is likely to result in enforcement action being taken against you under both the Town and Country Planning (Scotland) Act 1997 and the Roads (Scotland) Act 1984.

Further information on the Council's roads standards can be found at: <http://www.highland.gov.uk/yourenvironment/roadsandtransport>

Application forms and guidance notes for access-related consents can be downloaded from:

[http://www.highland.gov.uk/info/20005/roads\\_and\\_pavements/101/permits\\_for\\_working\\_on\\_public\\_roads/2](http://www.highland.gov.uk/info/20005/roads_and_pavements/101/permits_for_working_on_public_roads/2)

### **Mud and Debris on Road**

Please note that it is an offence under Section 95 of the Roads (Scotland) Act 1984 to allow mud or any other material to be deposited, and thereafter remain, on a public road from any vehicle or development site. You must, therefore, put in place a strategy for dealing with any material deposited on the public road network and maintain this until development is complete.

### **Construction Hours and Noise-Generating Activities**

You are advised that construction work associated with the approved development (incl. the loading/unloading of delivery vehicles, plant or other machinery), for which noise is audible at the boundary of the application site, should not normally take place outwith the hours of 08:00 and 19:00 Monday to Friday, 08:00 and 13:00 on Saturdays or at any time on a Sunday or Bank Holiday in Scotland, as prescribed in Schedule 1 of the Banking and Financial Dealings Act 1971 (as amended).

Work falling outwith these hours which gives rise to amenity concerns, or noise at any time which exceeds acceptable levels, may result in the service of a notice under Section 60 of the Control of Pollution Act 1974 (as amended). Breaching a Section 60 notice constitutes an offence and is likely to result in court action.

If you wish formal consent to work at specific times or on specific days, you may apply to the Council's Environmental Health Officer under Section 61 of the 1974 Act. Any such application should be submitted after you have obtained your Building Warrant, if required, and will be considered on its merits. Any decision taken will reflect the nature of the development, the site's location and the proximity of noise sensitive premises. Please contact [env.health@highland.gov.uk](mailto:env.health@highland.gov.uk) for more information.

## **Protected Species – Halting of Work**

You are advised that work on site must stop immediately, and Scottish Natural Heritage must be contacted, if evidence of any protected species or nesting/breeding sites, not previously detected during the course of the application and provided for in this permission, are found on site. For the avoidance of doubt, it is an offence to deliberately or recklessly kill, injure or disturb protected species or to damage or destroy the breeding site of a protected species. These sites are protected even if the animal is not there at the time of discovery. Further information regarding protected species and developer responsibilities is available from SNH: [www.snh.gov.uk/protecting-scotlands-nature/protected-species](http://www.snh.gov.uk/protecting-scotlands-nature/protected-species)

Designation: Area Planning Manager – South

Author: David Mudie

Background Papers: Documents referred to in report and in case file.

Relevant Plans: **19/03933/FUL**

Plan 1 - KEP-XX-XX-DR-A-5020-0001 REV 0 - Location Plan

Plan 2 - KEP-XX-XX-DR-A-5020-0002 REV 2 - Site Plan

Plan 3 - KEP-XX-XX-DR-A-5020-0003 REV 0 - Section Plan

Plan 4 - KEP-XX-00-DR-A-7060-0110 REV 1 - Ground Floor Plan

Plan 5 - KEP-XX-01-DR-A-7060-0111 REV 1 - First Floor Plan

Plan 6 - KEP-XX-02-DR-A-7060-0112 REV 1 - Second Floor Plan

Plan 7 - KEP-XX-03-DR-A-7060-0113 REV 1 - Third Floor Plan

Plan 8 - KEP-XX-04-DR-A-7060-0114 REV 1 - Fourth Floor Plan

Plan 9 - KEP-XX-RL-DR-A-3010-0001 REV 1 - Roof Plan

Plan 10 – KEP-XX-XX-DR-A-5020-0003 REV 0 - Site Sections

Plan 11 - KEP-XX-EL-DR-A-7030-0112 REV 0 - Elevations Sheet 1

Plan 12 - KEP-XX-EL-DR-A-7030-0113 REV 2 - Elevations Sheet 2

Plan 13 - KEP-XX-EL-DR-A-7030-0114 REV 0 - Elevations Sheet 3

Plan 14 - KEP-XX-VS-A-7090-0001 – Artists Impression

**19/03991/FUL**

Plan 1 - KEP-XX-XX-DR-A-5020-0010 - Location Plan

Plan 2 - KEP-XX-XX-DR-A-5020-0011 REV 2 - Site Plan

Plan 3 - KEP-XX-XX-DR-A-5020-0012 REV 1 - Elevations



## Inverness Design Review Panel

### Panel Report

Hotel Proposal  
Glebe Street, Inverness

8 August 2019

This report is the view of the Inverness Design Review Panel and is not attributable to any one individual. It does not prejudice any of the organisations represented on the Panel forming a differing view about development proposals at a later stage.

#### Executive summary

The Panel welcomes the opportunity to comment further on a revised design for this prominent riverfront site, whose development has significant potential to shape the future of the city centre.

There is recognition of how far the design has evolved in order to address some of the key issues with the previous scheme. It is considered that the scale and massing is appropriate to the site. Breaking down the mass and creating distinct building forms is welcome and subject to achieving appropriate junction detailing will contribute to the successful modelling of the elevations. The use of a pitch roof form and wall head dormers will assist in reflecting some of the variety/distinctiveness of the surroundings. There remains a question over how the building relates to the river and this could be viewed as a limitation of the use of CLT/modular construction. There are differing views on this and how the development will be able to respond sensitively to the site's historic riverside setting in this regard. The introduction of the building on Glebe Street is a welcome addition.

The re-emphasis on connecting ground floor accommodation and activity to the public realm is welcomed and encouraged, along with proposals for tree planting and landscaping.

### 1. INTRODUCTION

- 1.1. This report relates to the proposed development of c.180-bedroom hotel at Glebe Street, Inverness. It should be read in conjunction with meeting papers, the project brief, 3-D models that describe the proposal and illustrate the wider site context, design concept, layout, massing and elevational studies, along with options for material palettes.

## **2. RECOMMENDATIONS**

- 2.1. In taking forward this proposal the Panel recommends that the design should:
- Respond positively to the riverside edge and the setting of Douglas Row
  - Ensure the design of all elevations/frontages/facades (including rear elevations) feature articulation and differentiation in massing and elevational treatment and respond sensitively to surrounding views, streets and spaces;
  - Glebe Street block to be delivered as an integral part of the scheme in order to improve connection with the City;
  - Potential for further height at the north-west corner of the site;
  - Potential within the north-west block to animate use of building higher up;
  - Maintain a strong focus on public realm with opportunities for larger scale tree planting along the river front; and
  - Connect with A82 – both physical connection i.e. ramp directly to site and public realm on A82 side.

## **3. OVERVIEW**

- 3.1. The Panel welcomes the opportunity to comment further on the revised design proposals for this important vacant site whose development plays a significant role in shaping the future of Inverness city centre. It appreciates the developer's commitment to securing Panel advice in the design process together with the high standard of presentation material put forward by the design team, in particular their 3-D massing models and material samples. The Panel thanks the team for a well-considered presentation of the scheme.
- 3.2. The Panel considers this to be an exciting development opportunity on a difficult site with a long planning history; of which the most recent appeal decision is most relevant. The Panel recognises that significant effort has been put into amending the scheme to address some of the issues raised against the previous scheme but advises that the proposal would benefit from a detail analysis of how the new proposal addresses the context, respecting the setting of the listed buildings on Douglas Row and the character and appearance of the Conservation Area; key features of the Reporters discussion on the merits of the previous scheme.
- 3.3. The Panel acknowledges the opportunities and benefits of using CLT/modular construction for development of this type and scale and at the same time recognises the limitations; in this case essentially around the building morphology. It is therefore important that the development can respond sensitively to the historic riverside setting both in terms of its positioning within the site and through the variety/distinctiveness of modelling and elevational treatment. The smaller block onto Glebe Street was considered to be a sensible and clever approach to creating a perimeter block and street. This and the building's interface with the river are viewed as an integral part of creating high quality public realm.

## **4. BUILT FORM: MODULAR CONSTRUCTION, MASSING, ARTICULATION**

- 4.1. The Panel had differing views regarding the alignment of the building with the river. Setting the building back, as proposed, is seen as a solution to providing greater space for public

interaction with the river as well as ensuring that in more distant and higher views, such as the Castle viewpoint, the mass would compete less with the scale of neighbouring buildings. There was a question over why it did not follow the river edge, a solution that was, for some, considered preferable from a wider city perspective; i.e. continue the building line with Douglas Row.

- 4.2. The alignment to Friar's Bridge is considered appropriate, as is stepping down of the mass from its highest point on the north-west corner, both to Friar's Bridge and on the river. The view was expressed that the development could take more height at the south-west corner but addressing the river rather than Friar's Bridge. The Glebe Street building is considered important to creating a perimeter block and completing the street.
- 4.3. The loss of roof terraces that provided attractive outdoor space for guest use was commented on; with the recommendation that perhaps more could be done on the south-west corner to animate the use at a higher level.
- 4.4. Splitting the mass down into several distinct buildings is considered a positive step. Modelling, articulation and differentiation of all facades, including the rear elevations, is required to make that successful. The decision to keep a simple palette of materials, which were well received, is a positive one. Differentiation between facades can be achieved through introduction of setback at junctions. The use of pitched roofs, which were considered to have been handled elegantly and considered well detailed at their edges, and wall head dormer details assist with reflecting the variety/distinctiveness of the surroundings. In terms of further detailing a question was raised with regard to the potential for achieving oblique views out from rooms.
- 4.5. Views from Friars Street and Shore Street Roundabout should also be considered and the design intent well evidenced.

## **5. THE PUBLIC REALM**

- 5.1. The emphasis on connecting ground floor accommodation and activity to the public realm is welcome and encouraged, in particular:
  - The close relationship between ground floor level and the level of the river walkway;
  - The proposed new, high quality public open space at the corner of Glebe Street and Douglas Row, and
  - On Glebe Street with the creation of the perimeter block.
- 5.2. The Panel also welcomes proposals to:
  - Extend the line of riverside trees, providing new trees are semi-mature and an appropriate species of street tree; and
  - Introduce appropriate high quality hard and soft landscaping that enhances the existing streetscape, including views into and out of the site.
- 5.3. The Panel considered whether there were opportunities to provide:
  - More free-flowing space between the building and Friar's Bridge; and
  - An entrance/ramp directly from the Friar's Bridge

- 5.4. Although not directly discussed on this occasion previously the Panel strongly recommends expanding the scope of public realm improvements to cover enhancements to the underpass below Friars Bridge, including attractive, high quality lighting.
- 5.5. In progressing this proposal the developer is encouraged to:
- ensure that all open space within the footprint of the site is designed to be publicly accessible and an integral part of the public realm;
  - incorporate public art into the design of lighting, seating and paving;
  - ensure that benches and other seating are associated with entrances, bars and restaurants (to deter anti-social behaviour);
  - ensure public realm design, in particular parking areas, takes account of views from hotel bedrooms;
  - enhance the setting of the historic gravestones on the northern edge of the site.

## Appendix 3 – Assessment against the criteria of the Inverness City Centre Development Brief (ICCDB)

**Criterion B1** is related to footfall generating uses being sequentially considered with city centre first principles being applied. The proposal accords with this criterion.

**Criterion B2** sets out that developments including a mix of uses will be supported on sites identified on map 3.1 of the ICCDB will be supported if they accord with table 7.1 of the ICCDB. The site is not identified within the map and is single use development therefore this criteria does not apply. The criteria in table 7.1 of the ICCDB are considered elsewhere in this report.

**Criterion V1** sets out that footfall generating uses at ground floor level will be preferred land use at ground floor. The proposal accords with this criterion.

**Criterion V2** relates to increasing 24/7 activity. This proposal will increase activity within this part of the City at all hours but without significant impact on neighbouring residents. The proposal complies with this criterion.

**Criterion V3** relates to the adaptive reuse of Inverness Castle. This criterion is not applicable to the proposal.

**Criterion V4** sets out developments for new retail and leisure uses will be supported in key opportunity sites identified on map 4.1 of the ICCDB if they accord with table 7.1 of the ICCDB. The site is not identified on map 4.1 and does not fit with this criteria. The criteria in table 7.1 of the ICCDB are considered elsewhere in this report.

**Criterion V5** relates to riverside activity. The proposal will lead to an increase in activity within this part of the riverside which is currently not used. The proposal accords with this criterion.

**Criterion L1** sets out residential developments will be supported in key opportunity sites identified on map 5.1 of the ICCDB if they accord with table 7.1 of the ICCDB. The site is identified as a site for new residential development on map 5.1, on the basis of the previous permission, but the criterion is not applicable to this development.

**Criteria L2 and L3** relate to exemptions for conversions. This criterion is not applicable to the proposal.

**Criteria L4** relates to houses of multiple occupation. This criterion is not applicable to the proposal.

**Criterion A1** requires integration of new development with priority routes for active travel improvements. The site is likely to be accessed from Friar's Street and also Chapel Street, which is identified as a key route for active travel. Work is required to improve the existing pedestrian refuge at the junction of Chapel Street and Glebe Street. Further contributions will be sought to enhance active travel improvements along Chapel Street/Academy Street.

**Criterion A2** requires developments to promote new or enhanced facilities for walking and cycling. The proposed development will provide enhancements to public realm along Glebe Street/River front.

**Criterion A3** relates to the wayfinding strategy. A contribution to the wayfinding strategy is sought.

**Criterion A4** relates to seeking contributions towards active travel improvements. The proposal will make a contribution to active travel improvements.

**Criterion A5** relates to Inverness Railway Station. This criterion is not applicable to the proposal.

**Criterion A6** identifies the issues which will be taken into account in determining parking requirements in the City Centre. Transport Planning has undertaken this exercise and the findings are that a commuted sum towards active travel improvements is required.

**Criterion D1** relates to the development of underused or neglected heritage assets. This criterion is not applicable to the proposal.

**Criterion D2** sets out that where current uses are not viable the redevelopment will be supported subject to it providing the development is high quality and makes a positive contribution to the visual and spatial character of the surrounding area. The building that was on this site has already been demolished.

**Criterion D3** requires all new development to accord with the key place making principles set out in table 7.1 of the ICCDB. These are considered in turn below:

*Principle 1 - Contextual Analysis* - contextual analysis of the site has been provided within the Design Statement which accompanies the application.

*Principle 2 - Key Views* - is addressed in paragraph 8.19 of the report. It is considered that the proposal accords with the principle set out.

*Principle 3 - Historic Buildings and Spaces* - the development creates space between the proposed hotel and the Category B listed buildings on Douglas Row. The new building is set back sufficiently and steps down from five storeys to three on the riverfront elevation which reflects the more domestic scale of these historic buildings. This approach respects the setting of the listed buildings.

*Principle 4 - Contemporary Design* - the building is of a contemporary design while responding positively to the historic features and context of the City, through the use of detailing and material choice. In particular the ordered fenestration and parapets gives a strong horizontal emphasis to the building like so many of the Victorian buildings in the City. The use of large format cladding in a blonde stone like texture again reflects that predominant character yet in a very modern way.

*Principle 5 - Block Structure and Permeability* - The proposal creates a street block, albeit only on two sides as a result of Friar's Bridge which, as well as the flood wall, limits permeability. There is however permeability within the ground floor of the building with its

outside terrace on the corner of Douglas Row and Glebe Street and main entrance opposite Friar's Street.

*Principle 6 - Height, scale and massing* - The height, scale and mass is considered appropriate. The site requires a building of scale, given the proximity to the bridge. The massing on the riverside wing has been broken down by stepping the building down towards Douglas Row. Set back in elevations, stepping building heights and clever use of wall head dormers providing an illusion of height differentiation assist in breaking down the mass.

*Principle 7 - Frontages* - The proposed development appropriately addresses the River Ness. On the riverside there is a continuous active frontage that wraps around onto Glebe Street. This is where the front of house (including food and beverage offering) will be located and this corner at the riverfront will be where most activity will be centred. There is a missed opportunity with regard to the Glebe Street block in that it does not contain a similar active frontage, however this building is a welcome addition to the street nonetheless.

*Principle 8 - Elevational Treatment* - The elevational treatment is appropriate - as indicated in Principle 4. It is considered the proposal accords with this principle.

*Principle 9 - Materials and Colour* - it is considered that the range of materials now proposed is appropriate. Final details of the materials can be secured by condition.

*Principle 10 - Access* - it is considered that the proposed development will be easy to access for pedestrians, cyclists and disabled people. Active travel routes require upgrading and this proposal is expected to contribute to that.

*Principle 11 - Parking* - the proposed development contains car parking although a shortfall has been identified. This has been accepted by Transport Planning subject to a commuted sum. Servicing arrangements have also, in principle been accepted by Transport Planning.

*Principle 12 - Public Realm* - The revised scheme creates a large area of open space that can contribute to the public realm. There is potential for this to include public art. Further details of the landscaping/public realm improvements and public art can be secured by condition.

*Principle 13 - Open Space* - open space is proposed within this development. This will by and large be publically accessible.

*Principle 14 - Trees and Planting* - some limited planting is proposed. The final details of landscaping can be secured by condition.

*Principle 15 - Security* - It has been highlighted by the Design Review Panel that it may be beneficial to improve the underpass and consider the stair for security purposes. This is out with the scope of this project.

**Criterion D4** relates to specific development sites. This criterion is not applicable to the proposal.

**Criterion D5** seeks to ensure that significant developments incorporate public art. The development will deliver landscaping/public realm works. This provides an opportunity to secure an appropriate scheme of public art.

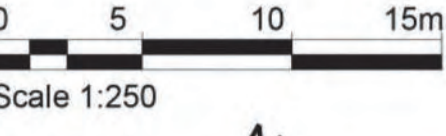
**Criterion D6** relates to alteration, reinstatement or improvement of shopfronts. This criterion is not applicable to the proposal.

**Criterion D7** seeks to ensure that sufficient off-street bin storage is provided. Based on the submissions provided by the application, it is considered that the proposed development is acceptable in this regard.

**Criterion D8** seeks to safeguard and enhance landscape structure, green infrastructure, and avoid impact on the condition of the River Ness. It is considered that the existing landscape structure will not be adversely affected. Exact details of the landscaping can be secured by condition.

**Criterion D9** requires consideration of developments that may impact on air quality. The uses proposed are unlikely to impact on air quality. Therefore this criterion is not applicable to the proposal.





KEY:  
- - - PLANNING BOUNDARY  
- - - ADDITIONAL LAND WITHIN CLIENT OWNERSHIP

- KEY
- 1 Main 4\* Hotel Block
  - 2 Glebe Street 4\* Hotel Block
  - 3 Warehouse (Use tbc)
  - 4 External River Ness Terrace Area
  - 5 River Ness Landscaping
  - 6 Refuse / Deliveries / Services Area
  - 7 Car Parking, Circa 41 including 2 disabled
  - 8 Existing Trees
  - 9 Hotel Entrance
  - 10 Vehicular Access
  - 11 Taxi & Coach Drop-off
  - 12 Hostel (Existing)
  - 13 Substation (Existing)
  - 14 Central Cycle Store
- Primary Entrance  
● Secondary Entrance

Rev	Description	Dr'n	Ch'd	Date
2	Updated Pedestrian Route Width to 2M	RP	ES	08/11/19
1	Updated Substation Location, Updated Glebe Street Block	RP	ES	28/10/19
0	First Issue For Planning	RP	ES	28/08/19

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Project  
**Glebe Street Inverness**

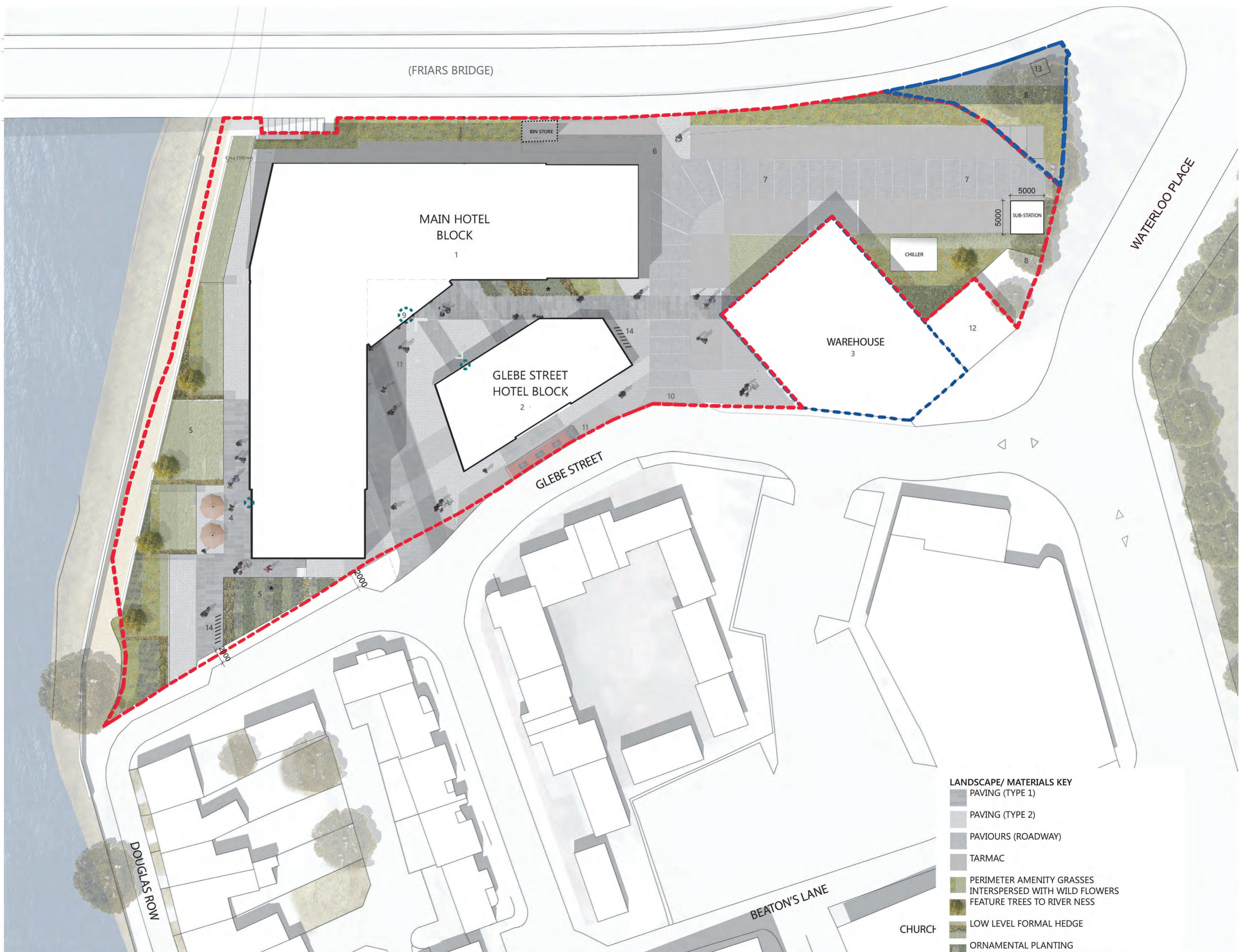
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**Proposed Site Plan**

Project No.  
**P17149**

Drawing No.  
**KEP-XX-XX-DR-A-5020-0002** Rev **2**

Status  
**PLANNING**

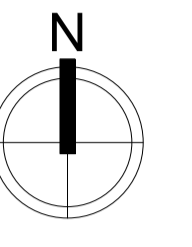
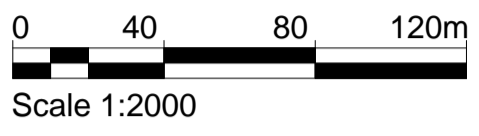
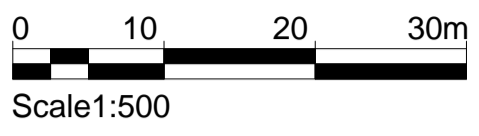
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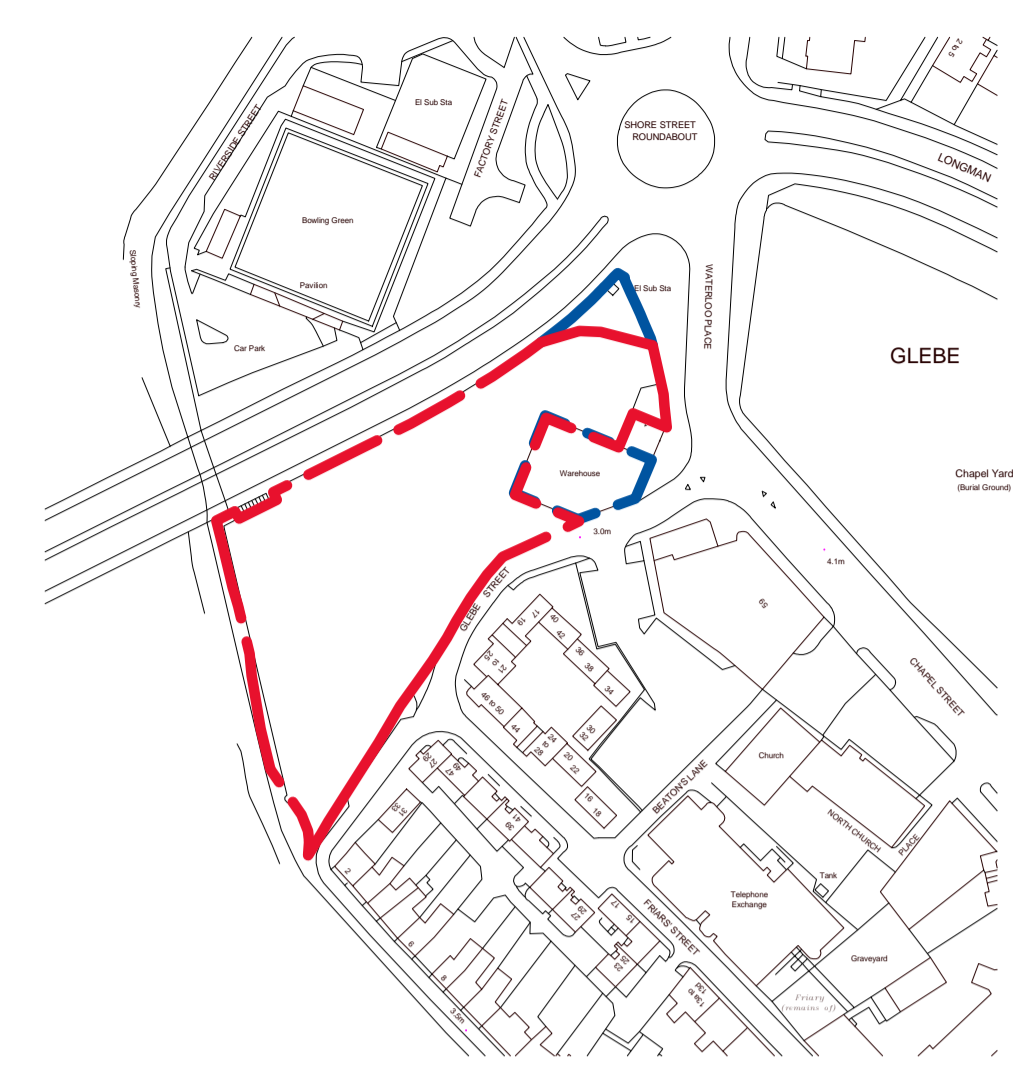
- LANDSCAPE/ MATERIALS KEY**
- PAVING (TYPE 1)
  - PAVING (TYPE 2)
  - PAVIOURS (ROADWAY)
  - TARMAC
  - PERIMETER AMENITY GRASSES INTERSPERSED WITH WILD FLOWERS
  - FEATURE TREES TO RIVER NESS
  - LOW LEVEL FORMAL HEDGE
  - ORNAMENTAL PLANTING (\*GLEBE STREET AND MAIN ENTRANCE)

**A-5020-Proposed Site Plan**  
 Scale - 1 : 250





- KEY:
- - - PLANNING BOUNDARY
  - - - ADDITIONAL LAND WITHIN CLIENT OWNERSHIP



**A-5020-Existing Location Plan**  
 Scale - 1 : 2000



**A-5020-Existing Site Plan**  
 Scale - 1 : 500

Rev	Description	D/r's	CHK'd	Date
0	First Issue For Planning		RC ES	28/08/19

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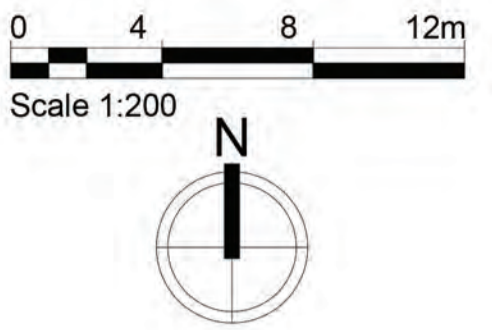
Project  
**Glebe Street Inverness**

Drawing  
**Existing Site Plan & Location Plan**

Project No.	Rev
P17149	
Drawing No.	
KEP-XX-XX-DR-A-5020-0001	0
Status	

Created	RC	Checked	ES
Date	28/08/19	Scale	As Indicated





----- EXISTING SITE LEVEL  
 \_\_\_\_\_ PROPOSED SITE LEVEL

**A-5020-Site Section A**  
 Scale - 1 : 200



Rev	Description	Dr'n	CHK'd	Date
0	First Issue For Planning	RC	ES	28/08/19

**A-5020-Site Section B**  
 Scale - 1 : 200

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Drawing  
**Existing & Proposed Site Sections**

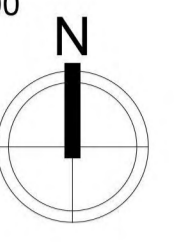
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Drawing No.	KEP-XX-XX-DR-A-5020-0003
Rev	0
Status	

Created • RC  
 Date • 28/08/19  
 Checked • ES  
 Scale • As indicated  
 @ A1





Scale 1:200



- KEY:
- BEDROOMS
  - BAR/ RESTAURANT/  
MEETING ROOM
  - STAIR / LIFT CORE
  - CIRCULATION
  - ANCILLARY

GROUND FLOOR LEVEL BLOCK A  
 +4,690m  
 GROUND FLOOR LEVEL BLOCK B  
 +4,690m

GROUND FLOOR BEDROOMS  
 BLOCK A : --  
 BLOCK B : 8  
 TOTAL: 8



**A-7060 Level 00 (1.200)**  
 Scale - 1 : 200

1	For Planning	RC	ES	05/09/19
0	First Issue For Planning	RC	ES	28/08/19
Rev	Description	D/n	CHK'd	Date



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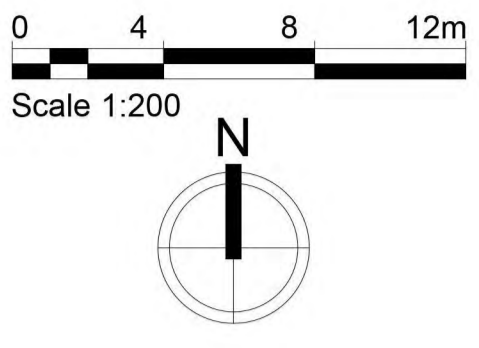
Project  
**Glebe Street Inverness**

Drawing  
**Ground Floor Plan**

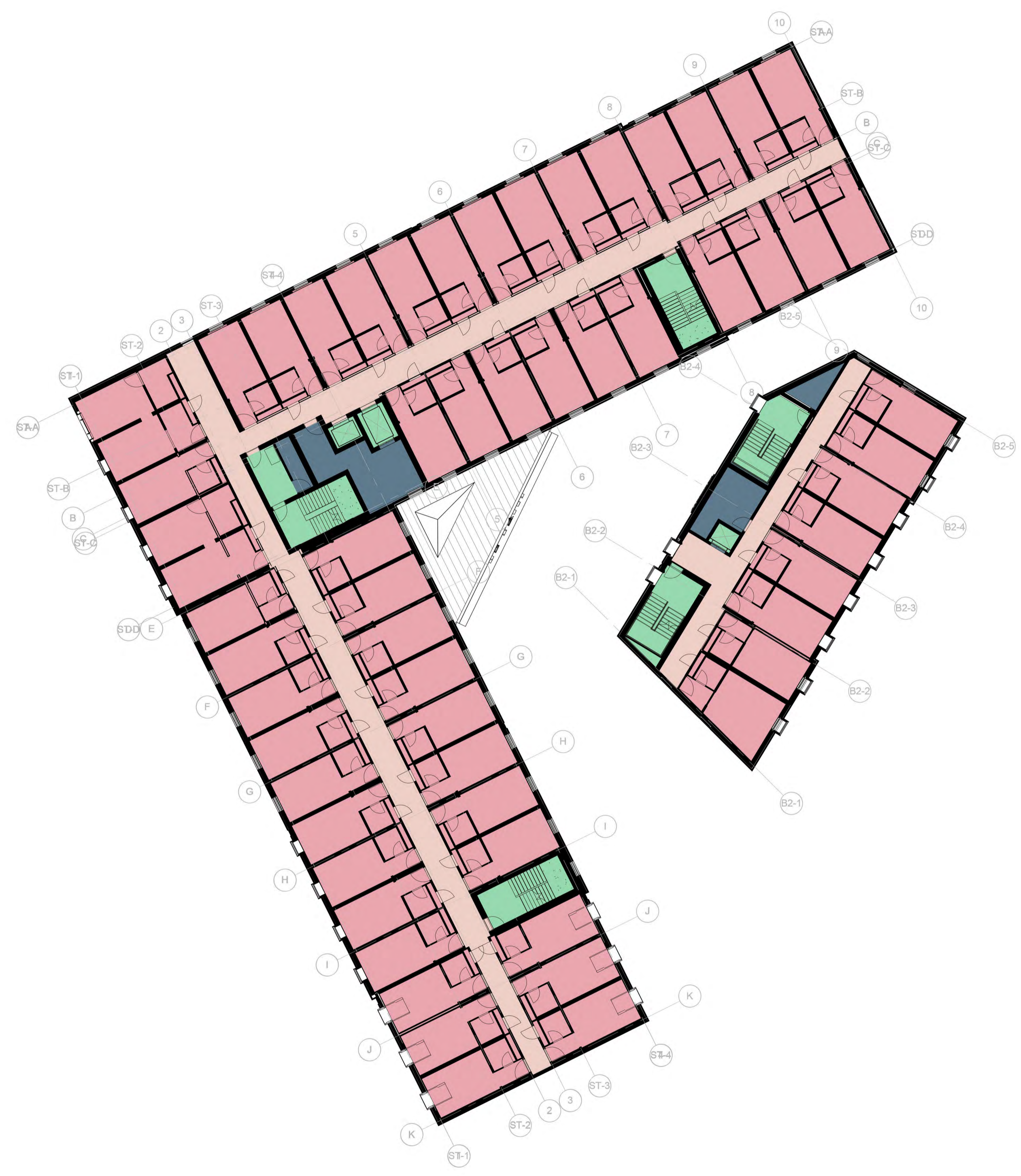
Project No.  
**P17149**  
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**KEP-XX-00-DR-A-7060-0110**  
 Rev  
**1**

Status  
**PLANNING**  
 Created: RC  
 Date: 28/08/19  
 Checked: ES  
 Scale: As indicated  
 Date: 05/09/19  
 Scale: As indicated





- KEY:
- BEDROOMS
  - STAIR / LIFT CORE
  - CIRCULATION
  - ANCILLARY
- FIRST FLOOR LEVEL BLOCK A  
 +9,090m  
 FIRST FLOOR LEVEL BLOCK B  
 +7,645m
- FIRST FLOOR BEDROOMS  
 BLOCK A : 50  
 BLOCK B : 8  
 TOTAL: 58  
 (INCLUDING 2 DELUXE)



Rev	Description	D/n	CHK'd	Date
1	For Planning	RC	ES	05/09/19
0	First Issue For Planning	RC	ES	28/08/19

**A-7060 Level 01 (1.200)**  
 Scale - 1 : 200

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Project  
**Glebe Street Inverness**

Drawing  
**First Floor Plan**

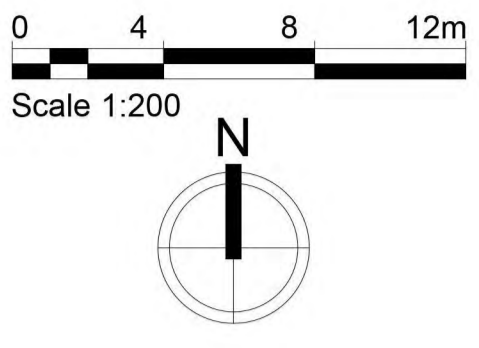
Project No.  
**P17149**

Drawing No.  
**KEP-XX-01-DR-A-7060-0111**

Status  
**PLANNING**

Created: RC 28/08/19  
 Checked: ES  
 Date: 28/08/19 Scale: As indicated @ A1

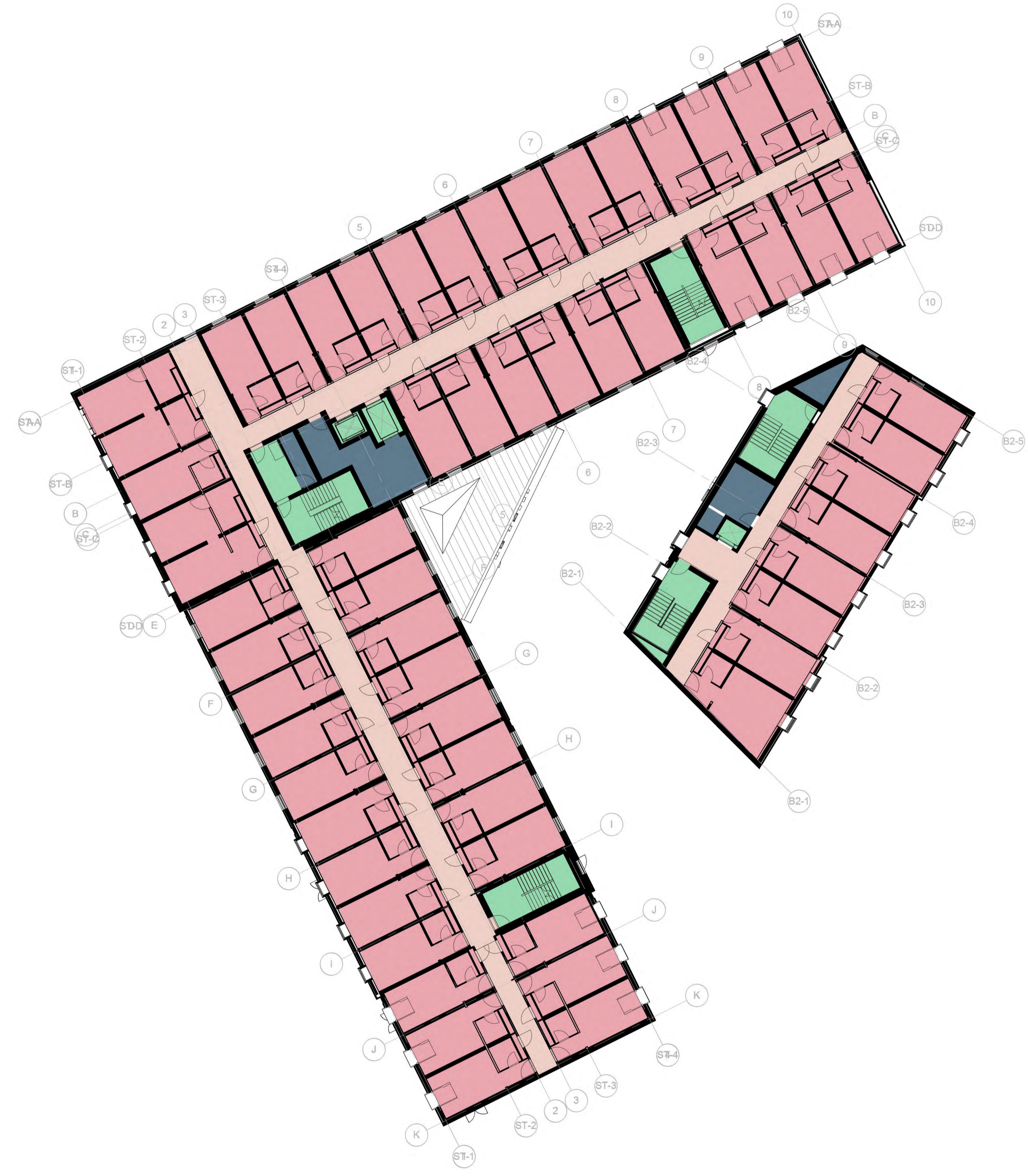




KEY:  
 ■ BEDROOMS  
 ■ STAIR / LIFT CORE  
 ■ CIRCULATION  
 ■ ANCILLARY

SECOND FLOOR LEVEL BLOCK A  
 +12,045m  
 SECOND FLOOR LEVEL BLOCK B  
 +10,600m

SECOND FLOOR BEDROOMS  
 BLOCK A : 50  
 BLOCK B : 8  
 TOTAL: 58  
 (INCLUDING 2 DELUXE)



Rev	Description	D/r	CHK'd	Date
1	For Planning	RC	ES	05/09/19
0	First Issue For Planning	RC	ES	28/08/19

**A-7060 Level 02 (1.200)**  
 Scale - 1:200

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Client  
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Project  
**Glebe Street Inverness**

Drawing  
**Second Floor Plan**

Project No.  
**P17149**

Drawing No.  
**KEP-XX-02-DR-A-7060-0112**

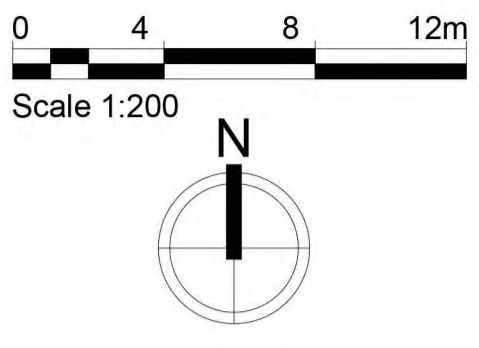
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Created: RC  
 Date: 28/08/19

Checked: ES  
 Scale: As indicated

Rev: 1  
 @ A1

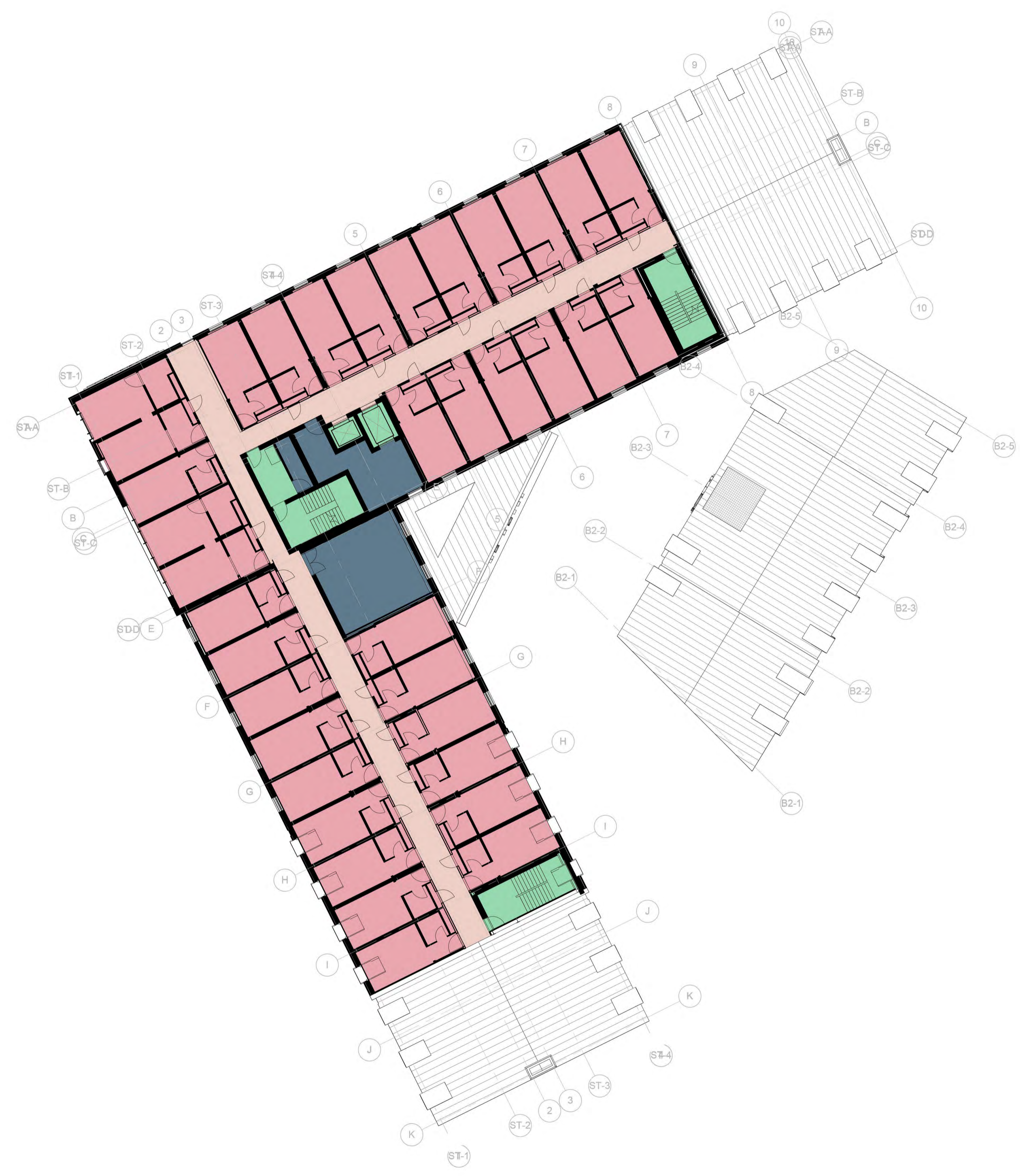




KEY:  
 BEDROOMS  
 STAIR / LIFT CORE  
 CIRCULATION  
 ANCILLARY

THIRD FLOOR LEVEL BLOCK A  
 +15,000m

THIRD FLOOR BEDROOMS  
 BLOCK A : 34  
 (INCLUDING 2 DELUXE)



**A-7060 Level 03 (1.200)**  
 Scale - 1 : 200

Rev	Description	D'n	CHK'd	Date
1	For Planning	RC	ES	05/09/19
0	First Issue For Planning	RC	ES	28/08/19

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Client  
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Project  
**Glebe Street Inverness**

Drawing  
**Third Floor Plan**

Project No.  
**P17149**

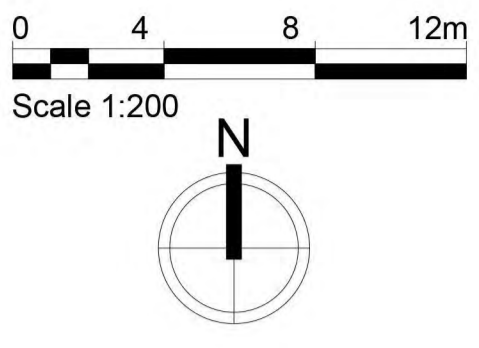
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Status  
**PLANNING**

Created • RC  
 Date • 28/08/19

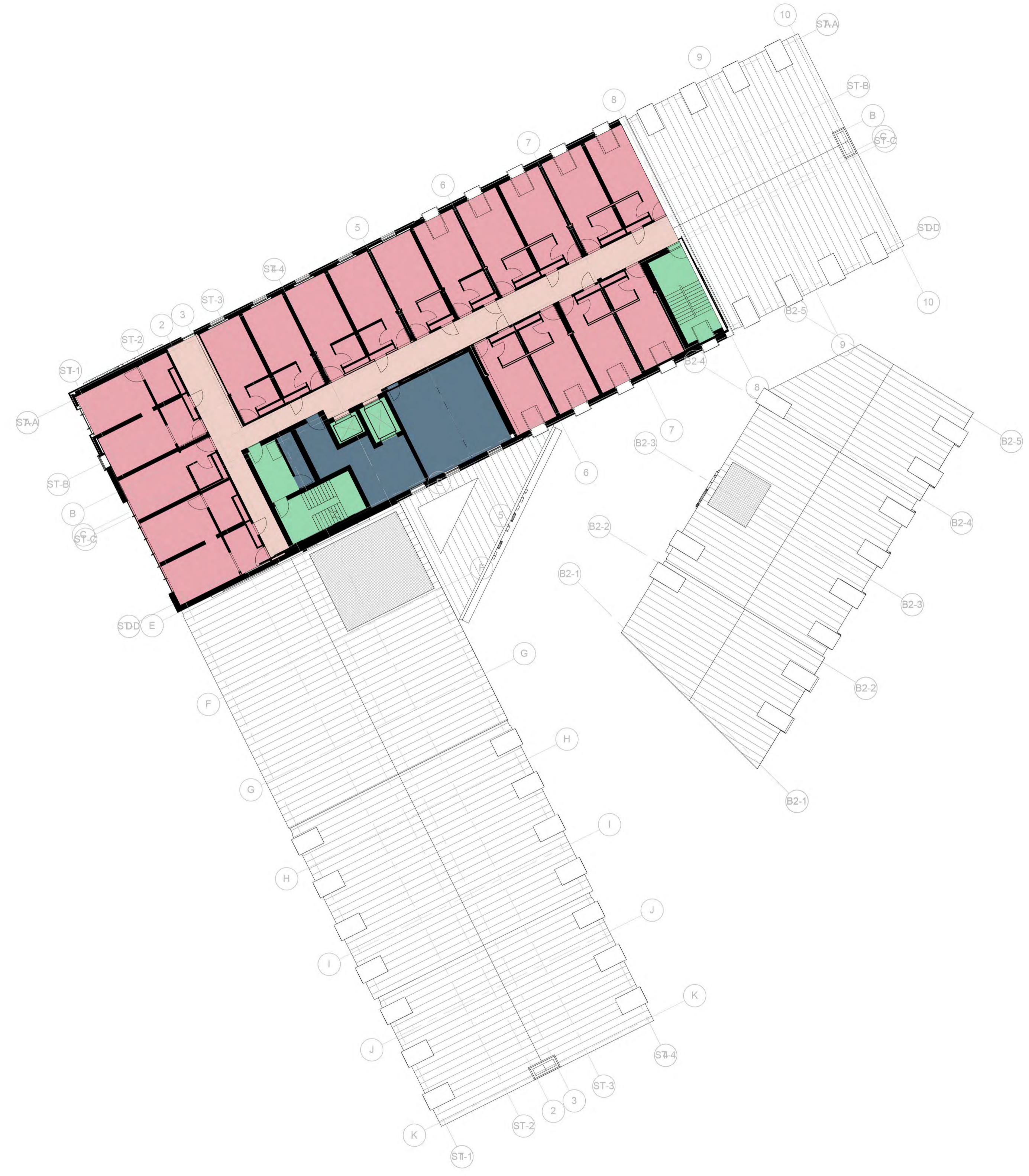
Checked • ES  
 Scale • As indicated





- KEY:
- BEDROOMS
  - STAIR / LIFT CORE
  - CIRCULATION
  - ANCILLARY

FOURTH FLOOR LEVEL +17,955m  
 FOURTH FLOOR BEDROOMS 17  
 (INCLUDING 2 DELUXE)  
 TOTAL  
 BLOCK A 151 BEDROOMS  
 BLOCK B CIRCA 24 BEDROOMS



**A-7060 Level 03 (1.200)**  
 Scale - 1 : 200

Rev	Description	D/r	CHK'd	Date
1	For Planning	RC	ES	05/09/19
0	First Issue For Planning	RC	ES	28/08/19

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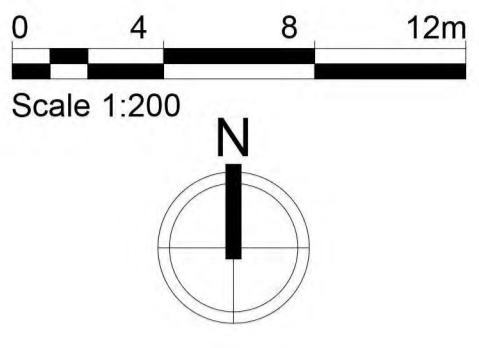
Project  
**Glebe Street Inverness**

Drawing  
**Fourth Floor Plan**

Project No.	Rev
P17149	1
Drawing No.	Rev
KEP-XX-04-DR-A-7060-0114	1
Status	
<b>PLANNING</b>	

Created • SO  
 Date • 28/03/19  
 Checked • ES  
 Scale • As  
 @ A1  
 indicated





**A-7060 Roof Level (1.200)**  
 Scale - 1 : 200

Rev	Description	D'n	CHK'd	Date
1	For Planning	RC	ES	05/09/19
0	First Issue For Planning	RC	ES	28/08/19

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Project  
**Glebe Street Inverness**

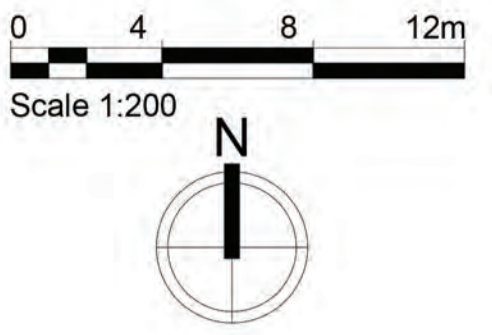
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**Roof Plan**

Project No.  
**P17149**  
 Drawing No.  
**KEP-XX-RL-DR-A-3010-0001**  
 Rev  
**1**

Status  
**PLANNING**

Created ● RC Checked ● ES  
 Date ● 28/08/19 Scale ● 1:200 @ A1





----- EXISTING SITE LEVEL  
 \_\_\_\_\_ PROPOSED SITE LEVEL

**A-5020-Site Section A**  
 Scale - 1 : 200



Rev	Description	Dr'n	CHK'd	Date
0	First Issue For Planning	RC	ES	28/08/19

**A-5020-Site Section B**  
 Scale - 1 : 200

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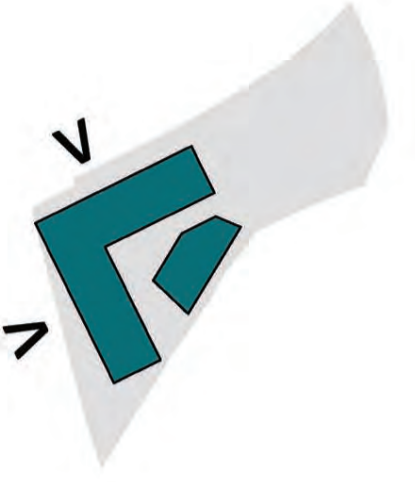
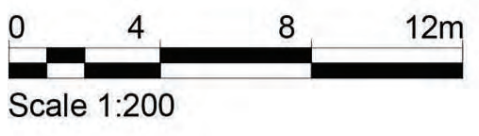
Client  
**Vastint Hospitality B.V.**  
 Project  
**Glebe Street Inverness**

Drawing  
**Existing & Proposed Site Sections**

Project No.  
**P17149**  
 Drawing No.  
**KEP-XX-XX-DR-A-5020-0003**  
 Rev  
**0**  
 Status

Created • RC  
 Date • 28/08/19  
 Checked • ES  
 Scale • As indicated  
 @ A1





**West Elevation as Proposed**  
 Scale - 1 : 200

- Materials Key:**
1. PPC louvre screening to plant areas (colour TBA)
  2. Low-E glazing with PPC aluminium framing, PPC aluminium projecting surround and opening vent (colour TBA)
  3. Low-E glazing (recessed) with PPC aluminium framing, PPC aluminium projecting surround and opening vent (colour TBA)
  4. Projecting feature dormer window with low-E glazing, PPC aluminium framing, PPC aluminium surround and opening vent (colour TBA)
  5. Low-E glazing with PPC aluminium framing (colour TBA)
  6. Back painted glazed spandrel (colour TBA)
  7. Ceramic stone panel (colour TBA)
  8. Double glazed curtain walling system with PPC aluminium frame (colour TBA)
  9. Structured natural stone panel (colour TBA)
  10. Standing seam zinc roof (colour TBA)
  11. PPC aluminium rainwater pipe (colour TBA)
  12. Feature gable windows with projecting PPC aluminium frames and mullions.



**North Elevation as Proposed**  
 Scale - 1 : 200

Rev	Description	Dr'n	CHK'd	Date
0	First Issue For Planning	RC	ES	28/08/19

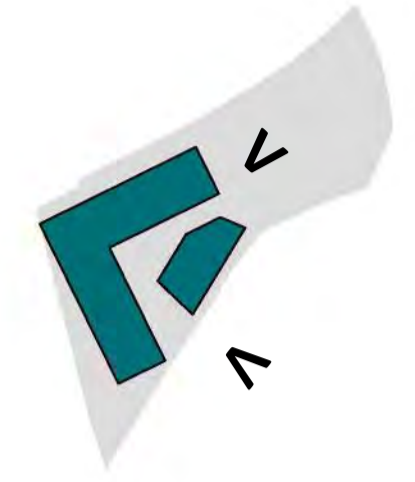
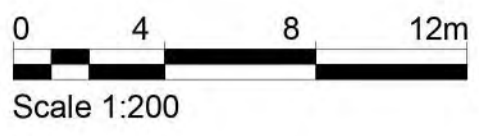
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 Client

Project  
**Glebe Street Inverness**  
 Drawing  
**Proposed Elevations - Sheet 01**

Project No.	Drawing No.	Rev
P17149	KEP-XX-EL-DR-A-7030-0112	0

Created: RC  
 Date: 28/08/19  
 Checked: Scale  
 Checker: As indicated  
 @ A1





**East / Courtyard Elevation**  
 Scale - 1 : 200

**Materials Key:**

1. PPC louvre screening to plant areas (colour TBA)
  2. Low-E glazing with PPC aluminium framing, PPC aluminium projecting surround and opening vent (colour TBA)
  3. Low-E glazing (recessed) with PPC aluminium framing, PPC aluminium projecting surround and opening vent (colour TBA)
  4. Projecting feature dormer window with low-E glazing, PPC aluminium framing, PPC aluminium surround and opening vent (colour TBA)
  5. Low-E glazing with PPC aluminium framing (colour TBA)
  6. Back painted glazed spandrel (colour TBA)
  7. Ceramic stone panel (colour TBA)
  8. Double glazed curtain walling system with PPC aluminium frame (colour TBA)
  9. Structured natural stone panel (colour TBA)
  10. Standing seam zinc roof (colour TBA)
  11. PPC aluminium rainwater pipe (colour TBA)
  12. Feature gable windows with projecting PPC aluminium frames and mullions.
- Indication of smaller hotel building massing. Please refer to drawing KEP-XX-EL-DR-A-7030-0114 Proposed Elevations Sheet 03 for further information.



**South / Courtyard Elevation**  
 Scale - 1 : 200

Rev	Description	D/r	CHK'd	Date
2	Updated Entrance	RP	ES	29/10/19
1	Issued for Planning	RC	ES	05/09/19
0	First Issue For Planning	RC	ES	28/08/19

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Client  
**Vastint Hospitality B.V**

Project  
**Glebe Street Inverness**

Drawing  
**Proposed Elevations - Sheet 02**

Project No.  
**P17149**

Drawing No.  
**KEP-XX-EL-DR-A-7030-0113**

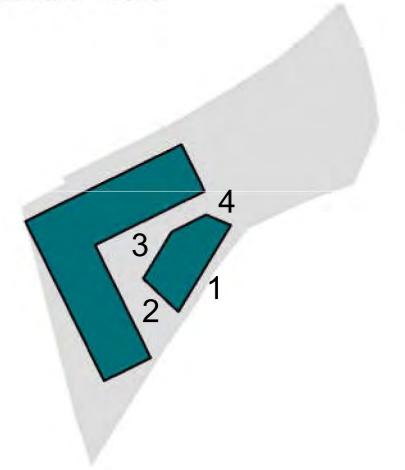
Status  
**Planning**

Created: RC 28/08/19  
 Checked: ES  
 Date: 28/08/19  
 Scale: A4 @ A1  
 Indicated





Scale 1:200



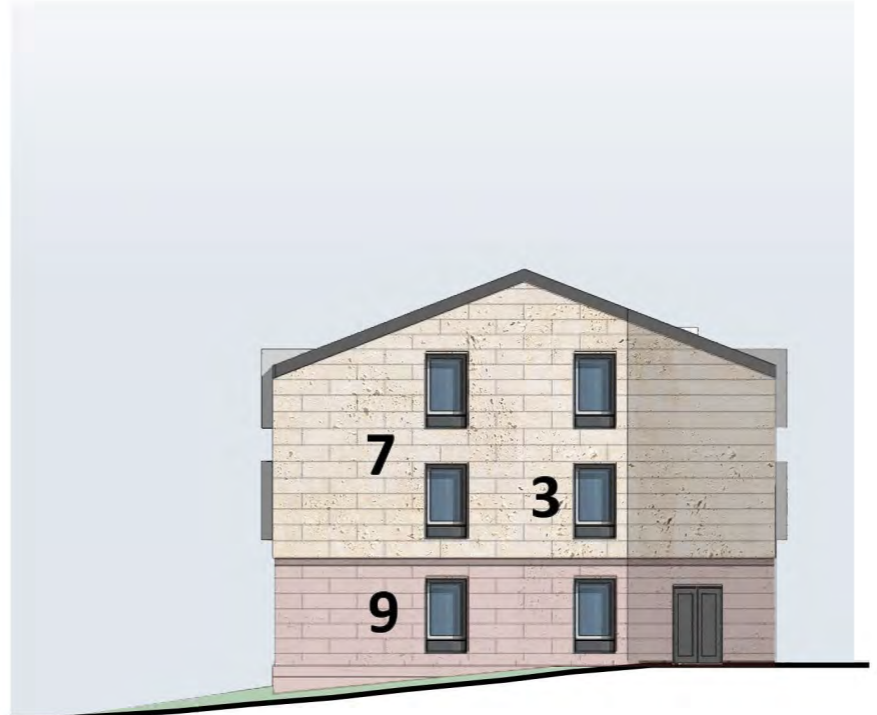
**Materials Key:**

1. PPC louvre screening to plant areas (colour TBA)
2. Low-E glazing with PPC aluminium framing, PPC aluminium projecting surround and opening vent (colour TBA)
3. Low-E glazing (recessed) with PPC aluminium framing, PPC aluminium projecting surround and opening vent (colour TBA)
4. Projecting feature dormer window with low-E glazing, PPC aluminium framing, PPC aluminium surround and opening vent (colour TBA)
5. Low-E glazing with PPC aluminium framing (colour TBA)
6. Back painted glazed spandrel (colour TBA)
7. Ceramic stone panel (colour TBA)
8. Double glazed curtain walling system with PPC aluminium frame (colour TBA)
9. Structured natural stone panel (colour TBA)
10. Standing seam zinc roof (colour TBA)
11. PPC aluminium rainwater pipe (colour TBA)
12. Feature gable windows with projecting PPC aluminium frames and mullions.

Rev	Description	Dr'n	Chk'd	Date
0	Issued for Planning	RC	ES	05/09/19



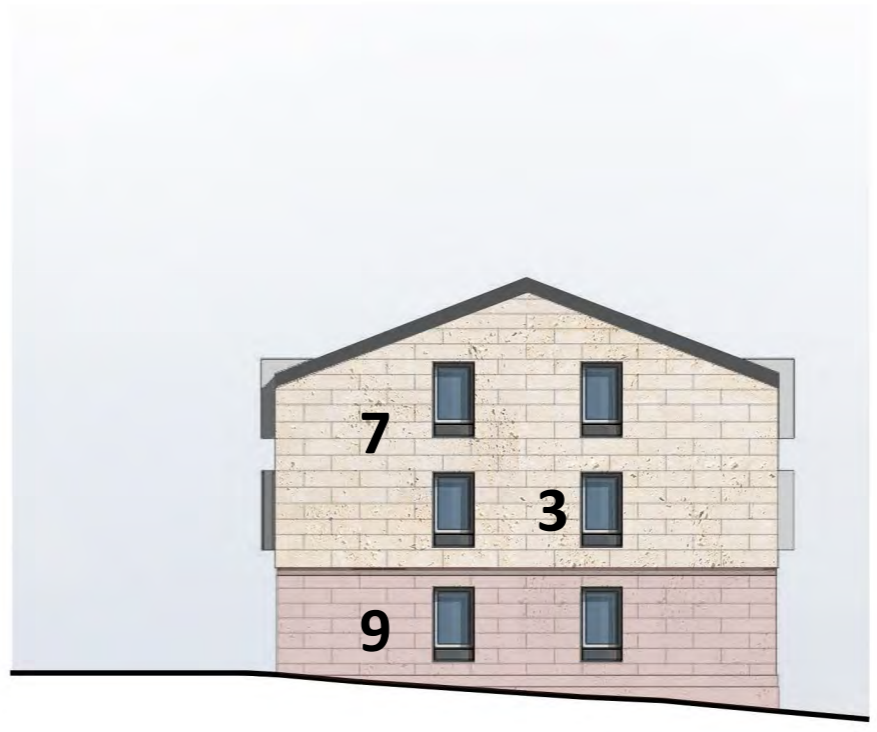
Elevation 1 Glebe Street



Elevation 4 Glebe Street



Elevation 3 Glebe Street



Elevation 2 Glebe Street

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Client  
Vastint Hospitality B.V

Project  
Glebe Street Inverness

Drawing  
Proposed Elevations  
Sheet 03

Project No.  
P17149

Drawing No.  
KEP-XX-EL-DR-A-7030-0114

Status  
Planning

Created Date: 05/09/19  
Checked Date: 05/09/19  
Checked By: ES  
Scale: As Indicated  
Rev: 0





Glebe Street Inverness-View from Friars Street looking North-West



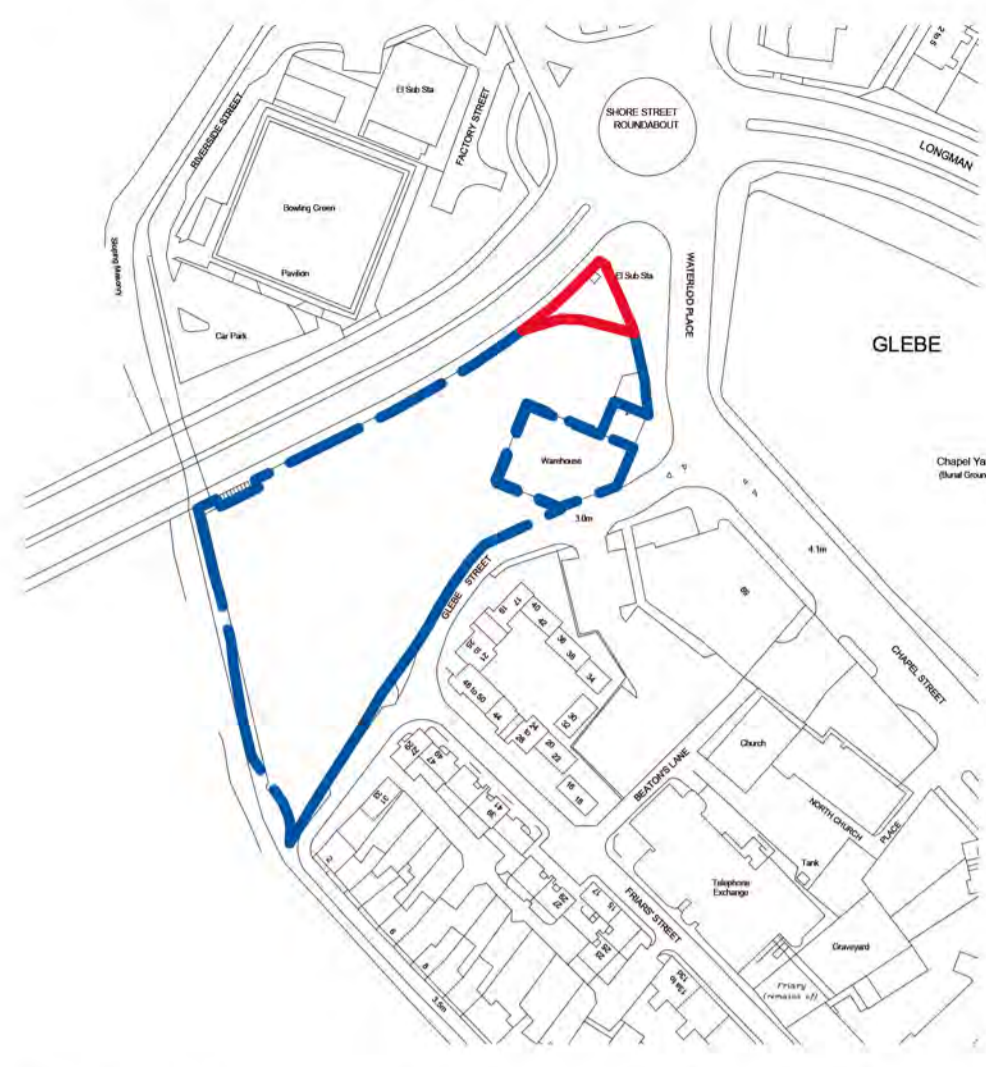


Glebe Street Inverness-View from Shore Roundabout looking South-East

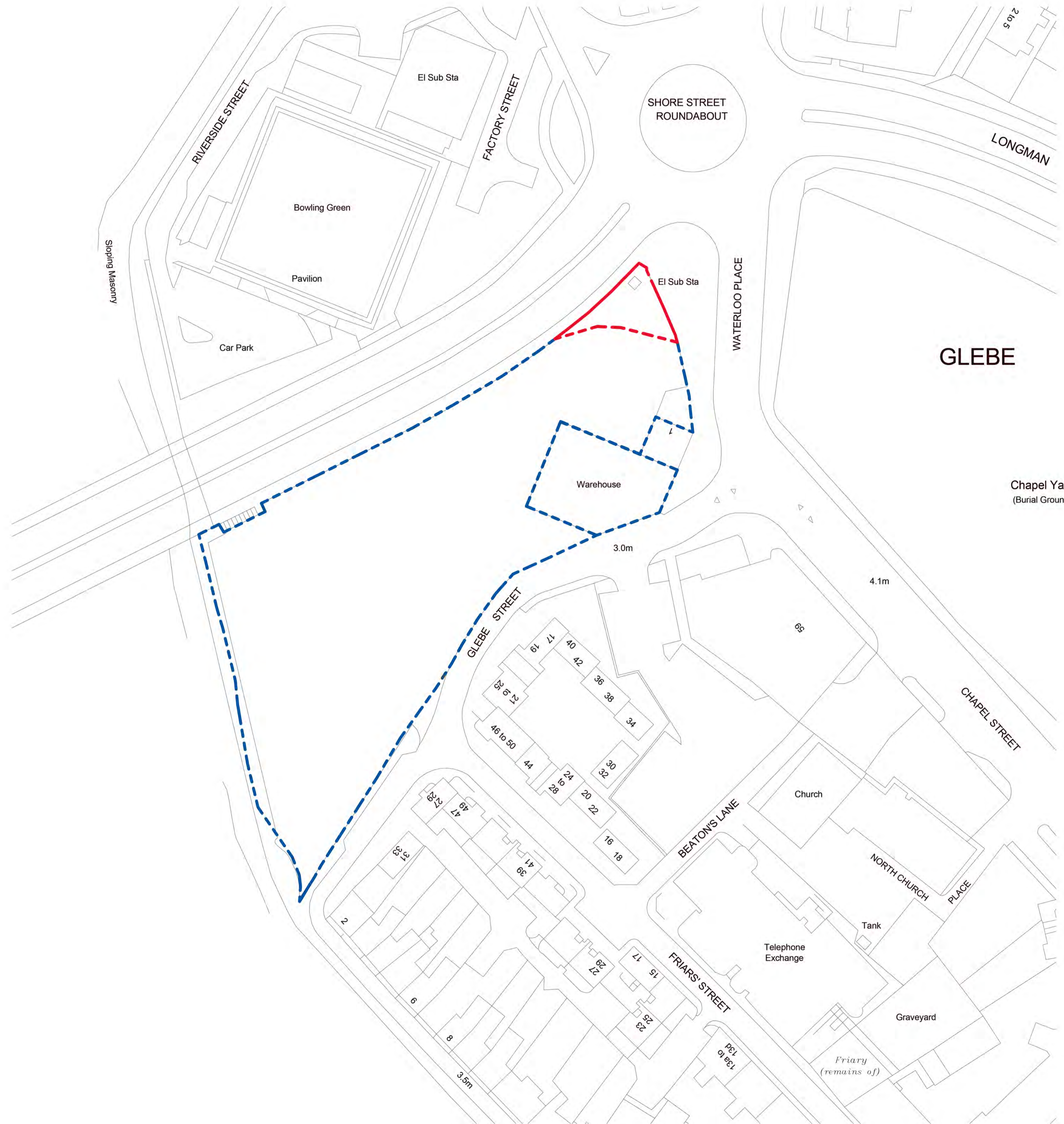
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Vastint Hospitality B.V -



**A-5020-Existing Location Plan - Substation**  
Scale - 1:2000

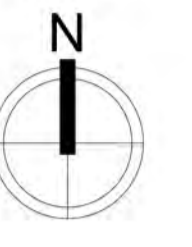
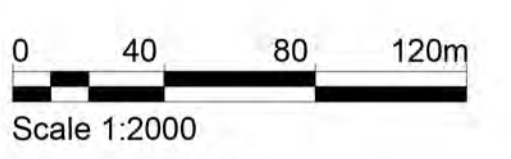
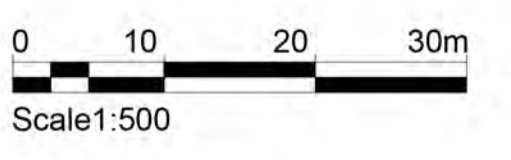


**A-5020-Existing Site Plan - Substation**  
Scale - 1:500

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Figured dimensions only are to be taken from this drawing. All dimensions are to be checked on site before any work is put in hand. IF IN DOUBT ASK.

CDM: Hazard Elimination & Risk Reduction has been undertaken and recorded where appropriate, in accordance with the requirements of "The Construction (Design and Management) Regulations 2015" and the associated "Industry Guidance for Designers"



KEY:  
--- PLANNING  
--- ADDITIONAL LAND WITHIN CLIENT OWNERSHIP

Rev	Description	D'n	CHK'd	Date
1	First Issue For Planning	RC	ES	02/09/19

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Client  
**Vastint Hospitality B.V**

Project  
**Glebe Street Inverness**

Drawing  
**Existing Site Plan & Location Plan (Substation)**

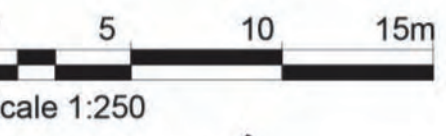
Project No.  
**P17149**

Drawing No.  
**KEP-XX-XX-DR-A-5020-0010**

Status  
**PLANNING**

Created: RC Date: 02/09/19  
 Checked: ES Scale: A4 @ A1  
 Date: 02/09/19





- KEY:
- PLANNING BOUNDARY
  - ADDITIONAL LAND WITHIN CLIENT OWNERSHIP
  - INDICATION OF EXISTING STONE WALL + METAL FENCE

Rev	Description	D/r	CHK'd	Date
2	Updated Pedestrian Route Width to 2M	RP	ES	05/11/19
1	Updated Substation Location, Updated Glebe Street Block	RP	ES	29/10/19
1	First Issue For Planning	RC	ES	02/06/19

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Client  
**Vastint Hospitality B.V**

Project  
**Glebe Street Inverness**

Drawing  
**Proposed Site Plan (Substation)**

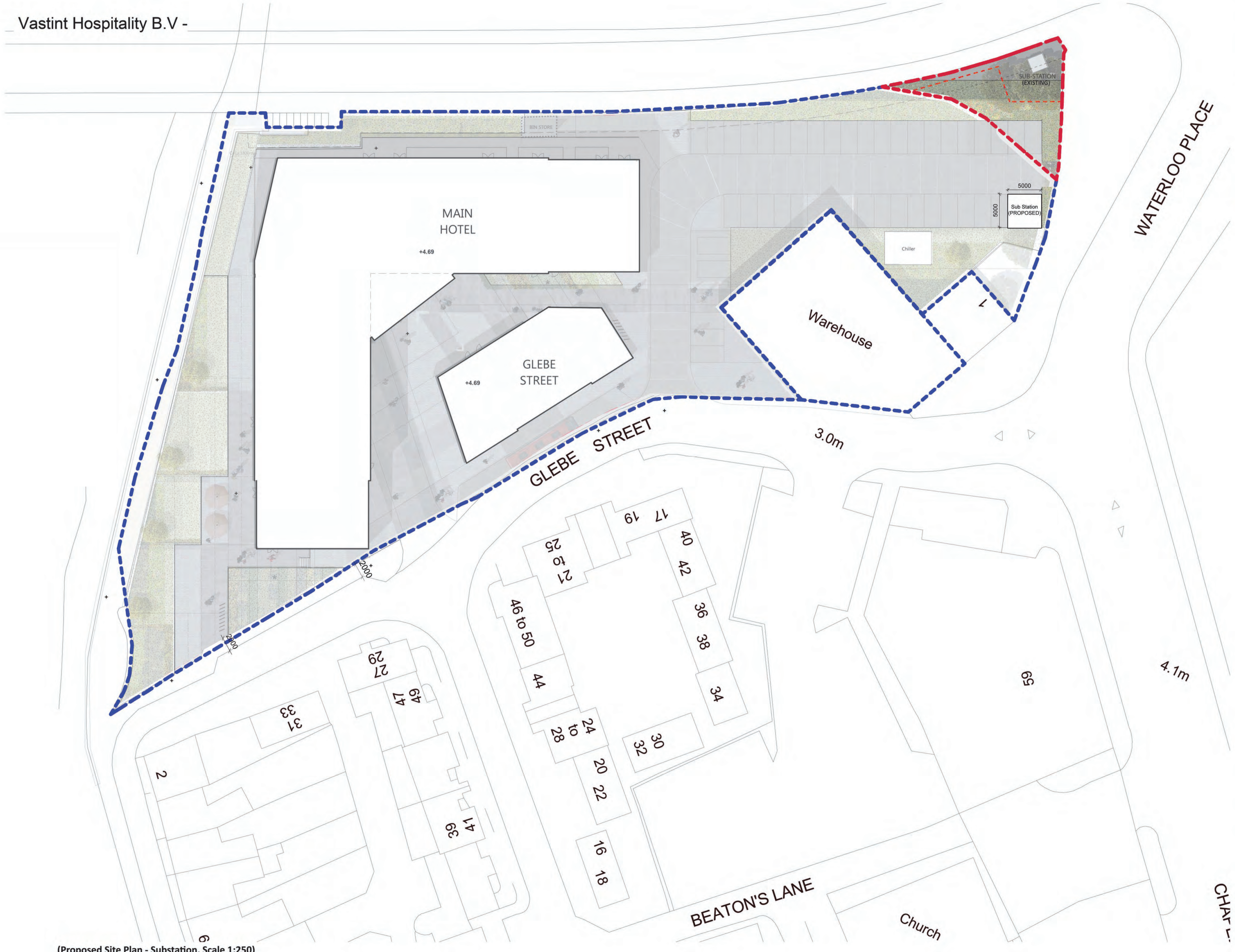
Project No.  
**P17149**

Drawing No.  
**KEP-XX-XX-DR-A-5020-0011**

Status  
**PLANNING**

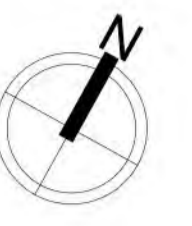
Created	RC	Checked	ES
Date	02/06/19	Scale	As indicated

(Proposed Site Plan - Substation, Scale 1:250)



CHAR





- KEY:
- - - PLANNING
  - - - ADDITIONAL LAND WITHIN CLIENT OWNERSHIP
  - - - INDICATION OF EXISTING STONE WALL + METAL FENCE

**NOTES**

**CONCRETE**  
THE CONCRETE TO BE IN ACCORDANCE WITH THE SPECIFICATION AND ATTAIN THE RELEVANT CUBE CRUSHING STRENGTH AT 28 DAYS.

**EARTHWORKS**  
PLINTH TO BE SET ON UNDISTURBED ORGANIC STRATA THAT PROVIDE THE REQUIRED MINIMUM DESIGN SAFE GROUND BEARING CAPACITY.

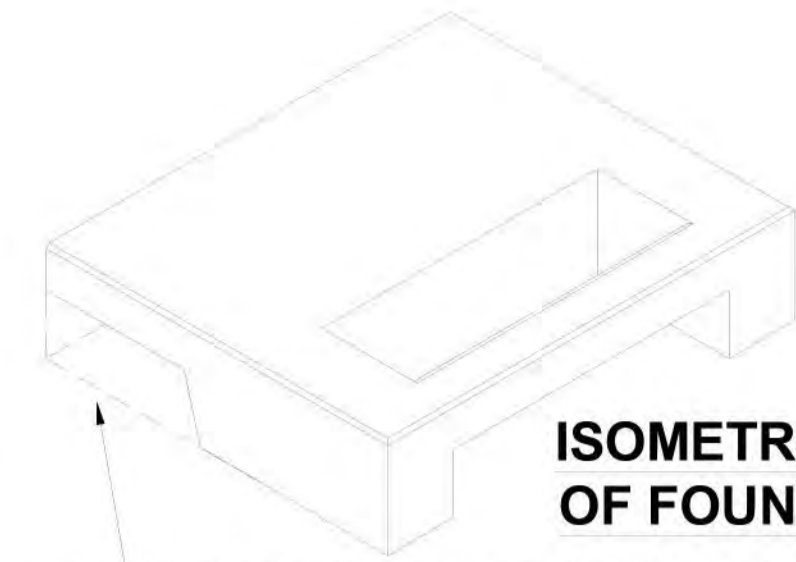
PLINTH (CONCRETE 40N/mm<sup>2</sup> 28 DAY CUBE STRENGTH) A FLAT, LEVEL AND SMOOTH FLOOR SURFACE IS ESSENTIAL FOR INSTALLATION OF PLANT. TOLERANCE TO FINISHED LEVEL EXPRESSED AS A MAXIMUM PERMISSIBLE DEVIATION BENEATH A STRAIGHT EDGE WITH FEET PLACED ANYWHERE ON THE FLOOR SHALL NOT EXCEED 1mm IN 1M OR 3mm IN 3M.

CABLE SLOT  
ON COMPLETION OF CABLING, CABLE AREA TO BE FILLED WITH DRY SAND AND TOPPED WITH 150mm DEPTH OF 20mm ROUNDED GRAVEL CHIPPINGS

FOUNDATION & FLOOR LAYOUT DETAILS INDICATED ARE TYPICAL FOR UNIT SUBSTATIONS HOUSING OUTDOOR EQUIPMENT AND WOULD NOT THEREFORE BE APPLICABLE TO OTHER SUBSTATION TYPES.

NB: All detailed drawings dimensions in mm.

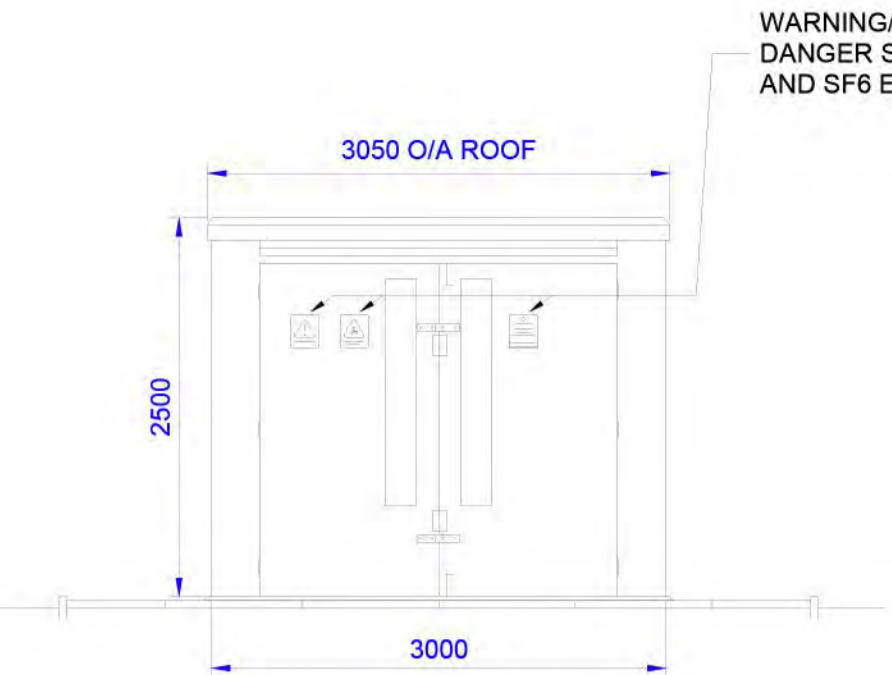
**DRAWING IS INDICATIVE AND SHOULD BE READ IN CONJUNCTION WITH CIVIL & STRUCTURAL + MEP ENGINEERS DRAWINGS.**



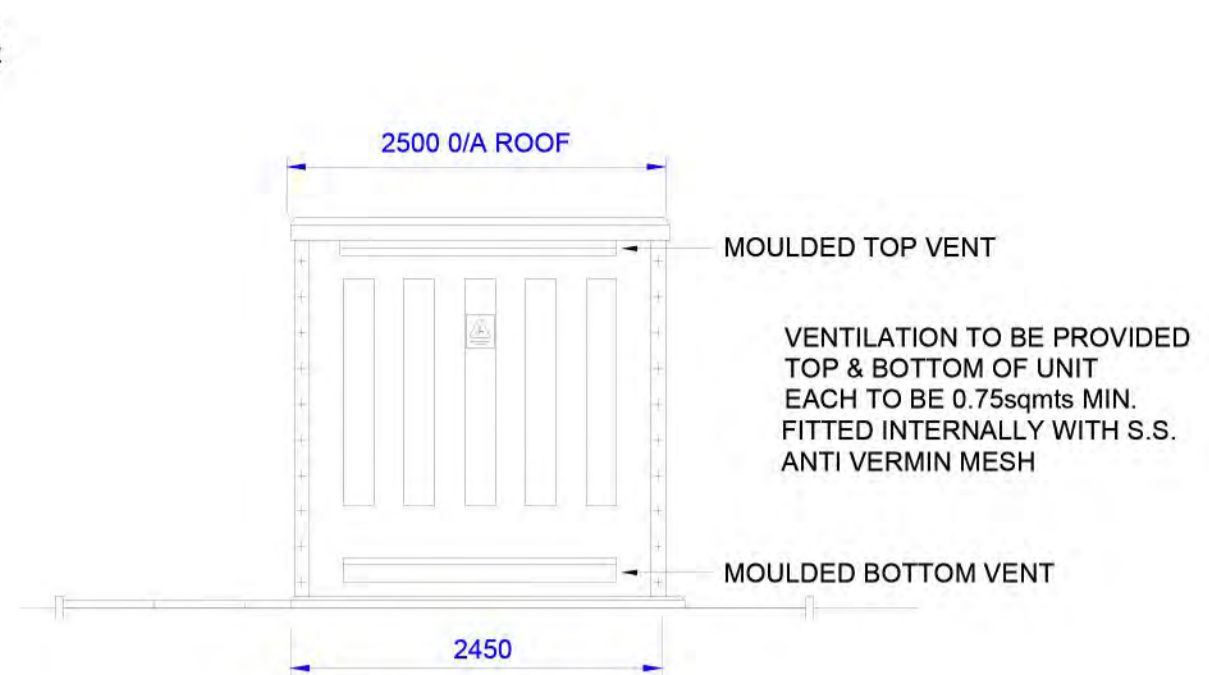
**ISOMETRIC VIEW OF FOUNDATION**

THE FOUNDATION CAN BE DEEPENED IN THIS AREA TO ALLOW FOR A SLOPING SITE OR UNSUITABLE GROUND BEARING STRATA.

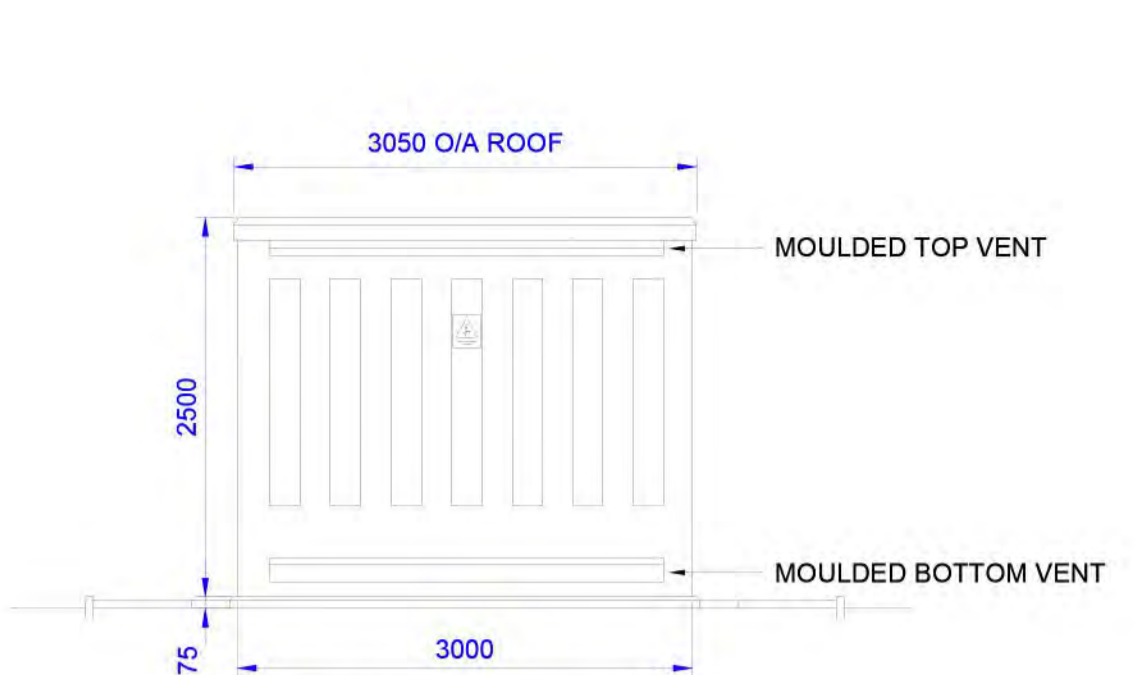
ENCLOSURE ONLY SUITABLE WHEN ERECTED AT A DISTANCE OF ONE METRE OR GREATER FROM BOUNDARY



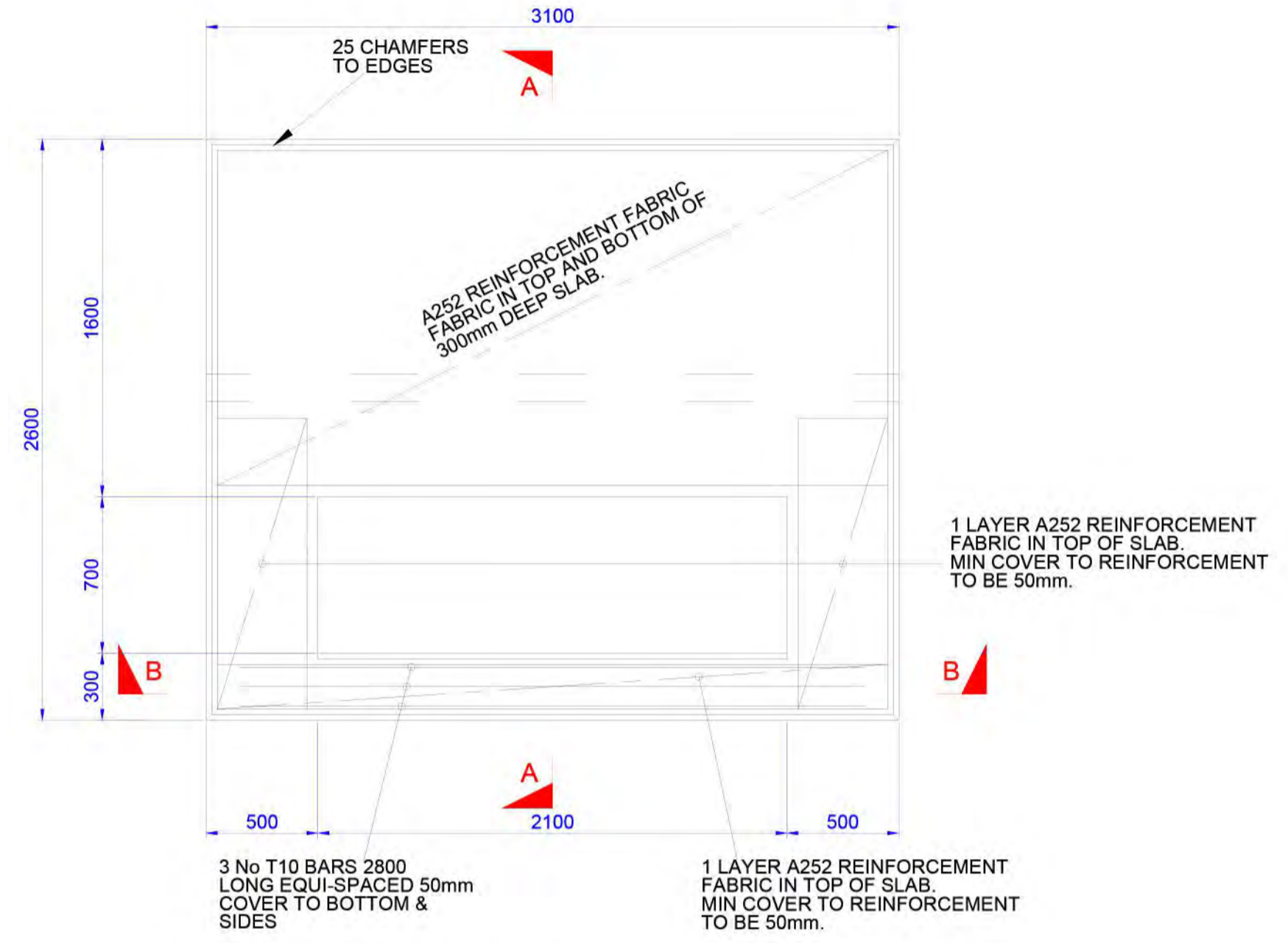
**FRONT ELEVATION**



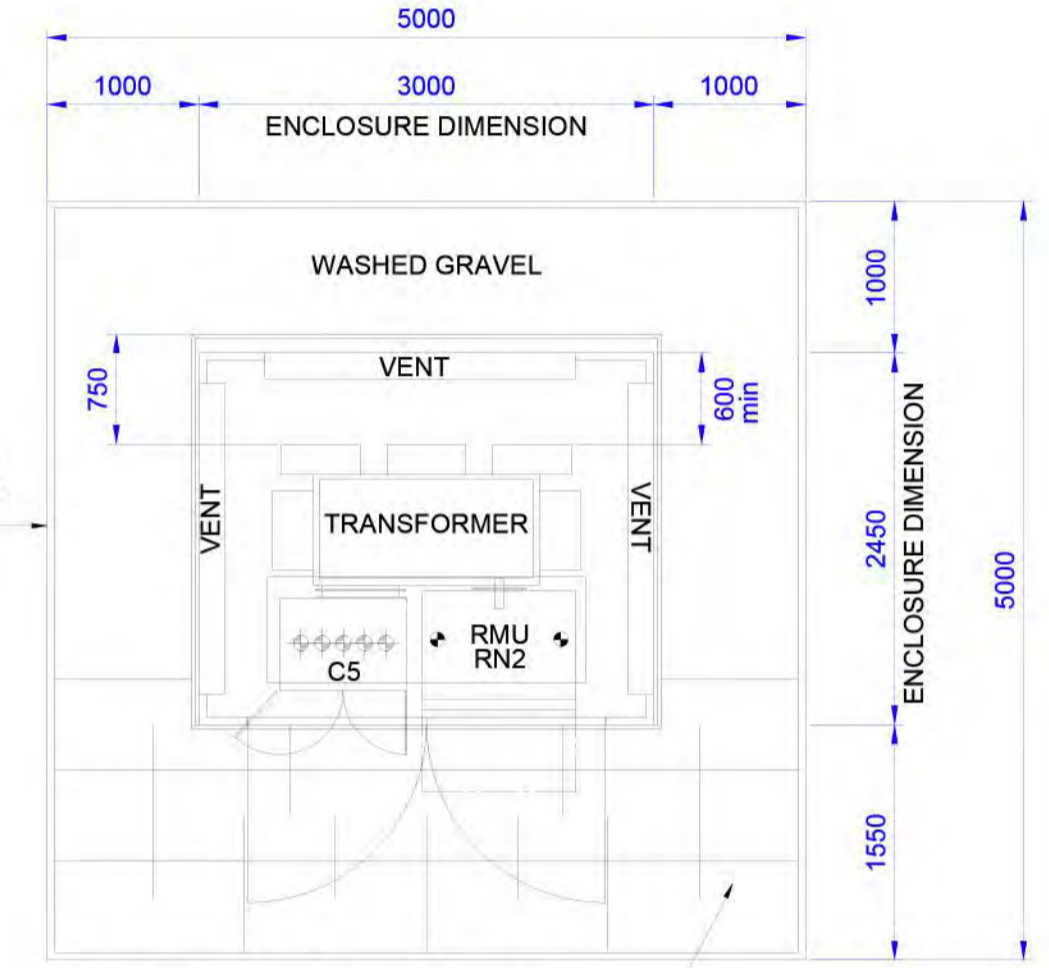
**SIDE ELEVATION (opposite side identical)**



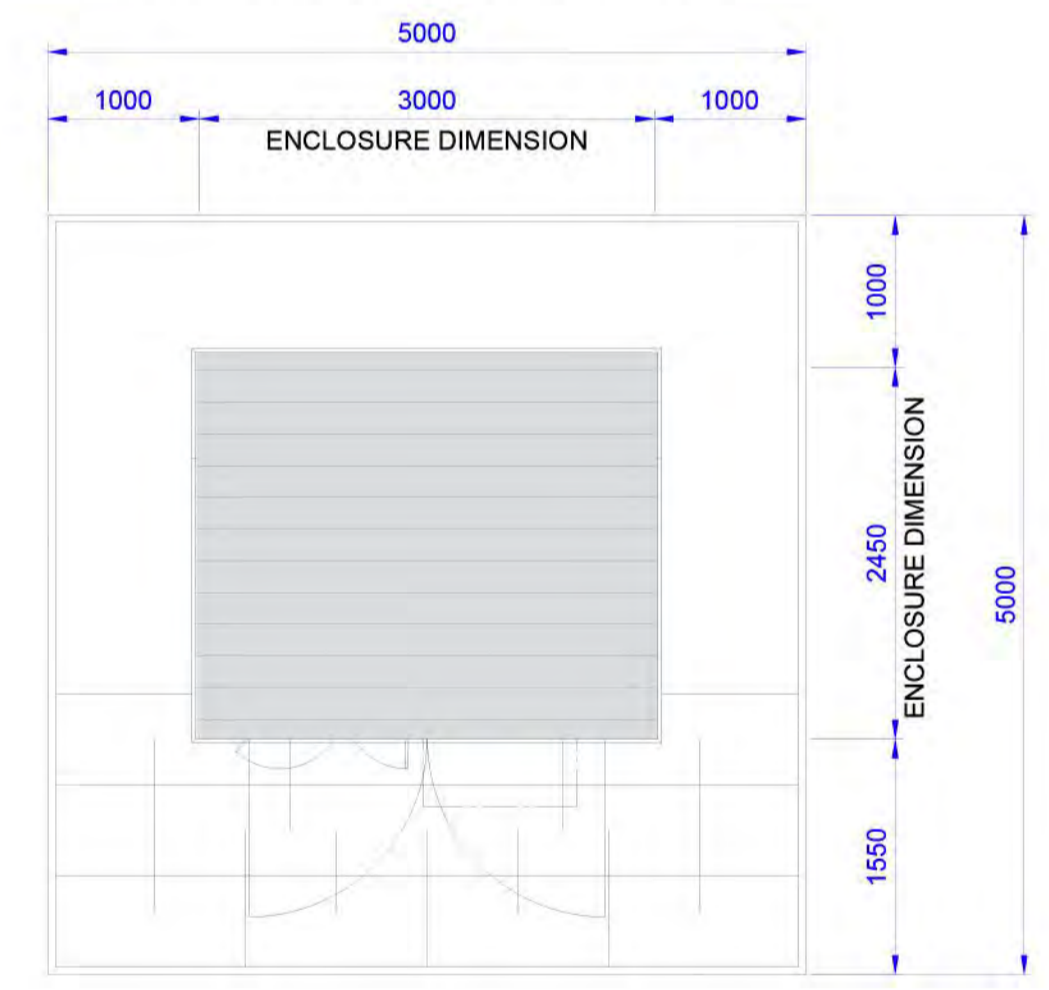
**REAR ELEVATION**



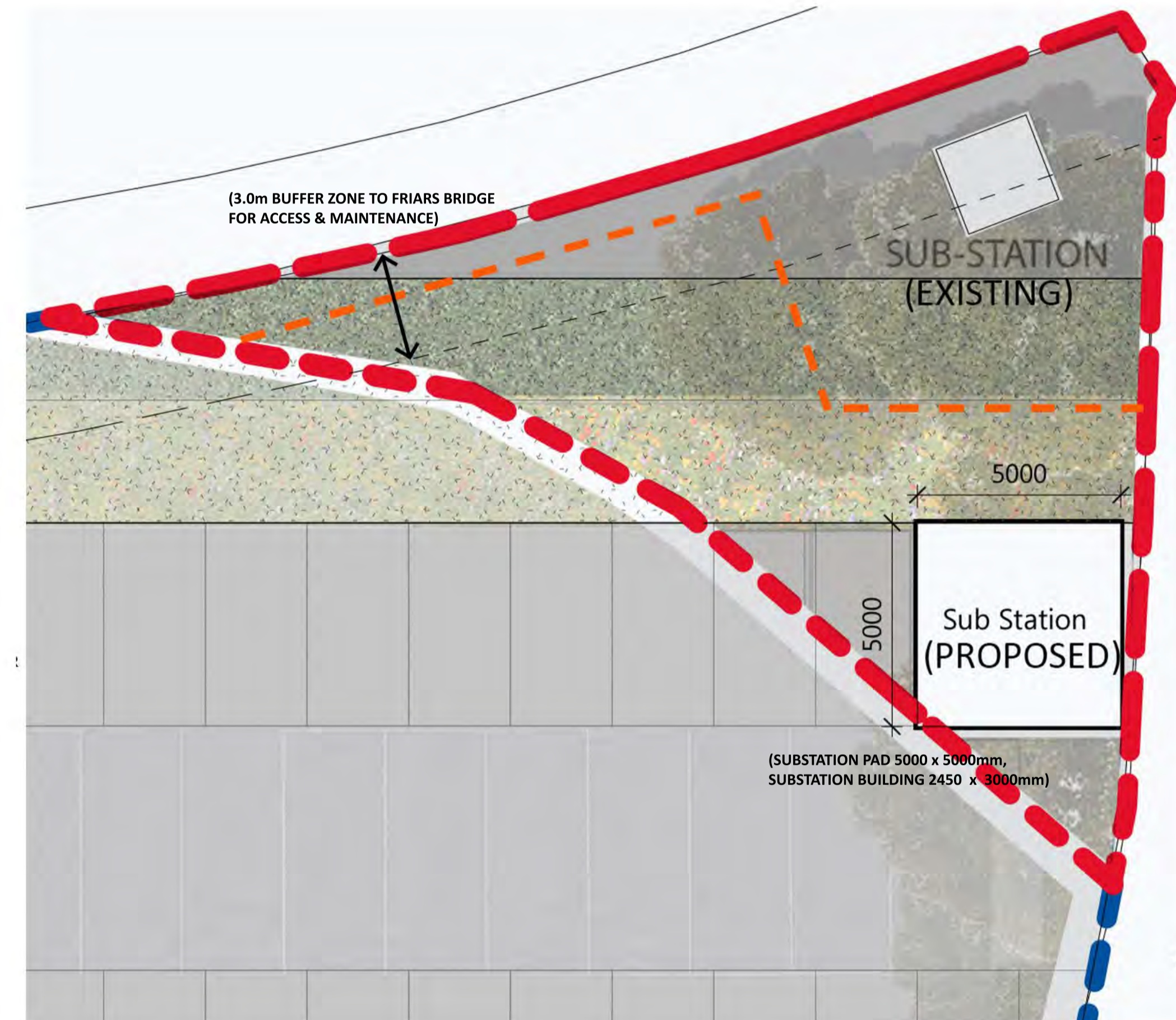
**FOUNDATION PLAN**



**SITE LAYOUT DETAILS**



**ROOF PLAN**



**BLOCK PLAN (SCALE 1:100) (ELEVATIONS & DETAILS NTS)**

Rev	Description	D'n	CHK'd	Date