

Agenda Item	6.5
Report No	PLS-068-21

## HIGHLAND COUNCIL

**Committee:** South Planning Applications Committee

**Date:** 21 September 2021

**Report Title:** 21/02124/FUL: Highland Housing Alliance  
Co-Operative Retail Services Ltd, Balmacaan Road, Drumnadrochit

**Report By:** Area Planning Manager – South

### Purpose/Executive Summary

**Description:** Erection of 15. No 2 and 2 ½ storey blocks of 2-bedroom flats with associated parking and services

**Ward:** 12 – Aird and Loch Ness

**Development category:** Local Development

**Reason referred to Committee:** More than 5 objections and Community Council objection

All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

### Recommendation

Members are asked to agree the recommendation to **GRANT** the application as set out in section 11 of the report

## **1. PROPOSED DEVELOPMENT**

- 1.1 This application for planning permission relates to the erection of 15 two-bedroom affordable flats, which are to be 2 and 2 ½ storey in height, situated on the site of the former Scotmid and Drum Takeaway to the west side of the A82 public road in Drumnadrochit.
- 1.2 Pre-Application Consultation: Pre-Application advice was sought under 20/02377/PREAPP which was initially for 17. No flats. The Pre-Application advice concluded that the principle of development was supported however this was subject to the buildings being of an appropriate scale and design with a preference for limiting the height to 2 storey blocks identified within the report.
- 1.3 Supporting Information:
- Design Statement
  - Drainage Impact Assessment
  - Supporting Statement
  - Arboricultural Impact Assessment
  - Tree Schedule
  - Tree Survey Report
- 1.4 Variations:
- Revisions to tree related information
  - Revisions to drainage information
  - Submission of swept path analysis
  - Submission of detail on EV charging
  - Submission of Waste Management Plan
  - Bicycle Storage Plans

## **2. SITE DESCRIPTION**

- 2.1 The site is located in a prominent location to the west side of the A82 in Drumnadrochit. The site is well known locally and currently comprises the 2-storey flat roofed derelict building which formally housed the local shop and takeaway and the associated car parking area. The application site covers an area measuring 2122m<sup>2</sup> which includes the existing access.
- 2.2 The site is partially bound to the north and east by the A82. Blairbeg Park (Glenurquhart Shinty Club) is located to the north east, Balmacaan Road and residential properties are located to the south, an existing access and landscaped area with trees is located on Balmacaan Road. An existing play area and carparking is located to the rear of the building.

## **3. PLANNING HISTORY**

- 3.1 17 Sept 2020 17 no Affordable Flats comprising 2 storey Closed Private Entry and 2.5 Storey Common Entry with Play Area and associated parking and services (20/02377/PREAPP)

- 3.2 There are a large number of applications on the site relating to development ancillary to the shop including signage, gas storage facilities and refrigeration plant.

#### 4. PUBLIC PARTICIPATION

- 4.1 Advertised: Unknown Neighbour – Inverness Courier

Date Advertised: 21.05.2021

Representation deadline: 23.06.2021

Timeous representations: 9

Late representations: 2

- 4.2 Material considerations raised are summarised as follows:

- a) Development Plan - Not identified for residential use in the Development Plan
- b) Siting and Design - Insensitive/inappropriate design for proposed location (scale and design of flats)
- c) Access and Servicing - Conflict between car users accessing events at Blairbeg Park and play area car park and impact to rights of access
- d) Lack of cycle and motorcycle spaces
- e) Bin storage at Balmacaan Road can cause hazard for play area users
- f) Open Space, Trees and Landscaping – impacts to existing trees
- g) Lack of garden space for residents
- h) Negative impact on the experience of visitors
- i) Planning approvals for house sites elsewhere and impacts to existing services and facilities including recreational
- j) Developer Contributions

- 4.3 All letters of representation are available for inspection via the Council's eplanning portal which can be accessed through the internet [www.wam.highland.gov.uk/wam](http://www.wam.highland.gov.uk/wam).

#### 5. CONSULTATIONS

- 5.1 **Glenurquhart Community Council object** to the proposals and comment that feedback from previous meetings with the applicant have not been reflected in the design. It highlights the following:

- IMFLDP requires that for development within Drumnadrochit that the character of the village is maintained and enhanced by quality of new buildings, layout and connectivity. The height is taller than the existing unit, there are no other high density, residential development blocks in the village and it is situated on a prominent site so will have a significant adverse visual impact.
- Cumulative impact of proposed development alongside the 94 houses proposed for Drum Farm will 'swamp' the village.
- The site is allocated for commercial use within adopted IMFLDP.
- Should note that 4m high ball stop fence may not prevent stray shinty balls damaging property and residents
- Development style not sympathetic to the surrounding properties or village location.

- Potential for impact to a village reliant on tourist income.

5.2 **Archaeology** has no objection subject to a condition. The site lies within an area of archaeological potential, there is potential for buried features or finds to be impacted by the development including burials. The nature and extent of any features shall require to be identified and recorded under archaeological supervision.

5.3 **Development Plans Team** consider that the proposal shows a degree of conformity with the development plan being a redevelopment of a brownfield site in the heart of a large village. It is currently allocated for commercial, and community uses which have or could be accommodated elsewhere in the wider settlement. The main planning policy is the development contribution to place making. The footprint and height of the proposed buildings are greater than that of those previously developed. Elevations show some attempt to break up the mass of the development through staggering and detailing of the blocks, although not from all viewpoints. The application should not result in net detriment in terms of design quality and placemaking. It is unfortunate no significant improvements have been made since the pre-application advice highlighted these issues. A policy assessment and assessment of the necessary developer contributions is included within the response.

5.4 **Flood Risk Management Team** has no objections. It is noted that SEPAs online mapping shows the site lies out with any area of flood risk during a 1 in 200-year return period storm event therefore no objections on the grounds of flood risk.

Though the FRM Team had accepted that conditions would likely be suitable for infiltration. The FRM team requested future soakaway tests to confirm. Following submission of this the FRM Team has no further requirements on this issue.

Version C of the drainage drawing appears to show that the access road would be drained via kerbing with the water being delivered to the main soakaway. Calculations appear to have been updated to include the extra impervious area (from the road). The soakaway proposal is shown to be increased to accommodate this. The FRM Team is satisfied that this issue is resolved.

Request a finalised DIA is submitted prior to construction in the event that proposals change

5.5 **Forestry Team** has no objection subject to conditions.

5.6 **Transport Planning Team** confirmed that it is accepted that the proposed development shall generate less vehicular trips than the previous shop and takeaway, therefore the development is not considered to have detrimental impact on the Council maintained public roads.

It recommends a condition which secures the location of dropped crossings and tactile paving.

It recommends a condition to secure further detail on bicycle parking as there is a shortfall of one cycle parking space for flats 5 – 13.

It was noted that no information had been submitted in regard to public transport and a developer contribution was requested for £1,500 toward the proposed real-time information portal at Loch Ness Transport hub.

Transport Planning is satisfied that disabled parking is adequate as is EV charging provision however it recommends a condition requiring information packs for residents on how to use the facilities and that an outline charging/structure payment arrangement is to be approved by the Council.

Condition recommended for the design of the refuse point and provision of a dropped crossing to be approved by the council before works start on site.

It highlights that the carry distance required for waste for blocks 1-4 does not meet with current standards.

Request a condition to secure further detail on drainage layout.

5.7 **Transport Scotland** has no objection.

## **6. DEVELOPMENT PLAN POLICY**

The following policies are relevant to the assessment of the application

### **6.1 Highland Wide Local Development Plan 2012**

- 28 - Sustainable Design
- 29 - Design Quality & Place-making
- 31 - Developer Contributions
- 32 - Affordable Housing
- 34 - Settlement Development Areas
- 42 - Previously Used Land
- 51 - Trees and Development
- 56 - Travel
- 64 - Flood Risk
- 65 - Waste Water Treatment
- 66 - Surface Water Drainage
- 75 - Open Space

### **6.2 Inner Moray Firth Local Development Plan (2015)**

Policy 2 – Delivering Development

Site DR8: Retail Units on the A82/balmacaan Road – allocated for mixed use

### **6.3 Highland Council Supplementary Planning Policy Guidance**

- Developer Contributions (March 2018)
- Flood Risk and Drainage Impact Assessment (Jan 2013)
- Open Space in New Residential Developments (Jan 2013)
- Public Art Strategy (March 2013)
- Sustainable Design Guide (Jan 2013)
- Trees, Woodlands and Development (Jan 2013)

## **7. OTHER MATERIAL POLICY CONSIDERATIONS**

7.1 Highland Council Roads and Transport Guidelines for New Developments (May 2013)

## 7.2 **Scottish Government Planning Policy and Guidance**

Scottish Planning Policy (as amended December 2020)

National Planning Framework 3

Designing Streets

Creating Places

## 8. **PLANNING APPRAISAL**

- 8.1 Section 25 of the Town and Country Planning (Scotland) Act 1997 requires planning applications to be determined in accordance with the development plan unless material considerations indicate otherwise.

### **Determining Issues**

- 8.2 This means that the application requires to be assessed against all policies of the Development Plan relevant to the application, all national and local policy guidance and all other material considerations relevant to the application.

### **Planning Considerations**

- 8.3 The key considerations in this case are:
- a) compliance with the development plan and other planning policy
  - b) siting and design
  - c) open space, landscaping and trees
  - d) access and transport
  - e) infrastructure and servicing
  - f) any other material considerations

### **Development Plan and other planning policy**

- 8.4 The development plan comprises of the Highland wide Local Development Plan (HwLDP) and the Inner Moray Firth Local Development Plan (IMFLDP). The application site lies with the Settlement Development Area (SDA) of Drumnadrochit and is on land allocated for development within the IMFLDP for mixed use; comprising retail, business and community use (DR8).
- 8.5 The proposal is for housing, a use that does not form part of the allocation. Having said that, the development is within the SDA, where Policy 35 of the Highland wide Local Development Plan supports proposals that meet the requirements of Policy 28, are compatible with the existing pattern of development and uses and have no significant detrimental impact on natural, built and cultural heritage. There are no such features likely to be impacted by this development, but the site does contain trees, the impact on which will need to be assessed.
- 8.6 In the case of Drumnadrochit, the development plan sets out that this settlement 'has more facilities than would be expected for a village of its population.' So, while the Inner Moray Firth Local Development Plan seeks to safeguard the site for

commercial or other footfall generating uses, there are other, sufficient recent, proposed and/or allocated development sites to cater for local demand for these uses and therefore this "change of proposed use" is not considered to be a significant policy issue in principle. Further the applicant submits that there is a clause within the sale of the land that it shall not be used for a commercial purpose in order to protect the interest of the previous site owner, therefore it should be noted that, while not strictly compliant with the site allocation, the type of development which could be delivered on site appears to be restricted.

- 8.7 The site is a previously used site and therefore constitutes a beneficial reuse of a brownfield site in accordance with HwLDP Policy 42 - Previously Used Land. As the development is for low-cost housing, the proposal will also provide a much-needed supply of affordable homes. The type of affordable housing has not yet been set by the applicant. It is expected that they will market 6 or 9 of the properties for midmarket rent with the remainder being social housing. They advise that the last 2-bedroom property that was marketed by them received 20 notes of interest, which suggests that there is a demand for such properties in the village. This has been confirmed by Housing who indicate that there is currently demand for 25 2-bedroomed flats within the area.
- 8.8 Policies 28 Sustainable Design and 29 Design Quality and Placemaking assess all development against a list of criteria including potential impacts on individual and community residential amenity, demonstration of sensitive siting and a high-quality design which is in keeping with local character and makes use of appropriate materials. All applications require to make a positive, net contribution to the architectural and visual quality of the place within which it is proposed.
- 8.9 The other policies listed in paragraph 6.1 are relevant to this proposal and must be given due consideration.
- 8.10 Subject to the proposal demonstrating sensitive siting and a high-quality design in keeping with local character, having no significantly detrimental impact on individual and community residential amenity and capable of being adequately serviced then the development would comply with the development plan.

### **Siting and design**

- 8.11 The site is situated at a prominent location on the west side of the A82 in Drumnadrochit, the main thoroughfare in the village and a well-used tourist route utilised by those visiting Loch Ness, Urquhart Castle and beyond. To give an idea as to the number of people who may pass the site, Urquhart Castle recorded 400,000 visitors in 2019. While not all visitors would necessarily have passed through the village, it is likely a far larger number of people pass the site to access other recreation and tourist facilities around Loch Ness year-round. It is noted that concern has been raised over tourism interests of the village arising from the impact of the scale and design of the buildings. While a large number of visitors pass the site, it is not considered likely that the development would impact detrimentally on visitor numbers. It is likely that visitors would be passing through the village in any case, irrespective of the condition of the site. The number of visitors does however place emphasis on the weight given to visual amenity in terms of the siting, design, and external appearance of the buildings. This is emphasised in the site allocation within

the IMFLDP which states that *the redevelopment of the site shall require a high-quality design which respects the sites prominent tourist location.*

8.12 Pre-Application Advice was formally sought in July 2020 and although this related to a larger site area, taking in land to the west of the site where the play area and car park are currently situated, the proposals remain largely similar. The Pre-Application Advice provided to the applicant advised that the differing roof heights proposed appeared incongruous and that the massing was out of scale with any other development in the village giving rise to concerns over visual amenity. It was advised that it was unlikely that the 2 ½ storey element would be supported.

8.13 Since then, the applicant has endeavoured to work with officers, the Community Council and relevant stakeholders in order to achieve a development of reduced scale in terms of massing, including development over a larger site area. Several design iterations have been considered. However, a suitable compromise has not been possible. The applicant's response to this is:

*'The reason for reverting back to the original proposal was due to the lack of funding available which meant the Community Council could not proceed with the revised plans involving the relocation of the car park and play area. There was also a strong desire for the play area and parking to remain in their current locations given this is well used by the local community and more remote from the A82 which was also our preferred approach.'*

8.14 The development is read as three separate blocks laid out in a largely L-shaped plan. All buildings are oriented toward, and adjacent to the footway to, the A82 to the east, and Balmacaan Road to the south east. A proposed car parking area is situated behind the site, between the proposed flats and the existing playpark/car parking area.

8.15 These three blocks can best be described as being made up of a 2-storey block of 2 private entry cottage flats, a 2½-storey central block containing 9 flats and a further 2 storey block containing 4 communal entry flats.

## **2. No 2-storey private entry cottage flats (units 14 and 15)**

8.16 This, the smallest structure, is located to the southern corner of the site and accommodates two flats. The flats will overlook the car park and play area and have a frontage to Balmacaan Road which overlooks the existing landscaped area to the south east of the site. The building is similar in scale to the semi-detached row of housing on Balmacaan Road on the opposite side of the access into the site. This building is 8.5m in height to the ridge.

8.17 The entrance to the properties is taken from the rear (car park) with the upper floor accessed via an external staircase. This results in this development having no active frontage onto Balmacaan Road unlike the neighbouring properties to the west. While it would be preferable for residents to have covered access to upper floor accommodation, the proposed stairway will not be largely visible from within the wider public domain so is unlikely to present a significant issue in visual amenity terms. It could however give rise to some concern over a lack of natural surveillance



to the entrances to the flats. Notwithstanding, it is considered that the siting, scale and design of this block is appropriate to the site.

#### **9. No 2.5-storey flatted block (units 5 - 13)**

- 8.18 The largest of the three blocks sits centrally within the development. It is a corner block that addresses both Balmacaan Road and the A82. This block will contain 9 two-bedroom flats.
- 8.19 This block has a rough L-form plan with a stepped arrangement along the rear wall, resulting in a deeper plan form at its southerly end. This results in a roofline that is stepped; the highest part of which is over this southerly section. The ridge line sits 13.5m at its highest point, which is almost 6m above the existing building on site and 5m higher than the adjacent 2 storey block. It then drops down to continue the remaining length of the building.
- 8.20 The corner of the building has a hipped roof punctured by a stone clad corner feature wall with flat roofed projection in a wall head dormer style. The southern end of the Balmacaan Road elevation incorporates a similar feature that bookends this elevation. A further wall head dormer feature, although finished in render, provides additional visual interest. The remaining upper floor windows are set into the roof. These features help to provide a sense of modelling to what is essentially a flat wall.
- 8.21 The rear of the 2 ½ storey flats overlook the proposed car parking area and Blairbeg Park. To this side, the roof mass to the north west facing elevation and differing roof pitches is evident. A large internal glazed stairwell is proposed to this elevation. This is an effective way to break up the overall mass of the external wall from this side.
- 8.22 Concern has been raised over the potential for shinty balls to hit windows on this elevation. In response to this, the applicant has confirmed that there is a 40m offset from the existing 4m high fence. In addition, the glazing will be toughened/laminated and therefore resistant to breakage. The risk of damage to parked cars or windows is to be further mitigated by inclusion of a secondary fence that will extend the north west boundary, which would be an improvement over the current fencing arrangement on site. The new ball fence would be maintained by the applicant under a factoring agreement.

#### **4. No 2-storey communal entry flats (units 1 – 4)**

- 8.23 The final block comprises of two structures, one stepped back from the other, with a glazed link covered entrance stairway that results in a staggered H-shaped plan form. The principal elevations are to the A82 and to the northern corner of the site, although the main entrance is to the car park side. This block partially screens and thereby reduces the visibility of the main block in views to the south.
- 8.24 The principal issue raised in representations, including those from the Community Council, relates to the scale and design of the flatted block given that it will be the dominant building on the site.
- 8.25 The applicant has provided examples of larger structures within the village in order to justify the scale of the proposals. There are larger buildings, such as the Loch

Ness Exhibition Centre and the former Bank of Scotland, although these are largely traditional structures which have unique architectural or historic interest. There are no contemporary structures of this scale. Having said that, of all the sites in the village this is perhaps the only one where this scale of building can be accommodated without dominating surrounding property.

- 8.26 The depth of plan and resultant scale of the roof of the southern part of the main block is considered to present the greatest challenge. This results in a perception of a building of three storeys, rather than 2 ½, increases the overall mass of the roof and results in an odd roofline relationship in views from the parking court, from the front (Balmacaan Road elevation) and from the west.
- 8.27 Amending this to draw in the footprint and lower the roof height, or indeed dropping the height of the block as a whole, would result in a reduction in floorspace and the number of units to a level that would likely to render the development uneconomic.
- 8.28 The materials across each of the buildings includes stone cladding, render and a grey concrete roof tile. The materials provide cohesiveness to this group of buildings and are largely appropriate in an urban setting. However, some elements could be improved, particularly given the visual prominence of the site, such as the use of a high-quality render as opposed to dry dash render. Such details could be secured as a condition to any permission.
- 8.29 The building is sufficiently separated from adjoining residential premises to ensure that there will be no adverse impact on individual or community residential amenity. While representations raise the prospect of overlooking of the play park, visibility will be oblique rather than direct albeit having a development of this nature built here will enhance natural surveillance and provide for a safer environment overall.

### **Open Space, Landscaping and Trees**

- 8.30 The site is relatively constrained by the footprint of the proposed flats and the car parking requirements. This limits the amount of open space which is proposed within the site. The Council's Open Space Calculator, for Inverness and the surrounding area, requires that a 15-unit affordable housing development should generate a minimum of 1228m<sup>2</sup> provision of all types of green space. Given the constrained nature of the site only 440m<sup>2</sup> can be delivered. To off-set this shortfall a £10,000 contribution toward upgrade and maintenance of the existing play area adjacent to the site has been agreed.
- 8.31 While concern has been raised around a lack of garden space for residents, the impact to residential amenity arising from this is not considered to be significant in this instance. The site benefits from surrounding open space, including a park and recreational facility adjacent, and takes advantage of what is a relatively treed landscape setting on its doorstep. Residents will be aware of the lack of private space within the site prior to moving in and having no garden could be considered a personal choice.
- 8.32 The hardstanding will be a combination of permeable pavements and tarmac with the soft landscaped areas will be laid to grass. A natural stone wall is proposed along the A82 to reflect the character of the setting and soften this edge. It is proposed

that this shall form public art for the site, a condition is therefore recommended to secure further detail on the design of this.

- 8.33 The existing trees that lie outwith the site boundary on the south-eastern side of the development will not be impacted by the development. Unfortunately, those on the north side of the access road will be removed. Additional tree planting is proposed around the perimeter of the site. The Forestry Officer has no objection to the proposal.

### **Access and transport**

- 8.34 The site is to be accessed via an existing access that formally served the shop. This is to be upgraded to bring the road which currently serves the play area car park, and Blairbeg Park to an adoptable standard. This is considered an improvement to the existing situation on site.
- 8.35 Representations highlight concern over conflicts between residents, users of the play area car park and those accessing Blairbeg Park, including when there are events on. It is not proposed to restrict or impede any existing rights through the proposed development. It is noted and accepted by Transport Planning that the proposed residential use shall likely generate less vehicular trips than the current use of the site. It is therefore considered that there is less chance of conflict between potential affected parties and there are not considered to be any issues in this regard.
- 8.36 Transport Scotland has been consulted on the proposals as the revised access point joins the A82. No objections have been raised and their consent will be required to carry out works to the access within the verge. There are not considered to be any issues around access to the site.
- 8.37 There are 23 car parking spaces proposed. Transport Planning note in its response that this meets with Council requirements for communal parking, which requires 1.5 spaces per dwelling and 2 all-accessible parking bays. It is noted within representations that there are no motorcycle parking bays. The requirement for such spaces is assessed on a case-by-case basis and it would not appear that this development would generate any substantiated need for motorcycle parking over and above any other residential development of this scale. Motorcycles owned by residents can be accommodated within the dedicated parking bays proposed.
- 8.38 Concern is also raised in representations around the potential for residents parking to be compromised by users of Blairbeg Park. Use of resident's spaces for such purpose falls out with the remit of planning and is up to the developer and individuals to manage.
- 8.39 A lack of cycle parking is highlighted as an issue in the representations. 15 spaces were proposed initially, comprising 1 covered area per flat which meets with the Council Roads Guidelines. Following Transport Planning's consultation response an additional Sheffield bike stand is proposed to accommodate two visitor cycle spaces. This is considered acceptable. The application layout offers reasonable active travel connectivity within and beyond the site.

- 8.40 An Electric Vehicle Charging Strategy was submitted at the request of Transport Planning. This denotes the location of cabling around the entire car parking area, futureproofing the ability of the site to accommodate electric vehicle parking when demand increase. It is currently proposed to have 8 EV car charging points on completion of the development. Transport Planning has recommended a condition which secures detail on packs to be given to residents detailing use and instruction on use of the EV charging points; this shall be secured by condition as requested.
- 8.41 The Transport Planning Team recommended that a contribution of £1,500 should be secured to fund the proposed real-time information portal at the Loch Ness Transport Hub. The applicant has agreed to cover this via an upfront commuted sum.

### **Infrastructure and servicing**

- 8.42 It is noted within representations that concern is raised over the bin storage and waste management for the site, including the impact that this could have visually to Balmacaan Road. Concern was also raised around the position of this being a hazard to play area users. A Waste Management Plan was submitted at the request of Transport Planning. This clarifies that there are to be two bin stores. These will be positioned adjacent to the access to the car parking area and shall accommodate storage for two 240 litre bins for each property. The larger of the two bin stores shall have a cope rendered wall surround to reduce visual impact, the other store which shall contain 8 bins shall be finished with a timber fence surround. The storage areas are located out with any visibility splays and shall be screened to reduce visual impact; this is therefore not considered to be a significant issue. It is not considered that the bin store will have an impact or cause a hazard to users of the play area as it is situated within the site, with the exception of collection days. It doesn't appear that the bin store and collection would have an increased impact over the current lawful use of the site where there would be bin collection and deliveries to the shop on a more frequent basis than a once per week bin collection.
- 8.43 As the access road is not to be adopted, an area of hardstanding is proposed for bins to be taken by residents on collection day, these are located out with any visibility splay.
- 8.44 It should be noted that brown bins have not been included as there is no individual garden space, with a factor to maintain any green areas this is not considered to be necessary; such waste will be dealt with offsite. Overall, the waste management strategy for the site is acceptable.
- 8.45 A Drainage Impact Assessment has been submitted which clarifies the drainage strategy for the site. It is proposed to drain surface water via permeable block paved areas and discharge any water by infiltration. It is confirmed that there will be no runoff to the existing combined sewer from the development. Water on the access roads will cross fall and discharge into the parking and permeable areas, with attenuation and infiltration of this being dealt with, within the site. The proposal should not impact or exacerbate any existing issues in the area. The paving is all to be maintained by the applicant via factoring. It is therefore considered that the drainage solution for the site is appropriate.

- 8.46 It is proposed to construct new foul sewers to connect to the existing combined sewer. The applicant has submitted evidence of a Pre-Development Enquiry from Scottish Water which confirms that there is sufficient capacity within Glenconvinth Water Treatment Works and Drumnadrochit Wastewater Treatment Works. Connections to Scottish Water infrastructure will require direct application to Scottish Water, there are no known planning issues with this. The Flood Risk Management Team and Transport Planning have no objections to the proposals however request that a 'finalised' Drainage Impact Assessment is secured by condition in order there is an opportunity to further review this prior to works commencing on site.
- 8.47 The upgrade to the junction with the A82 will offer an improvement to existing and future users of the access. Transport Scotland has no objections to the proposals. It has been demonstrated that adequate car (including EV charging points) and cycle parking is accommodated within the site; it is not considered that additional spaces are required for motorcycles. The Drainage and Waste Management strategies for the site have also been assessed as appropriate. It is therefore not considered that there are any technical or policy issues in regard transport, access and servicing.

### **Developer Contributions**

- 8.48 The site lies within the catchment area of Glenurquhart Primary School which is currently projected to breach its 90% physical capacity threshold requiring a two-classroom extension for which developer contributions will be sought. For secondary school provision the site lies within the catchment area of Glenurquhart High School. It has no forecast capacity issues and no secondary education contribution is sought. Transport and Open Space contributions are required as detailed below and public art shall be secured by planning condition.

### **Other material considerations**

- 8.49 None.

### **Non-material considerations**

- 8.50 Representations received consider that alternative uses for the building should be pursued in preference to this development. While it is recognised that this may be a sustainable solution for buildings that are not at their end of life and are capable of conversion there is currently no requirement for an alternative re-use to be considered in the planning process. The application currently under consideration needs to be considered on its merits in accordance with the development plan.
- 8.51 Concern has been raised that there has been no wider community consultation. In advance of the submission of the planning application, the applicant was in consultation with a number of stakeholders to establish a design and layout for the site. There is no legal obligation to consult the wider community for a local scale development proposal however there has been pre-application consultation with representatives of the wider community through the Community Council. The application has also been advertised with 11 comments in total received, including from stakeholders and individual members of the public.

- 8.52 Comments received dispute that the land is wholly within the applicant's ownership. A landownership certificate has been submitted, as required, which states that the applicant owns all of the land. It is accepted that part of the application site is also within the road verge, however. Landownership is a civil matter between interested parties and not a material planning consideration.
- 8.53 Concern is raised that the site could impact the ability to develop Blairbeg Park. The development is contained on the former shop/takeaway site and does not encroach onto Blairbeg Park. It is not considered that the development would impact the ability of any future development on the adjacent park, even so, the potential for development, or not is currently speculative as there are no plans or approvals for the area outside the site to assess the proposals against.

### **Matters to be secured by Upfront Payment**

- 8.54 In order to mitigate the impact of the development on infrastructure and services the following matters require to be secured prior to planning permission being issued:
- a) Primary education contribution for 2 classroom extension to Glenurquhart Primary School
  - b) Transport contribution toward real time information portal at the Loch Ness Transport Hub
  - c) Open space contribution to upgrade play facility adjacent to the site

Further details are contained within Appendix 2.

## **9. CONCLUSION**

- 9.1 The proposed development is for 15 two-bedroom affordable flats situated within the defined settlement development area of Drumnadrochit where there is a presumption in favour of development where proposals can be seen to comply with all other applicable policies of the development plan.
- 9.2 The proposal relates to redevelopment of brownfield land that is situated in a well-connected location within the centre of the village. The existing building, which is situated in a prominent position and visible from the A82, is not considered to be of any significant architectural value. While the site is currently allocated for mixed use development the principle of developing the site for low cost/affordable housing can be supported given the options for redevelopment of the site are limited and that there appears to be a strong demand for this type of accommodation in the area.
- 9.3 There are no unresolved technical issues relating to access, servicing or trees and open space. Any other outstanding matters can be secured by the recommended planning conditions.
- 9.4 The most significant issue with this development has been the design, particularly the resultant height and mass. The applicant has, through various iterations, attempted to create a deliverable development, which included utilising some of the available adjacent land, that could meet with the aspiration of the community. It has not been able to reach consensus. What is considered good design is often a subjective judgement. While there are aspects of this development that could be

improved, some of which is possible through condition, it is considered that this location can support a building of this stature.

- 9.5 Drumnadrochit is a growing and much sought-after location to live which increases the demand for local affordable housing options. There needs to be supply to meet that demand. It is considered that the needs of the wider local community who require affordable housing out-weigh the issues and concern around the visual impact of the development. It is recommended that planning permission be granted, subject to conditions and payment of the relevant developer contributions.
- 9.6 All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

## 10. IMPLICATIONS

- 10.1 Resource: Not applicable
- 10.2 Legal: Not applicable
- 10.3 Community (Equality, Poverty and Rural): Not applicable
- 10.4 Climate Change/Carbon Clever: Not applicable
- 10.5 Risk: Not applicable
- 10.6 Gaelic: Not applicable

## 11. RECOMMENDATION

Action required before decision issued	Y	Up front Contributions	Developer
Notification to Scottish Ministers	N		
Conclusion of Section 75 Obligation	N		
Revocation of previous permission	N		

**Subject to the above actions**, it is recommended to **GRANT** the application subject to the following conditions and reasons:

1. No development or work (including site clearance) shall commence until proposals for an archaeological watching brief to be carried out during site clearance and excavation works, has been submitted to, and approved in writing by, the Planning Authority. Thereafter, the watching brief shall be implemented as approved.

**Reason:** In order to protect the archaeological and historic interest of the site

2. No development or work shall commence until a detailed specification for all proposed external materials and finishes (including trade names and samples where necessary) has been submitted to, and approved in writing by, the Planning Authority. Thereafter, development and work shall progress in accordance with these approved details.

**Reason:** In the interest of visual amenity.

3. With effect from the date of this permission, no trees are to be cut down, uprooted, topped, lopped (including roots) or wilfully damaged in any way, without the prior written permission of the Planning Authority.

**Reason:** To ensure the protection of retained trees during construction and thereafter.

4. Prior to any site excavation or groundworks, a suitably qualified Arboricultural Consultant must be employed by the applicant to ensure that the Approved Tree Protection Plan and Arboricultural Method Statement (AMS) are implemented to the agreed standard. Stages requiring supervision are set out in the AMS section 4.0 (Summary of Arboricultural Supervision) and certificates of compliance for each stage are to be submitted for approval.

**Reason:** To ensure the protection of retained trees during construction and thereafter.

5. Prior to any site excavation or groundworks, all retained trees are to be protected against construction damage using protective barriers located beyond the Root Protection Area (in accordance with BS5837:2012 Trees in Relation to Design, Demolition & Construction) and as per the approved Tree Protection Plan. Barriers are to remain in place throughout the construction period and must not be moved or removed without the prior written approval of the Planning Authority, unless specified in the AMS.

**Reason:** To ensure the protection of retained trees throughout the construction period.

6. No development shall commence until a detailed Landscape Plan and maintenance programme has been submitted to and approved by the planning authority. The Landscape Plan shall be implemented in full during the first planting season following commencement of development or as otherwise agreed in writing by the Planning Authority.

**Reason:** In the interests of amenity.

7. No development shall commence until a scheme for the maintenance, in perpetuity, of all on-site green spaces and/or trees or other spaces, facilities, features or parts of the development that are not the exclusive property of any identifiable individual home owner (such as communal parking areas, the common entrances to flatted developments and estate lighting, and those elements of surface water drainage regimes not maintained either by the Council or Scottish Water), have been submitted to, and approved in writing



by, the Planning Authority. Thereafter, the approved scheme shall be implemented in full and in accordance with the timescales contained therein.

**Reason:** To ensure that all communal spaces, facilities and landscaping areas are properly managed and maintained.

8. No development shall commence until full details of all dropped crossings and tactile paving both within and out with the application site shall have been submitted on a Site Layout Plan for the approval in writing by the Planning Authority in consultation with the Roads Authority.

**Reason:** As no detail has been provided and in the interest of road safety.

9. No development shall commence until an outline charging structure or payment arrangement for use of electric vehicle charging points, and full details of an electric vehicle charging communication pack to be provided to each household on occupation explaining how they can access electric vehicle charging infrastructure has been submitted to, and approved in writing by, the Planning Authority in consultation with the Roads Authority. The EV instructions for residents shall be provided to residents prior to occupation of any of the flats hereby approved.

**Reason:** To facilitate the transition to low carbon transport.

10. No development shall commence until full details of bicycle storage has been submitted to, and approved in writing by, the Planning Authority in consultation with the Roads Authority. For the avoidance of doubt the storage spaces shall be denoted on plan and shall include one space per flat and one space per block of flats. Thereafter, storage system shall be installed in accordance with these approved details prior to the first occupation of the development hereby approved.

**Reason:** In order to ensure that there is adequate cycle parking available.

11. No development shall commence until full details of the communal wheelie/kerbside recycling bin storage area, capable of accommodating 2.No 240 litre bins for 15 properties, located out with any visibility splays, including any dropped kerbs or crossing points for pedestrian access have been submitted to, and approved in writing by, the Planning Authority. The communal storage area shall be constructed in accordance with these approved details prior to the first occupation of the development and thereafter maintained in perpetuity.

**Reason:** In the interest of road safety and in order to ensure that the final siting and design of the bin storage area is acceptable in the interest of visual amenity.

12. No development shall commence on site until a construction phases Traffic Management Plan (including routing plan for construction vehicles) has been

submitted to, and approved in writing by, the Planning Authority. This shall detail construction traffic routes and construction site operating times.

The approved traffic management plan shall be implemented prior to development commencing and remain in place until the development is complete.

**Reason:** In the interest of road safety, to limit the impacts on the local road network, and to limit the amenity impacts of the construction phase of the development on local residents.

13. Car parking spaces shall be provided and formed in accordance with The Highland Council's Roads and Transportation Guidelines for New Developments, as detailed on the plans hereby approved, prior to first occupation of the development, thereafter, being maintained for this use in perpetuity.

**Reason:** To ensure adequate provision for car parking.

14. No development shall commence until a finalised Drainage Impact Assessment, to include the final drainage design, which shall include drainage design for the access road, to comply with Highland Council and Scottish Water guidance, has been submitted to, and approved in writing by, the Planning Authority in consultation with the Roads Authority and Flood Risk Management Team. Thereafter, all surface water drainage provision within the application site shall be implemented in accordance with the finalised drainage design and shall be completed prior to the first occupation of any of the development within that phase.

**Reason:** To ensure final drainage design is acceptable and delivered timeously.

15. No development shall commence until full detail of a scheme for Public Art, to be delivered by the developer has been submitted to and agreed in writing by the Planning Authority.

**Reason:** As no detail has been provided.

## **REASON FOR DECISION**

All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

## **TIME LIMIT FOR THE IMPLEMENTATION OF THIS PLANNING PERMISSION**

In accordance with Section 58 of the Town and Country Planning (Scotland) Act 1997 (as amended), the development to which this planning permission relates must commence within THREE YEARS of the date of this decision notice. If development has not commenced within this period, then this planning permission shall lapse

## **INFORMATIVES**

### **Initiation and Completion Notices**

The Town and Country Planning (Scotland) Act 1997 (as amended) requires all developers to submit notices to the Planning Authority prior to, and upon completion of, development. These are in addition to any other similar requirements (such as Building Warrant completion notices) and failure to comply represents a breach of planning control and may result in formal enforcement action.

1. The developer must submit a Notice of Initiation of Development in accordance with Section 27A of the Act to the Planning Authority prior to work commencing on site.
2. On completion of the development, the developer must submit a Notice of Completion in accordance with Section 27B of the Act to the Planning Authority.

Copies of the notices referred to are attached to this decision notice for your convenience.

### **Flood Risk**

It is important to note that the granting of planning permission does not imply there is an unconditional absence of flood risk relating to (or emanating from) the application site. As per Scottish Planning Policy (paragraph 259), planning permission does not remove the liability position of developers or owners in relation to flood risk.

### **Scottish Water**

You are advised that a supply and connection to Scottish Water infrastructure is dependent on sufficient spare capacity at the time of the application for connection to Scottish Water. The granting of planning permission does not guarantee a connection. Any enquiries with regards to sewerage connection and/or water supply should be directed to Scottish Water on 0845 601 8855.

### **Local Roads Authority Consent**

In addition to planning permission, you may require one or more separate consents (such as road construction consent, dropped kerb consent, a road openings permit, occupation of the road permit etc.) from the Area Roads Team prior to work commencing. These consents may require additional work and/or introduce additional specifications and you are therefore advised to contact your local Area Roads office for further guidance at the earliest opportunity.

Failure to comply with access, parking and drainage infrastructure requirements may endanger road users, affect the safety and free-flow of traffic and is likely to result in enforcement action being taken against you under both the Town and Country Planning (Scotland) Act 1997 and the Roads (Scotland) Act 1984.

Further information on the Council's roads standards can be found at: <http://www.highland.gov.uk/yourenvironment/roadsandtransport>

Application forms and guidance notes for access-related consents can be downloaded from:

[http://www.highland.gov.uk/info/20005/roads\\_and\\_pavements/101/permits\\_for\\_working\\_on\\_public\\_roads/2](http://www.highland.gov.uk/info/20005/roads_and_pavements/101/permits_for_working_on_public_roads/2)

### **Trunk Road – Transport Scotland**

To obtain permission to work within the trunk road boundary, contact the Area Manager through the general contact number below. The Operating Company has responsibility for co-ordination and supervision of works and after permission has been granted it is the developer's contractor's responsibility to liaise with the Operating Company during the construction period to ensure all necessary permissions are obtained.

TS Contact: - Area Manager (A82), 0141272 7100, Buchanan House, 58 Port Dundas Road, Glasgow, G4 OHF

Operating Company - NORTHWEST

Address - Bear House, Inveralmond Road, Inveralmond Industrial Estate, Perth, PH1 3TW

Phone - 0845 4130200

Email - [NWplanning@bearsotland.co.uk](mailto:NWplanning@bearsotland.co.uk)

### **Mud & Debris on Road**

Please note that it is an offence under Section 95 of the Roads (Scotland) Act 1984 to allow mud or any other material to be deposited, and thereafter remain, on a public road from any vehicle or development site. You must, therefore, put in place a strategy for dealing with any material deposited on the public road network and maintain this until development is complete.

### **Construction Hours and Noise-Generating Activities**

You are advised that construction work associated with the approved development (incl. the loading/unloading of delivery vehicles, plant or other machinery), for which noise is audible at the boundary of the application site, should not normally take place outwith the hours of 08:00 and 19:00 Monday to Friday, 08:00 and 13:00 on Saturdays or at any time on a Sunday or Bank Holiday in Scotland, as prescribed in Schedule 1 of the Banking and Financial Dealings Act 1971 (as amended).

Work falling outwith these hours which gives rise to amenity concerns, or noise at any time which exceeds acceptable levels, may result in the service of a notice under Section 60 of the Control of Pollution Act 1974 (as amended). Breaching a Section 60 notice constitutes an offence and is likely to result in court action.

If you wish formal consent to work at specific times or on specific days, you may apply to the Council's Environmental Health Officer under Section 61 of the 1974 Act. Any such application should be submitted after you have obtained your Building Warrant, if required, and will be considered on its merits. Any decision taken will reflect the nature of the development, the site's location and the proximity of noise

sensitive premises. Please contact [env.health@highland.gov.uk](mailto:env.health@highland.gov.uk) for more information.

### **Protected Species – Halting of Work**

You are advised that work on site must stop immediately, and NatureScot must be contacted, if evidence of any protected species or nesting/breeding sites, not previously detected during the course of the application and provided for in this permission, are found on site. For the avoidance of doubt, it is an offence to deliberately or recklessly kill, injure or disturb protected species or to damage or destroy the breeding site of a protected species. These sites are protected even if the animal is not there at the time of discovery. Further information regarding protected species and developer responsibilities is available from NatureScot: <https://www.nature.scot/professional-advice/protected-areas-and-species/protected-species>

Signature: David Mudie  
Designation: Area Planning Manager – South  
Author: Laura Stewart  
Background Papers: Documents referred to in report and in case file.  
Relevant Plans: Plan 1 - 4577-01-LOC – Location Plan  
Plan 2 - 4577-01-001 REV H – Site Layout Plan  
Plan 3 - 4577-02-007 – Bicycle Parking Layout  
Plan 4 - 4577-01-105 – Floor/Elevation Plan  
Plan 5 - 4577-01-106 – Floor Plan  
Plan 6 - 4577-01-107 – Elevation Plan  
Plan 7 - 4577-01-108 – Floor/Elevation Plan  
Plan 8 - 4577-01-109 – Elevation Plan  
Plan 9 - 4577-02-005 – Electric Vehicle Charging Plan  
Plan 10 - 4577-02-006 REV B – Waste Management Plan  
Plan 11 - 116801-TPP – Tree Protection Plan

## Appendix 2

	COMPLETE FOR LEGAL AGREEMENTS AND UPFRONT PAYMENTS				REQUIRED FOR LEGAL AGREEMENTS ONLY				
Type	Contribution	Rate (per house)	Rate (per flat)	Total Amount* <sup>1</sup>	Index Linked <sup>1</sup>	Base Date* <sup>2</sup>	Payment Trigger* <sup>3</sup>	Accounting Dates* <sup>4</sup>	Clawback Period* <sup>5</sup>
<b>Schools<sup>2</sup></b>									
Primary – Build Costs	Two classroom extension Glenurquhart Primary School	£0.00	£1,157	£17.355	BCIS	Q2 2018	TOC/CC	Apr/Oct	20
<b>Affordable Housing</b>									
On-site provision <sup>3</sup>	15 units.	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A
<b>Transport</b>									
Public Transport	Real time information portal at the Loch Ness Transport Hub	£0.00	£100.00	£1,500	BCIS		TOC/CC	Apr/Oct	20
<b>Green Infrastructure</b>									
Open Space	Upgrade play facilities at play area adjacent to the site	£0.00	£0.00	£10,000	BCIS		TOC/CC	Apr/Oct	20

\*<sup>1</sup> Adjust total to take account of flat exemptions

\*<sup>2</sup> Base Date – Set out in Supplementary Guidance on Developer Contributions

\*<sup>3</sup> TOC/CC – The earlier of the issue of either a temporary occupation certificate or a completion certificate – or specify alternative time if appropriate

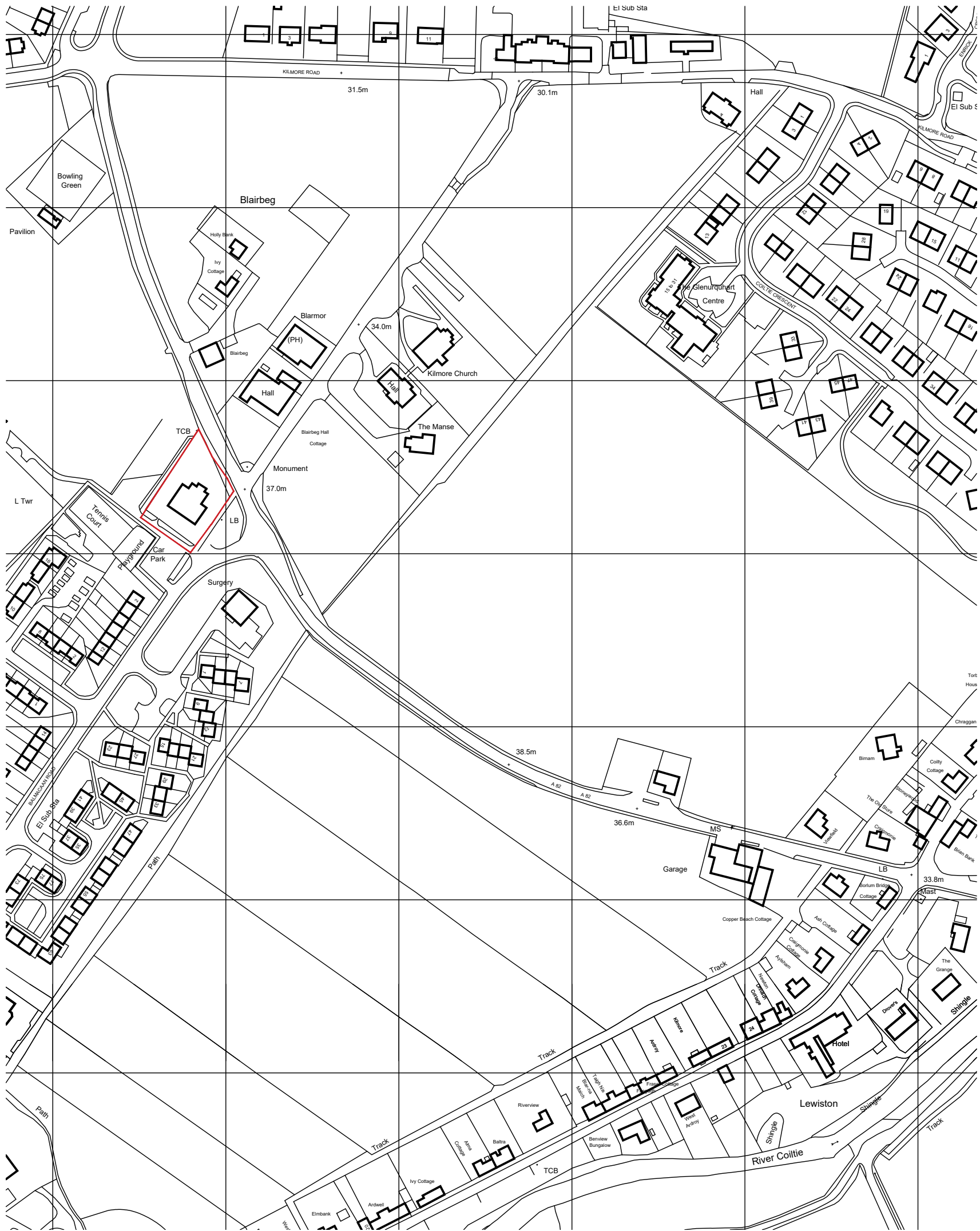
\*<sup>4</sup> Accounting dates - 1 April & 1 October each year of development (if the contribution is to be paid on a basis other than related to units completed in the preceding 6 months (e.g. lump sum on a specific date) then indicate this instead of the Apr/Oct payment dates)

\*<sup>5</sup> Clawback – 15 years for Major development; 20 years for Local development

<sup>1</sup> If the contribution is to be used towards infrastructure projects involving building e.g. new school, new cycle route etc BCIS ALL IN TENDER will be the index, if it doesn't involve building then another appropriate index may need to be chosen with the agreement of Team Leader


<sup>2</sup> Indicate whether or not 1 bed houses/flats are exempt

<sup>3</sup> Indicate whether a penalty payment due for late delivery (and, if so, what it is based upon).



**Note to Designers and Contractors**

Under the CDM2015 regulations BSC has sought to eliminate or reduce risks where possible as part of the design process.

Significant residual risks, or areas of work that require special attention during construction, which have been identified by BSC, are indicated on project drawings by the following symbol .

It is anticipated that other designers and contractors will co-operate to identify any potential construction hazards and to eliminate them where possible.

Measures to minimise residual hazards will be reviewed on a regular basis.

REV	DATE	DESCRIPTION	DRN

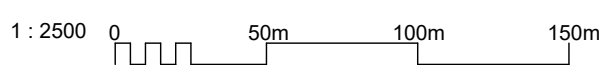
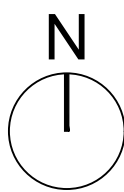
**Bracewell Stirling CONSULTING**

38 WALKER TERRACE, TILlicOLTRY, FK13 6EF 01259 750301  
 5 NESS BANK, INVERNESS, IV2 4SF 01463 233760  
 15 LOCHSIDE STREET, OBAN, PA34 4HP 01631 359054

FORMER SCOTMID SITE, DRUMNADROCHIT  
 NEW FLATTED DEVELOPMENT  
 HIGHLAND HOUSING ALLIANCE

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4577-01-LOC\_ LOCATION PLAN



SCALE:	1 : 2500	DRAWN:	--
PAPER SIZE:	A3	DATE:	Sep 2020
DWG No.	4577-01-LOC		REV.

**Note to Designers and Contractors**

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- Key:**
- Application Boundary
  - Naurl stone wall
  - 4.0m high ball stop fence
  - Parking Court
  - Communal Grass Areas
  - Existing trees proposed for removal (Total - 5 no.)
  - Existing trees to be retained
  - Proposed trees

2 Bed 3 Person (2/3 storey) common entry flat x13 no.

2 Bed 3 Person (2 storey) private entry cottage flat x2 no.

**TOTAL - 15 no. units**  
**- 23 parking spaces**

REV	DATE	DESCRIPTION	DRN
H	18.08.21	TRANSPORT PLANNING COMMENTS ADDRESSED	LB
G	05.08.21	TRANSPORT PLANNING COMMENTS ADDRESSED	LB
F	21.06.21	VISITOR CYCLE SPACES ADDED IN RESPONSE TO TRANSPORT PLANNING COMMENTS.	LB
E	03.06.21	POSITION OF BALL CATCH FENCE REVISED	LB
D	25.05.21	COMMUNAL GRASS AREA HATCH REVISED	LB
C	24.03.21	APPLICATION BOUNDARY REVISED	LB
B	23.03.21	APPLICATION BOUNDARY REVISED	LB
A	03.09.20	VARIOUS UPDATES	GB

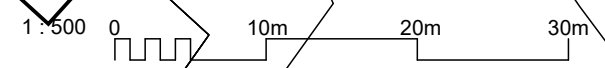
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38 WALKER TERRACE, TILlicouLTRY, FK13 6EF 01259 750301  
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 15 LOCHSIDE STREET, OBAN, PA34 4HP 01631 359054

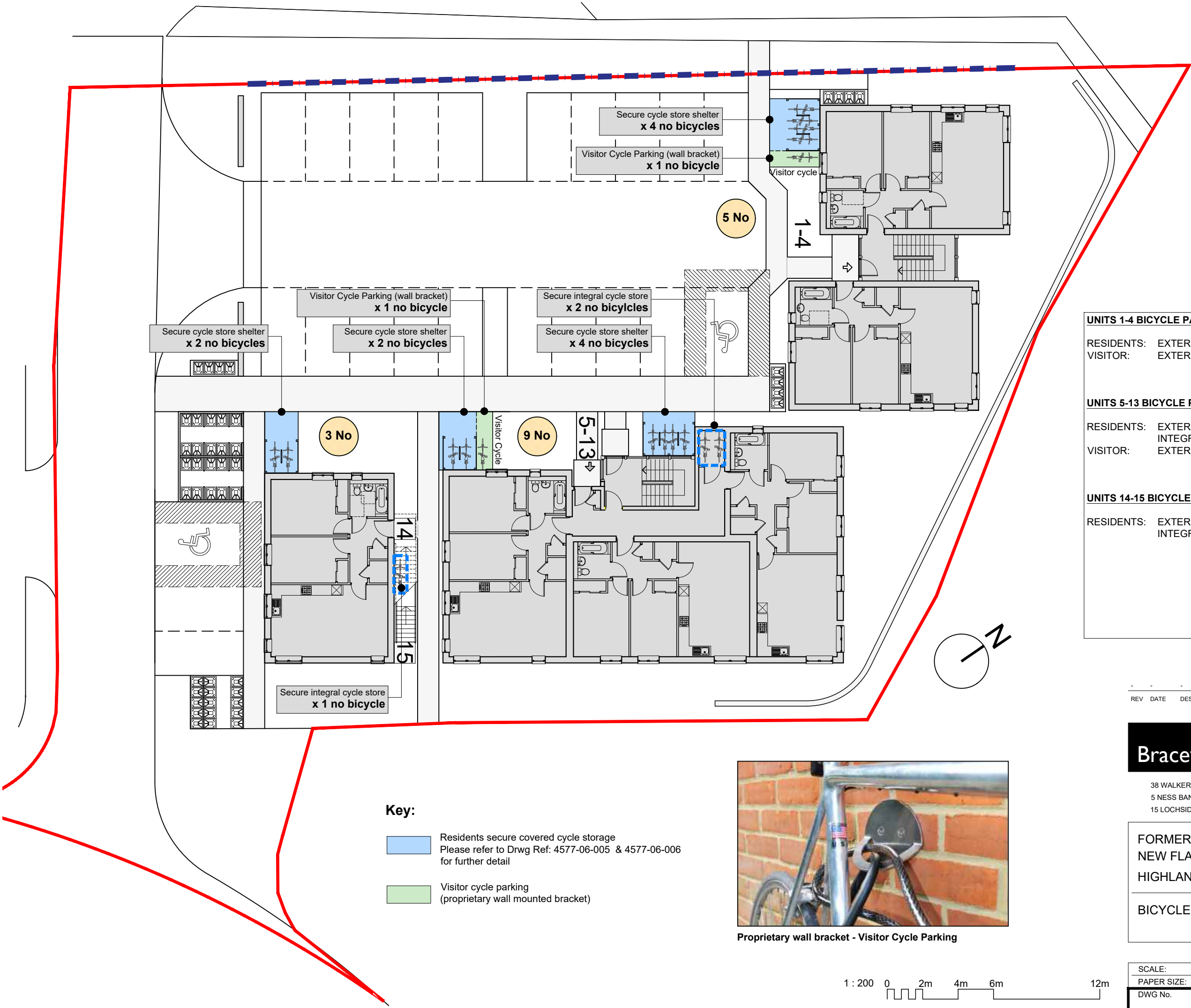
FORMER SCOTMID SITE, DRUMNADROCHIT  
 NEW FLATTED DEVELOPMENT  
 HIGHLAND HOUSING ALLIANCE

SITE LAYOUT

SCALE:	1 : 500	DRAWN:	--
PAPER SIZE:	A3	DATE:	Aug 2021
DWG No.	4577-01-001	REV.	H







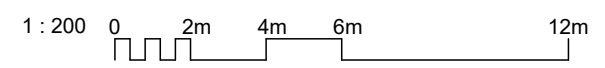
UNITS 1-4 BICYCLE PARKING			
RESIDENTS:	EXTERNAL SECURE CYCLE STORE	4 no	
VISITOR:	EXTERNAL PARKING SPACE	1 no	
			<b>TOTAL: 5 NO</b>
UNITS 5-13 BICYCLE PARKING			
RESIDENTS:	EXTERNAL SECURE CYCLE STORE	6 no	
	INTEGRAL SECURE CYCLE STORE	2 no	
VISITOR:	EXTERNAL PARKING SPACE	1 no	
			<b>TOTAL: 9 NO</b>
UNITS 14-15 BICYCLE PARKING			
RESIDENTS:	EXTERNAL SECURE CYCLE STORE	2 no	
	INTEGRAL SECURE CYCLE STORE	1 no	
			<b>TOTAL: 3 NO</b>
		-	15 NO RESIDENTS SPACES
		-	2 NO VISITOR SPACES
			<b>OVERALL TOTAL: 17 NO</b>

**Key:**

- Residents secure covered cycle storage  
Please refer to Drwg Ref: 4577-06-005 & 4577-06-006 for further detail
- Visitor cycle parking  
(proprietary wall mounted bracket)



Proprietary wall bracket - Visitor Cycle Parking



REV	DATE	DESCRIPTION	DRN

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 15 LOCHSIDE STREET, OBAN, PA34 4HP 01631 359054

FORMER SCOTMID SITE, DRUMNADROCHIT  
 NEW FLATTED DEVELOPMENT  
 HIGHLAND HOUSING ALLIANCE

BICYCLE PARKING LAYOUT

SCALE:	1 : 200	DRAWN:	-
PAPER SIZE:	A3	DATE:	Aug 2021
DWG No.	<b>4577-02-007</b>	REV.	-

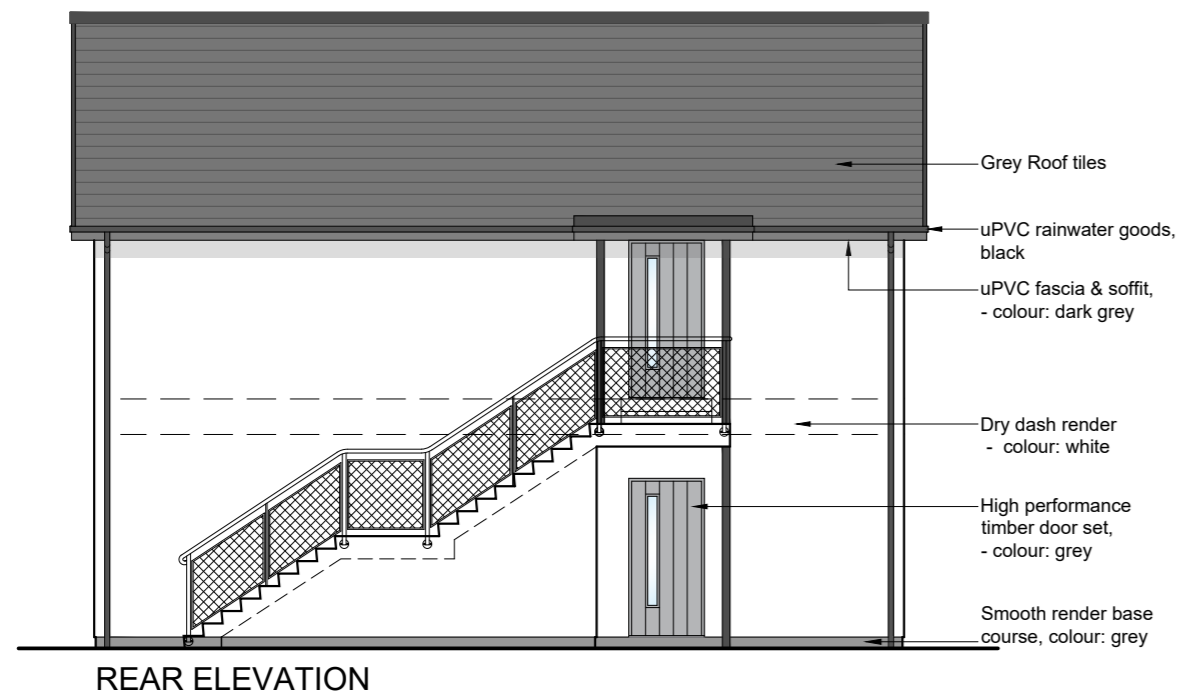
**Note to Designers and Contractors**

Under the CDM2015 regulations BSC has sought to eliminate or reduce risks where possible as part of the design process.

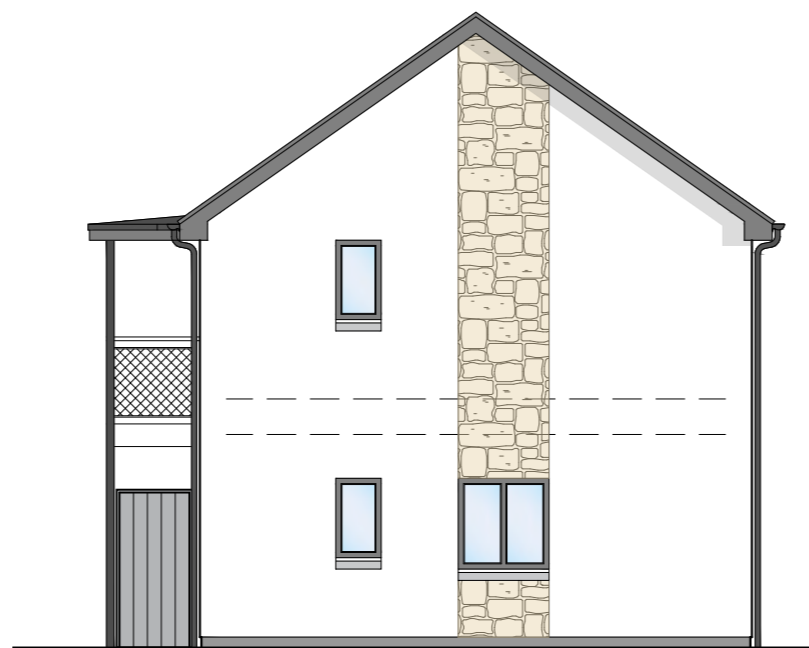
Significant residual risks, or areas of work that require special attention during construction, which have been identified by BSC, are indicated on project drawings by the following symbol **▲**.

It is anticipated that other designers and contractors will co-operate to identify any potential construction hazards and to eliminate them where possible.

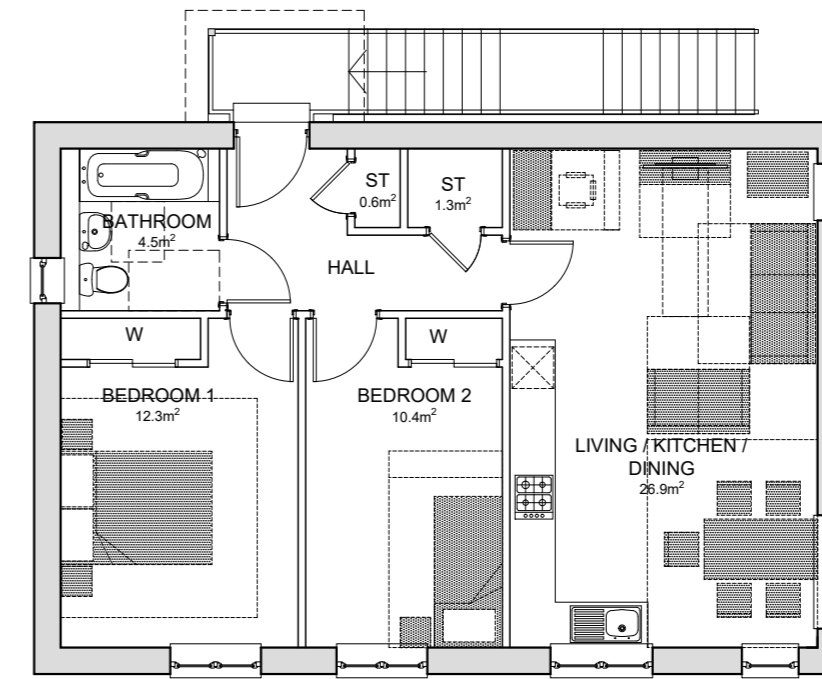
Measures to minimise residual hazards will be reviewed on a regular basis.



REAR ELEVATION



GABLE ELEVATION



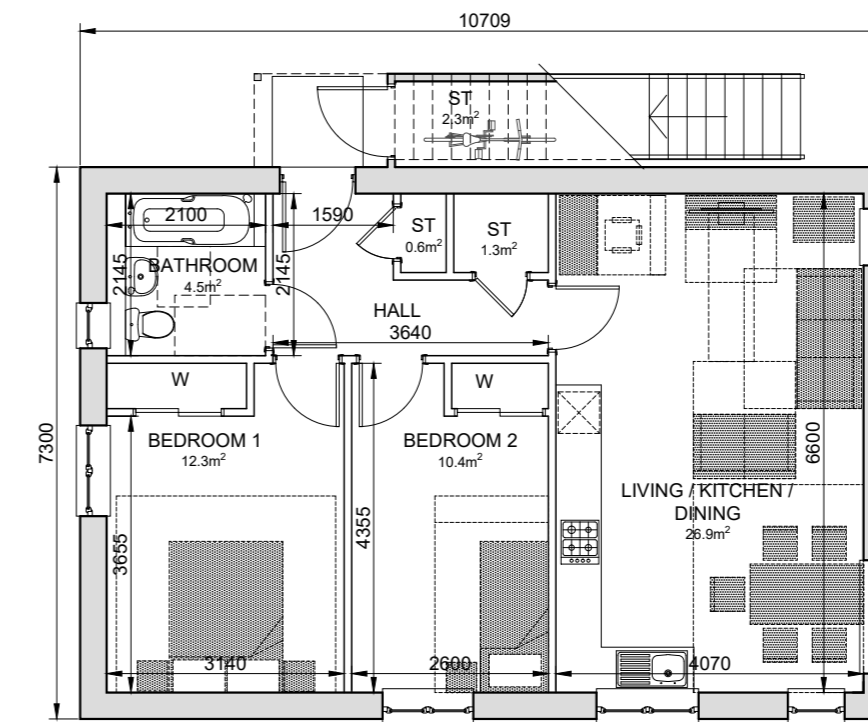
FIRST FLOOR PLAN  
 TWO BED COTTAGE FLATS  
 FLOOR AREA 66.1m<sup>2</sup>



FRONT ELEVATION



GABLE ELEVATION



GROUND FLOOR PLAN  
 TWO BED COTTAGE FLATS  
 FLOOR AREA 66.1m<sup>2</sup>

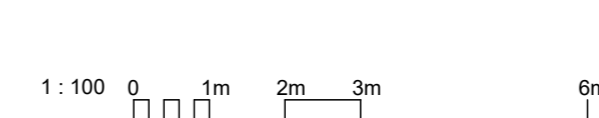
REV	DATE	DESCRIPTION	DRN

**Bracewell Stirling CONSULTING**

38 WALKER TERRACE, TILlicouLTRY, FK13 6EF 01259 750301  
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FORMER SCOTMID SITE, DRUMNADROCHIT  
 NEW FLATTED DEVELOPMENT  
 HIGHLAND HOUSING ALLIANCE


PROPOSED COTTAGE FLATS - BLOCK 1



SCALE:	1 : 100	DRAWN:	--
PAPER SIZE:	A2	DATE:	Mar 2021
DWG No.	<b>4577-01-105</b>	REV.	

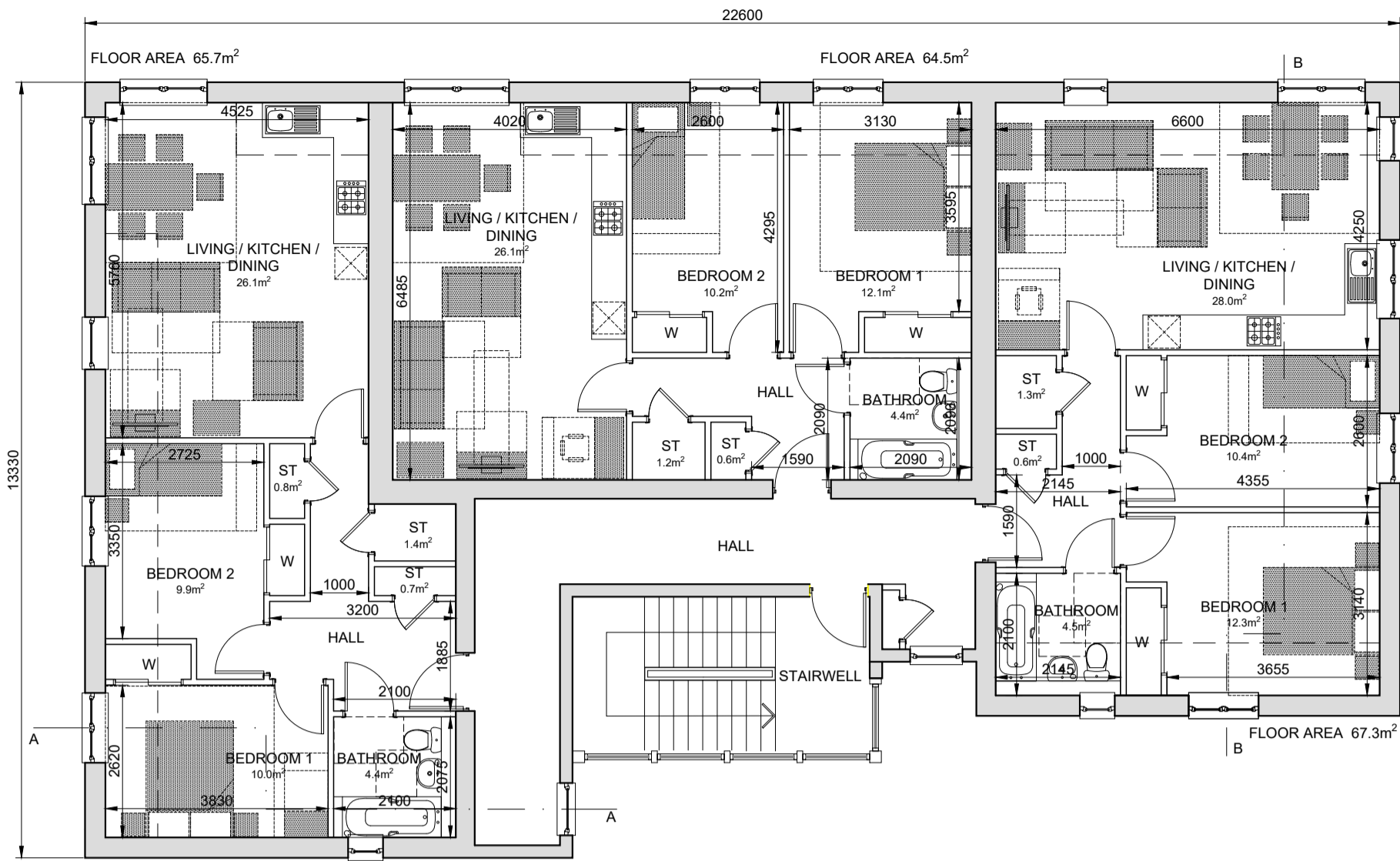
**Note to Designers and Contractors**

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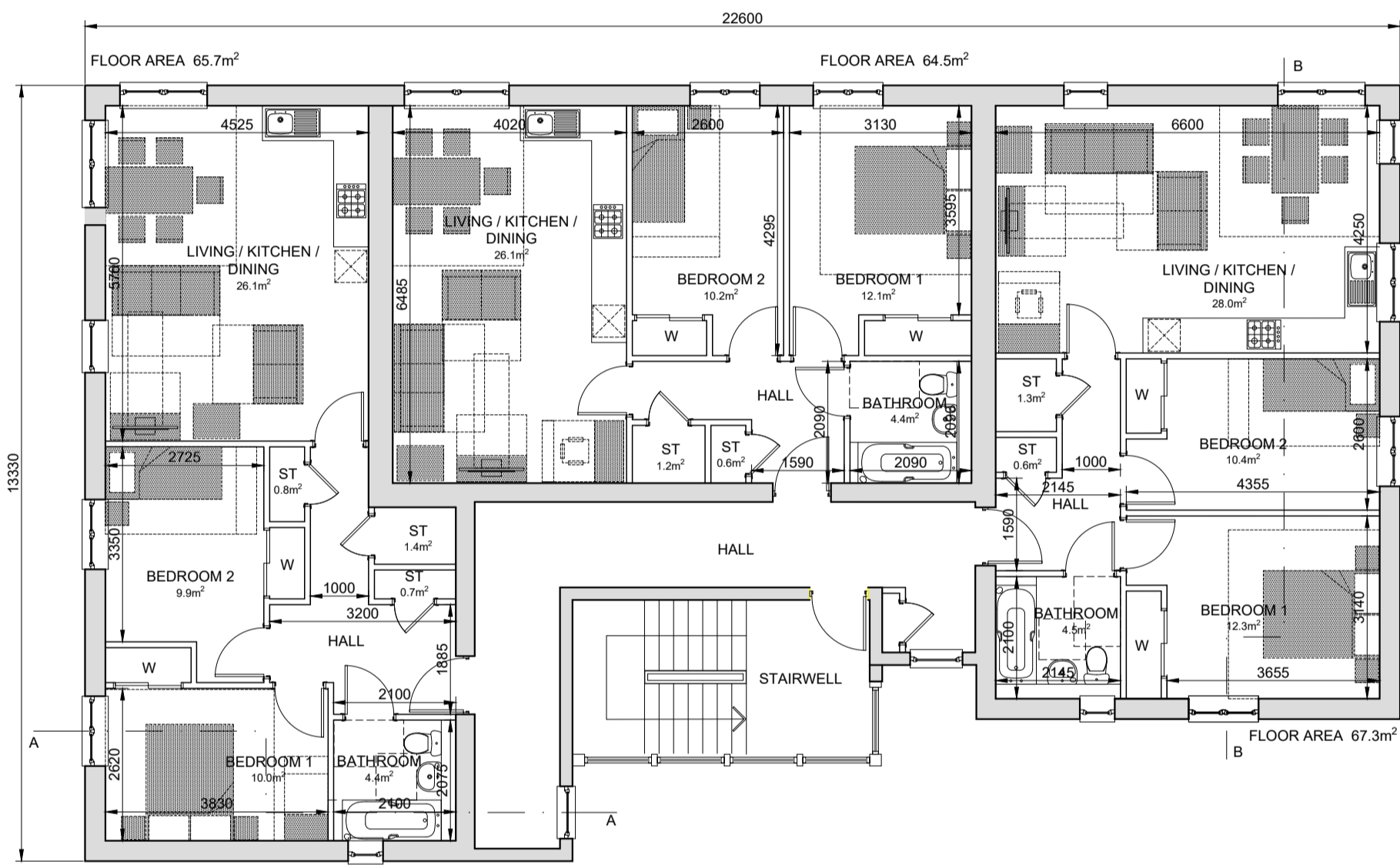
Significant residual risks, or areas of work that require special attention during construction, which have been identified by BSC, are indicated on project drawings by the following symbol .

It is anticipated that other designers and contractors will co-operate to identify any potential construction hazards and to eliminate them where possible.

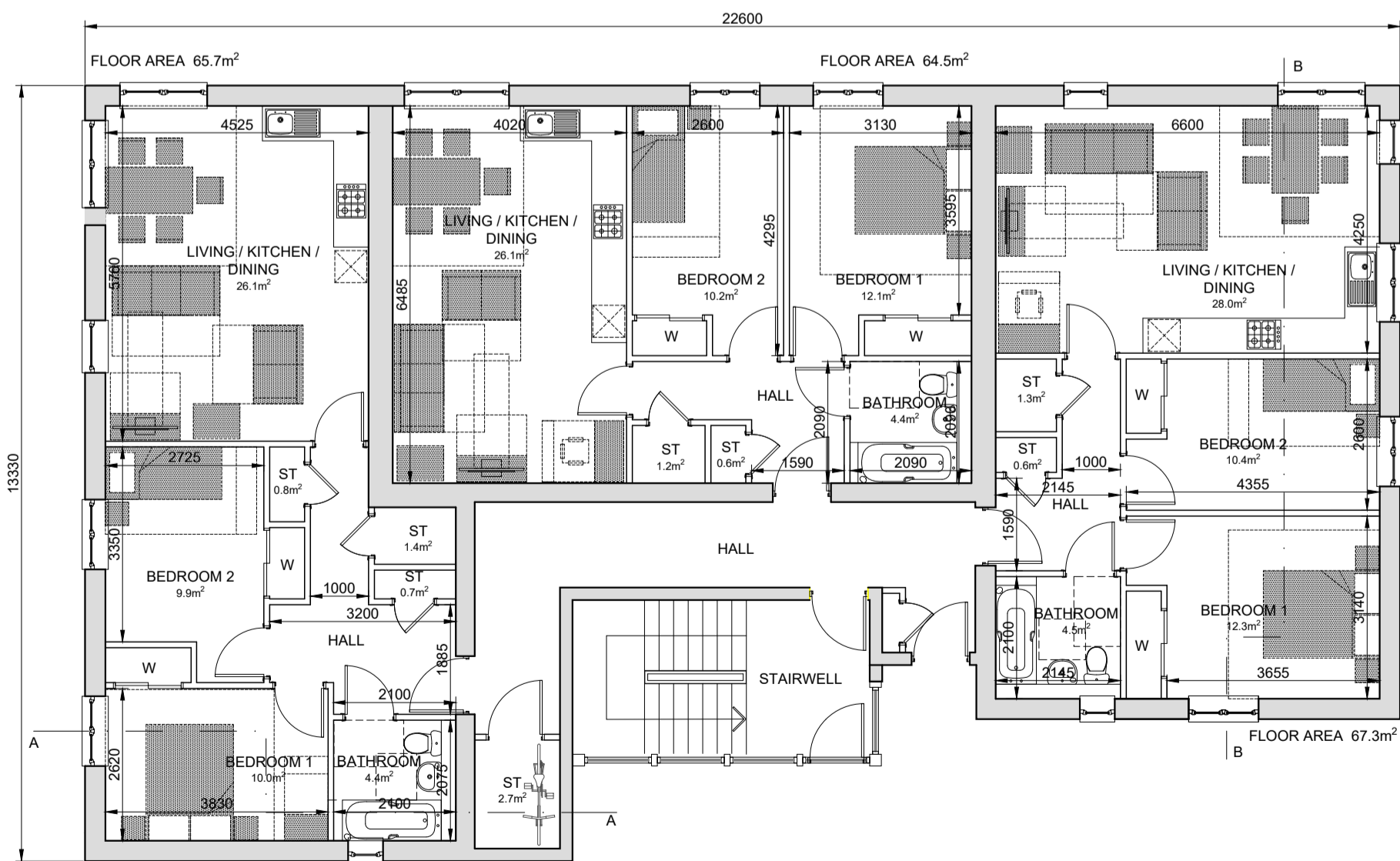
Measures to minimise residual hazards will be reviewed on a regular basis.



**SECOND FLOOR PLAN**  
TWO BED FLATS



**FIRST FLOOR PLAN**  
TWO BED FLATS



**GROUND FLOOR PLAN**  
TWO BED FLATS

REV	DATE	DESCRIPTION	DRN

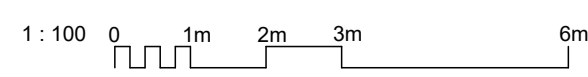
**Bracewell Stirling CONSULTING**

38 WALKER TERRACE, TILlicouTRY, FK13 6EF 01259 750301  
 5 NESS BANK, INVERNESS, IV2 4SF 01463 233760  
 15 LOCHSIDE STREET, OBAN, PA34 4HP 01631 359054

FORMER SCOTMID SITE, DRUMNADROCHIT  
 NEW FLATTED DEVELOPMENT  
 HIGHLAND HOUSING ALLIANCE

PROPOSED COMMON ENTRANCE FLATS -  
 BLOCK 2- FLOOR PLANS

SCALE:	1 : 100	DRAWN:	--
PAPER SIZE:	A2	DATE:	Mar 2021
DWG No.	<b>4577-01-106</b>	REV.	



**Note to Designers and Contractors**

Under the CDM2015 regulations BSC has sought to eliminate or reduce risks where possible as part of the design process.

Significant residual risks, or areas of work that require special attention during construction, which have been identified by BSC, are indicated on project drawings by the following symbol  $\Delta$ .

It is anticipated that other designers and contractors will co-operate to identify any potential construction hazards and to eliminate them where possible.

Measures to minimise residual hazards will be reviewed on a regular basis.



**STREET ELEVATION**



**GABLE ELEVATION**



**PARKING COURT ELEVATION**



**GABLE ELEVATION**

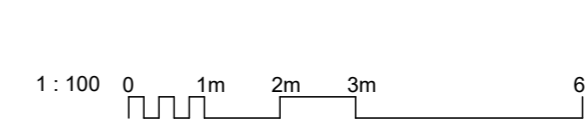
REV	DATE	DESCRIPTION	DRN

**Bracewell Stirling CONSULTING**

38 WALKER TERRACE, TILLCOUNTRY, FK13 6EF 01259 750301  
 5 NESS BANK, INVERNESS, IV2 4SF 01463 233760  
 15 LOCHSIDE STREET, OBAN, PA34 4HP 01631 359054

FORMER SCOTMID SITE, DRUMNADROCHIT  
 NEW FLATTED DEVELOPMENT  
 HIGHLAND HOUSING ALLIANCE

PROPOSED COMMON ENTRANCE FLATS -  
 BLOCK 2 - ELEVATIONS



SCALE:	1 : 100	DRAWN:	--
PAPER SIZE:	A2	DATE:	Mar 2021
DWG No.	4577-01-107	REV.	

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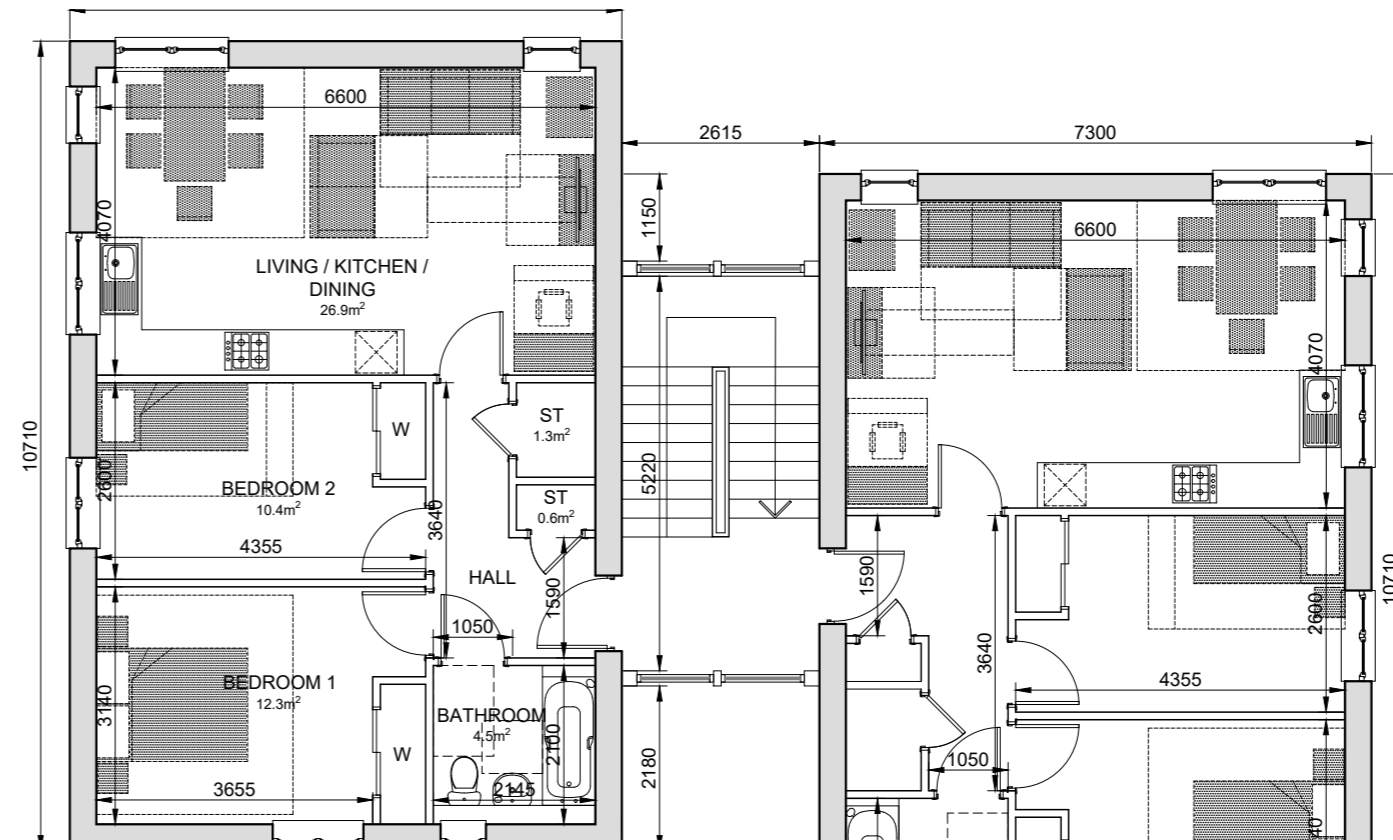
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**STREET ELEVATION**

- Grey Roof tiles
- uPVC rainwater goods, black
- uPVC fascia & soffit, - colour: dark grey
- uPVC Windows - colour: grey
- Reconstituted Stone features
- Dry dash render - colour: white
- Smooth render base course, colour: grey

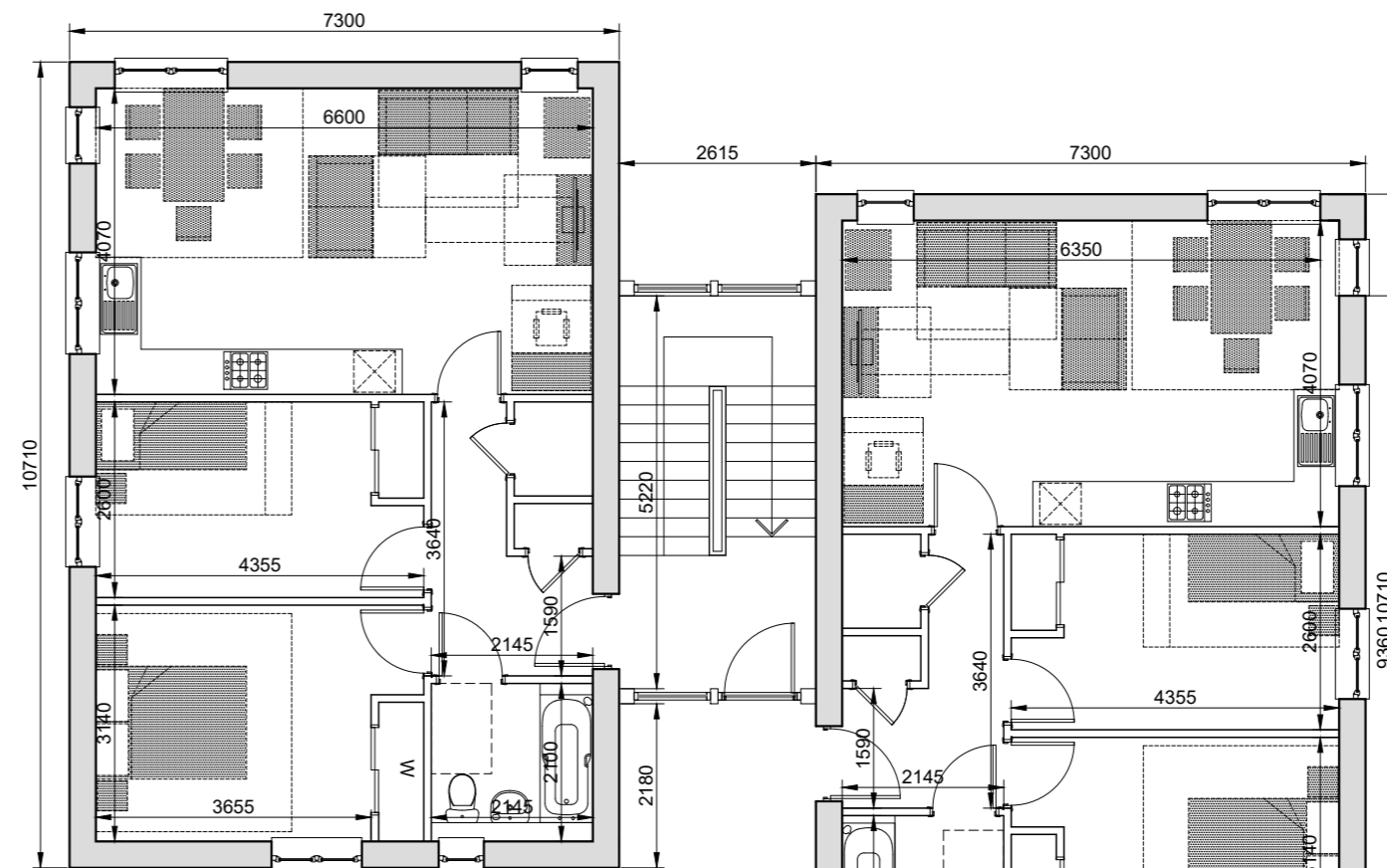


**FIRST FLOOR PLAN**  
TWO BED FLATS  
FLOOR AREA 66.1m<sup>2</sup>

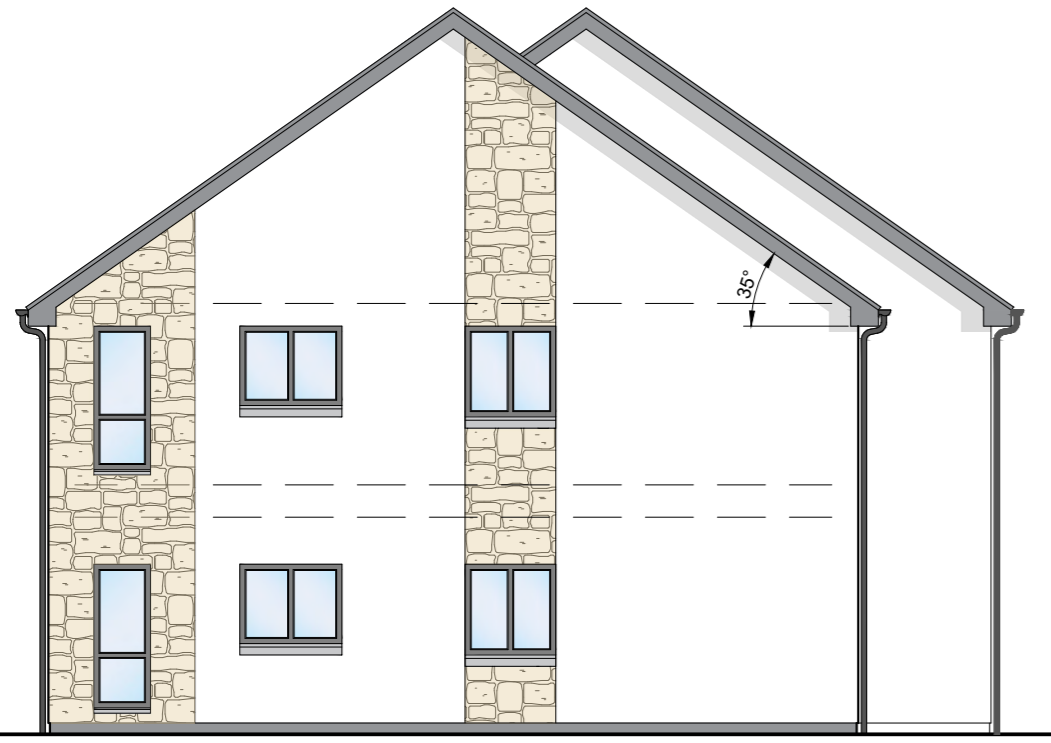


**FRONT ELEVATION**

- Grey Roof tiles
- uPVC rainwater goods, black
- uPVC fascia & soffit, - colour: dark grey
- uPVC Windows - colour: grey
- Reconstituted Stone features
- Dry dash render - colour: white
- Smooth render base course, colour: grey



**GROUND FLOOR PLAN**  
TWO BED FLATS  
FLOOR AREA 66.1m<sup>2</sup>



**GABLE ELEVATION**



**GABLE ELEVATION**

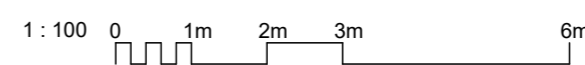
REV	DATE	DESCRIPTION	DRN

**Bracewell Stirling CONSULTING**

38 WALKER TERRACE, TILlicouTRY, FK13 6EF 01259 750301  
 5 NESS BANK, INVERNESS, IV2 4SF 01463 233760  
 15 LOCHSIDE STREET, OBAN, PA34 4HP 01631 359054

FORMER SCOTMID SITE, DRUMNADROCHIT  
 NEW FLATTED DEVELOPMENT  
 HIGHLAND HOUSING ALLIANCE


PROPOSED COMMON ENTRANCE FLATS -  
 BLOCK 3



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PAPER SIZE:	A2	DATE:	Mar 2021
DWG No.	4577-01-108		REV.

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SOUTH-EAST FACING ELEVATION - BALMACAAN ROAD



SOUTH-WEST FACING ELEVATION - PARKING COURT

NORTH-EAST FACING ELEVATION - A82



NORTH-WEST FACING ELEVATION - PARKING COURT

REV.	DATE	DESCRIPTION	DRN

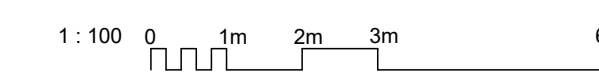
**Bracewell Stirling CONSULTING**

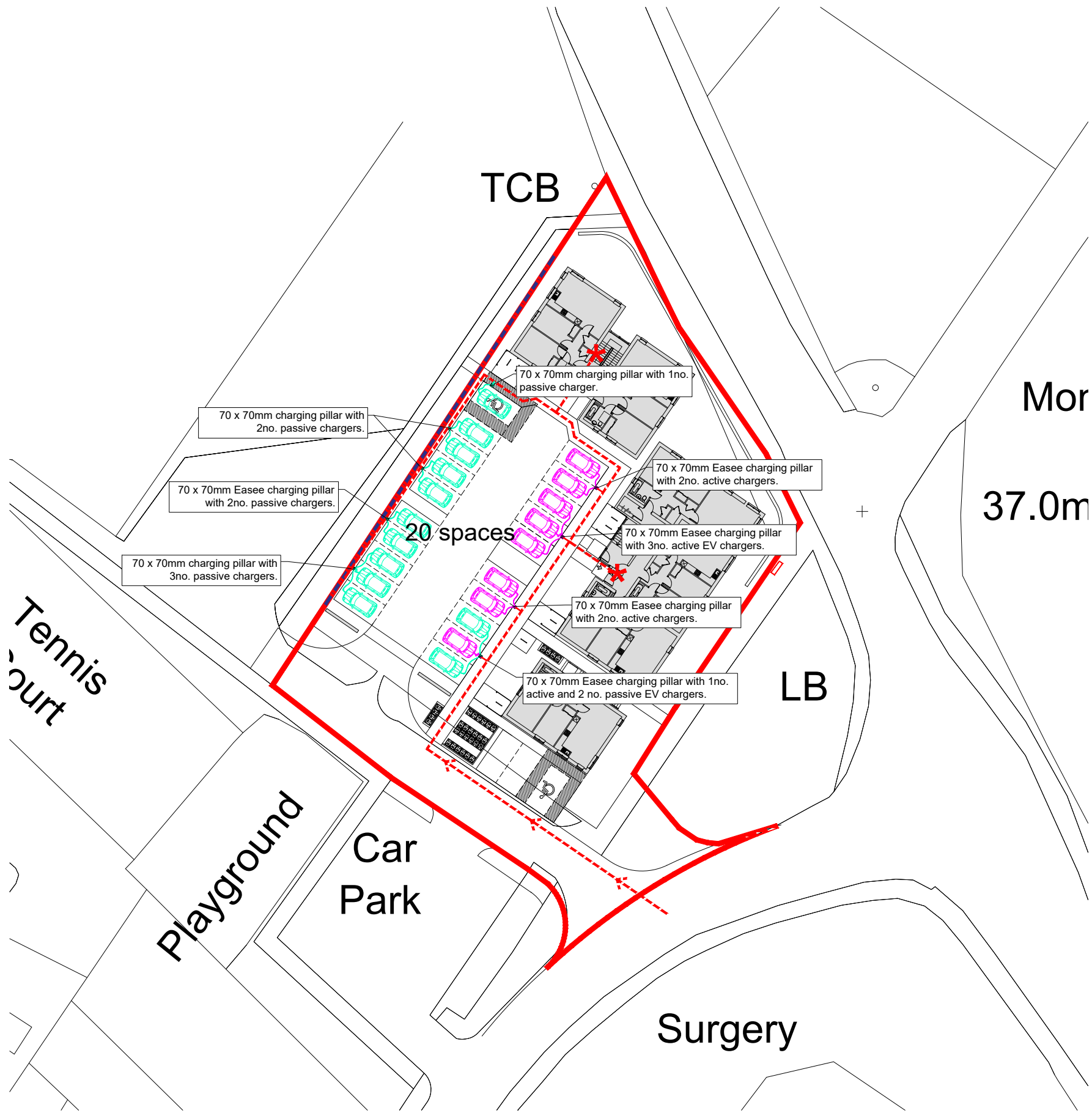
38 WALKER TERRACE, TILlicOLTRY, FK13 6EF 01259 750001  
 5 NESS BANK, INVERNESS, IV2 4SF 01463 233760  
 15 LOCHSIDE STREET, OBAN, PA34 4HP 01631 359054

FORMER SCOTMID SITE, DRUMNADROCHIT  
 NEW FLATTED DEVELOPMENT  
 HIGHLAND HOUSING ALLIANCE

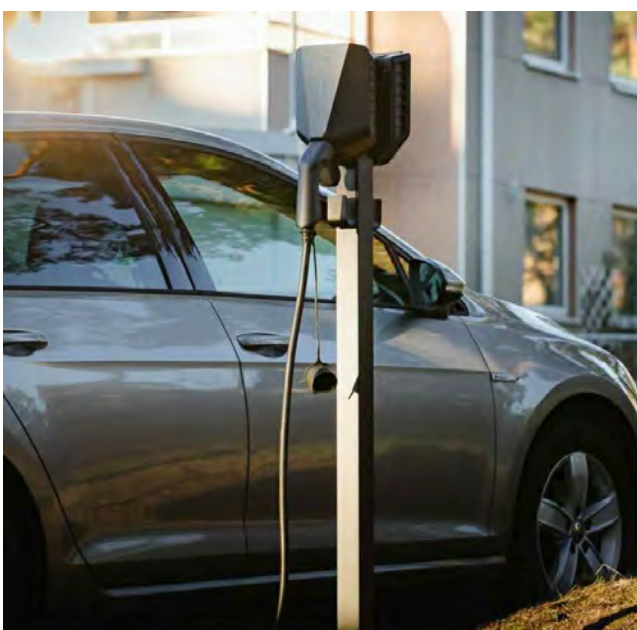
PROPOSED STREET ELEVATIONS -  
 STREET ELEVATIONS






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DWG No.	4577-01-109	REV.	





Proposed Site Layout




-  70 x 70mm charging pillar with active EV charging robots.
-  70 x 70mm charging pillar with passive EV charging robots for future demand.
-  70 x 70mm charging pillar with both active & passive EV charging robots.
-  EV cable connected to landlord supply
-  Denotes indicative location of 100mm Upvc duct for EV charging cable to be installed by others.

Please Note: The IET Wiring Regulations (BS7671:2018 & A1:2020) and the requirements of the Highland Council document EV001 July 2019 Domestic EV Provision.

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- 2 Bed 3 Person (2/3 storey) common entry flat x13 no.
- 2 Bed 3 Person (2 storey) private entry cottage flat x2 no.

**TOTAL - 15 no. units**

Expected Minimum EV Provision Required:  
50% of properties to include Active EV charge point per property at time of development completion.

**TOTAL SPACES INC. ACTIVE EV CHARGE POINT - 8 no.**

REV	DATE	DESCRIPTION	DRN
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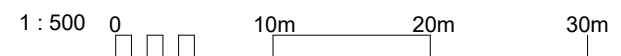
38 WALKER TERRACE, TILlicouLTRY, FK13 6EF 01259 750301  
 5 NESS BANK, INVERNESS, IV2 4SF 01463 233760  
 15 LOCHSIDE STREET, OBAN, PA34 4HP 01631 359054

FORMER SCOTMID SITE, DRUMNADROCHIT  
 NEW FLATTED DEVELOPMENT  
 HIGHLAND HOUSING ALLIANCE

**ELECTRIC VEHICLE CHARGING STRATEGY**

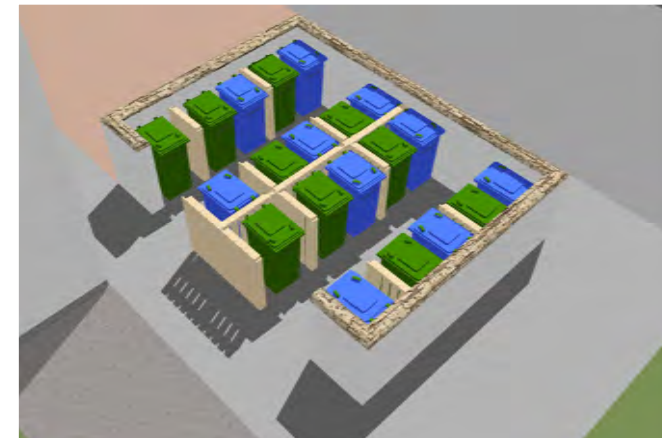
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PAPER SIZE:	A3	DATE:	Jun 2021

DWG No. **4577-02-005** REV.





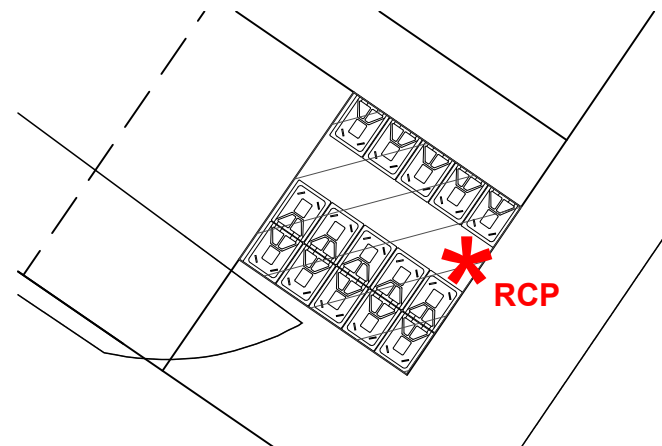
**Main Bin Store Area** (capacity for storage of 22 no. 140 litre waste bins)



**Main Bin Store Area Visual**








**Main Bin Store Area** (Example Photo showing segregation)



**Refuse Collection Point** (capacity for storage of 15 no. 140 litre waste bins)

**Waste Collection Legend:**

-  Communal Bin store to Units 5-13 & 14-15. Includes capacity for storage of 22 no 240 litre bins
-  Bin Store Areas to Units 1-4  
Hardstanding area with 1.2m high fence which includes capacity for storage of 8 no 240 litre bins
-  Refuse Collection Point (RCP)  
Hardstanding area to allow for the placement of 15 no 240 litre waste bins on bin collection day
-  1.2m high timber fence around 2 no. smaller bin stores.
-  1.2m high wall around larger bin store. Wall finished in render with cope stone

**Please Note:**  
Each property will be supplied with 1 x 240 litre general waste wheelie bin & 1 x 240 litre recycle waste wheelie bin.

The combined bin store area will require storage for up to 30 no. 240 litre bins in total. An allocated area for the storage of 2 no. bins per apartment will be provided within the bin store(s) which will be segregated off through the addition of 1.2m high timber partitions as demonstrated in the above photo.

A hardstanding refuse collection point (RCP) will also be provided directly off the junction to allow for the placement of waste containers by each householder on bin collection day. No garden waste bins are to be provided for these units as all soft landscaped grounds will be communal with the factor responsible for collecting and removing waste.

B	18:08:21	TRANSPORT PLANNING COMMENTS ADDRESSED	LB
A	05:08:21	TRANSPORT PLANNING COMMENTS ADDRESSED	LB
REV	DATE	DESCRIPTION	DRN

**Bracewell Stirling CONSULTING**

38 WALKER TERRACE, TILlicouLTRY, FK13 6EF 01259 750301  
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WASTE MANAGEMENT PLAN


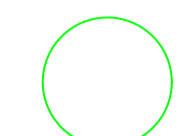
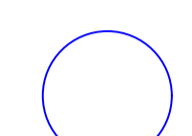


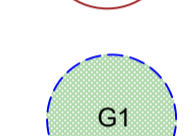


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PAPER SIZE:	A3	DATE:	Aug 2021
DWG No.	4577-02-006	REV.	B

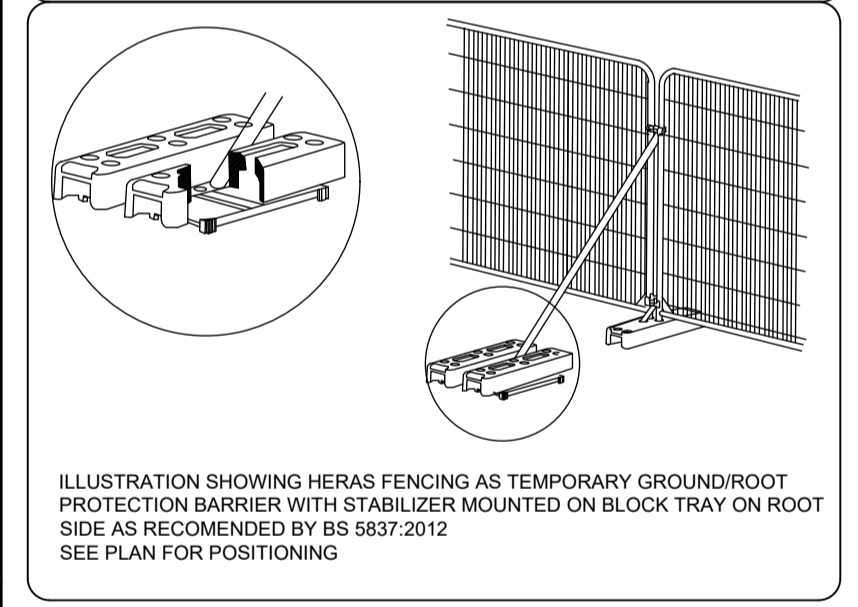




**Tree Protection Plan showing tree categories and root protection zones.**

**BS5837:2012 Tree Categories**

-  BS 5837:2012 Root Protection Area RPA, or Construction Exclusion Zone
-  **Category A**  
Trees of high quality with an estimated remaining life expectancy of at least 40 years.
-  **Category B**  
Trees of moderate quality with an estimated remaining life expectancy of at least 20 years.
-  **Category C**  
Trees of low quality with an estimated remaining life expectancy of at least 10 years, or young trees with a stem diameter below 150 mm.
-  **Category U**  
Those in such a condition that they cannot realistically be retained as living trees in the context of the current land use for longer than 10 years.
-  **Group**  
Showing canopy extents and category colour which will also represent RPA unless detailed separately
-  Tree for Removal
-  Tree protection Fencing



**Old Co-op  
Balmacaan  
Drumnadrochit  
Tree Protection Plan**

Client: Bracewell Stirling Consulting 5 Ness Bank Inverness, IV2 4SF	Drawing No: <b>116801-TPP</b>
Scale: <b>1:250</b>	Surveyed by: LCM
Date: 12th July 2021	Checked by: RW
ACAD File No: 116801.DWG	Sheet: 1 of 1
Grid Used: Local Based on OS	Level Datum Used: Ordnance (GPS)

