

Agenda Item	6.3
Report No	PLN/041/23

## HIGHLAND COUNCIL

**Committee:** North Planning Applications Committee  
**Date:** 7<sup>th</sup> June 2023  
**Report Title:** 22/03011/FUL: Cruive Ltd  
Land 600M NE Of Tralorn, Battery Road, Castletown  
**Report By:** Area Planning Manager - North

### Purpose/Executive Summary

**Description:** Erection of 4no. bothys and office/service building and associated infrastructure.  
**Ward:** 02 – Thurso and North West Caithness  
**Development category:** Local Development  
**Reason referred to Committee:** Community Council Objection

All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

### Recommendation

Members are asked to agree the recommendation to **GRANT** the application as set out in section 11 of the report

## 1. PROPOSED DEVELOPMENT

- 1.1 The applicant seeks full planning permission for four off-grid bothies, an office/service building, renewable energy infrastructure (roof mounted solar panels) and associated infrastructure along the shore edge to the east of the repurposed Gun Emplacement building located to the north east of Castletown. The site measures approximately 2 hectares however the proposal is considered low impact, with the developable area much smaller (approximately 1.7 hectares), therefore given the provisions of the Hierarchy of Developments circular, the proposal can be considered a local development.
- 1.2 The proposal includes a site office / service building that would be located on site of the former rifle range building, four bothy type structures, upgraded access, car parking and turning area. The proposal includes recording and displaying of any artefacts that are found during pre-commencement survey work. It is also proposed that the access to the site between Battery Road and Main Street (A836) will be upgraded and signage will be erected. The signage includes a new standing flagstone with 'CRUIVE' on it. The sign will be located at the end of the existing strong wall on the western side of the junction.
- 1.3 Pre Application Advice was sought through the Council's formal service in April 2022. The response set out a number of complex issues that the proposal would be assessed against, including landscape and visual impacts. General support was provided for a well-considered, appropriately scaled, laid out, and designed development at this location, which should be acceptable in all aspects.
- 1.4 The application is accompanied by the following Supporting Information:
  - Design and Access Statement
  - Ecological Assessment Report
  - Geo-Environmental Risk Assessment
  - Habitat Survey
  - Protected Species Survey Reports
  - Private Access Checklist
  - Project Design, Methods Statement and Risk Assessment (Archaeology)
  - Soakaway Summary
  - Trail Pit Plan, Log and Infiltration Test Results
  - Visualisations
- 1.5 The proposal has gone through a number of variations during the assessment of the application to reduce and mitigate the impact on various factors including effects on the historic environment and ecological concerns. The initial proposal included an array of solar panels which were removed due to the impact on the historic environment and domestic turbines were subsequently proposed to power the buildings. These were also removed and replaced with roof mounted solar panels on the buildings to reduce the visual impact. The proposed site office / service building was also amended to a temporary structure to reduce the effect on the historic fabric of the remains of the former rifle range building. Further amendments were also sought in terms of access. The parking and turning layout was amended to reduce

the impacts on the natural environment, and the installation of a passing place was secured on Battery Road to alleviate concerns in relation to traffic flow.

## **2. SITE DESCRIPTION**

- 2.1 The site access and part of the route to the site along Battery Road lie within Castletown Settlement Development Area (SDA) as set out within the Caithness and Sutherland Local Development Plan (CaSPlan). However, the majority and main elements of the site lie within an area defined as Remote Rural using the Scottish Government's 6 fold urban-rural classification map 2016. The main body of the site consists of an access track and rough grazing ground located parallel to the north coast.
- 2.2 The site access would be taken from an upgraded junction at the south of Battery Road with the A836. It is proposed that the track will be improved, including the installation of a new passing place on Battery Road, a turning area and parking for up to 8 cars will be provided as you approach the main site. It is also proposed that there will be a recycling area and composting area to the west of the car parking area. It is proposed to include some partial bunded landscape to provide screening and minimise visual impact from sea views. The proposed bunding would be a maximum height of 1.5m, located to the northern boundary of the carpark to minimise the visual impact of the parking area on the historic setting.
- 2.3 A core path (Battery Walk CA03.01) runs along the southern boundary of the proposal, it is proposed to include hedge planting between the path and the field boundary over time with site generated compost. There are a number of field accesses along the route, these will be retained and kept clear during the construction and operation of the development.
- 2.4 The Castletown Batter, Battery 505m N of Tralorn, Scheduled Monument lies approximately to the north west boundary of the site. Castlehill broch 400m NNW of Kylerhea lies approximately 285m to the east of the site boundary.
- 2.5 The former rifle range building has collapsed on 2 sides and no longer has a roof.
- 2.6 The foreshore consists of a mix of sandy and shingle beaches and flagstone rock formations creating geos and rock pools. There are several access tracks giving way down to the foreshore with some more used and visible than others which are intended to be made good to improve general access onto the foreshore.

## **3. PLANNING HISTORY**

- 3.1 Advice Issued 22/00464/PREAPP Erection of 4 Eco Glamping Pods and associated solar array 01 April 2022

## **4. PUBLIC PARTICIPATION**

- 4.1 Advertised: Unknown Neighbour / Schedule 3 Development  
Date Advertised: 29 July 2022

Representation deadline: 12 August 2022

Timeous representations 9 Objections (from 6 Households) and 3 Support Comments

Late representations 4 Objections (from 4 Households) and 6 Support Comments

4.2 Material objection considerations raised are summarised as follows:

- a) Concerns over potential increase in traffic and all abilities pedestrian safety on Battery Road Junction and Battery Road, Castletown;
- b) Adverse impacts on residential amenity, including from noise and lighting;
- c) Adverse impact on ecology, habitat loss (including Great Yellow Bumblebee), butterflies (including Small Blue Butterfly), plants (native and non-native) and wildflowers (including Kidney Vetch);
- d) Adverse ornithological impacts;
- e) Adverse impact on cultural heritage;
- f) Adverse construction impacts;
- g) Adverse effects from coastal erosion or effects from the sea;
- h) Adverse drainage impacts;
- i) No on-site management;
- j) Adverse impact on existing agricultural and pedestrian access; and
- k) Limited socio-economic benefits.

4.3 Material support considerations raised are summarised as follows:

- a) Provide tourist accommodation and socio-economic benefits;
- b) Provide improved access at the junction between Battery Road and the A896 and the access track from Battery Road;
- c) Provide improved recreational access (including walking, surfing and cycling);
- d) The proposal demonstrates a suitably designed, sustainable and low impact development; and
- e) This type of development will reduce traffic as it promotes eco-tourism.

4.4 Non-material objection considerations raised are summarised as follows:

- a) Concerns that traffic heading in and out of Castletown are speeding;
- b) Lack of street lights;
- c) Increase in wild campers; and
- d) The use of ill-judged and inaccurately researched pseudo-vernacular terms in the development.

4.5 Non-material support considerations raised are summarised as follows:

- e) The proposed development would stop the area being used by wild campers.

4.6 All letters of representation are available for inspection via the Council's eplanning portal which can be accessed through the internet [www.wam.highland.gov.uk/wam](http://www.wam.highland.gov.uk/wam).

## 5. CONSULTATIONS

5.1 **Castletown and District Community Council** object to the application. It has concerns in relation to road safety, in particular the junction between Battery Road



and the A838 due to high-speed vehicles. The proposed development did not incorporate any further passing places and restricts pedestrian access due to the incorporation of a gate at the Bothies. It also has concerns that the site is not appropriate. Further concerns were raised with the proposed wind turbines, that have subsequently been removed from the proposal.

- 5.2 **Access Officer** does not object to the application. The core path which runs adjacent and through this development shall remain open for public and recreational use at all times during the construction and operation of the development. The wording of any permanent site signs should be approved by the Access Authority prior to installation to ensure the core path use is not restricted.
- 5.3 **Contaminated Land** do not object to the application, subject to an appropriate pre-commencement condition to ensure any potential contamination on site is dealt with appropriately. Its records indicate that the site has a former historic use as part of a former rifle range, which may have resulted in land contamination by way of elevated lead/metals and UXO. This may affect the proposed development depending on where the firing and target areas of the rifle range were in relation to the site layout plan.
- 5.4 **Ecology Officer** does not object to the application, subject to appropriate conditions. The Council's ecologist originally objected to the proposed development due to the lack of survey works that had been undertaken. The objection was removed after the Phase 1 Habitat Survey had been undertaken. This survey identified that the vast majority of the site is species rich coastal grassland. It should be noted that coastal grassland in the north of Scotland is quite a rare and ecologically rich habitat, that can support a number of rare plant and invertebrate species. Previously the site had been identified as being potentially important for the rare small blue butterfly which is a priority species within the Highland Nature Biodiversity Action Plan (HNBAP) and the UK Biodiversity Action Plan (UKBAP). However, no kidney vetch (the food source of the small blue butterfly) was identified during the survey. Although it is early in the season for many plants and has been a cold spring the surveyor is confident that it would have been visible on site. Due to the anecdotal evidence of this species being present there are recommendations within the report for a further survey later in the year, which is supported.

The Great Yellow Bumble Bee (GYBB) is a rare species of bumble bee that is only found in the northern Scottish Islands and northern mainland within Caithness and Sutherland. The GYBB is a priority species within the HNBAP and UKBAP. The Phase 1 Habitat Survey recorded an abundance of common knapweed, which is the GYBB's food source throughout the site.

The ecologist has requested that pre-construction survey work is undertaken (small blue butterfly and great yellow bumblebee), a Habitat Management Plan and checks for any nesting birds are made if construction works coincides within the main bird breeding season (March – August inclusive). All wild bird nests are protected from damage, destruction, interference and obstruction under the Wildlife and Countryside Act 1981 (as amended). Some birds (listed on schedule 1 of the Wildlife and Countryside Act) have heightened protection where it is also an offence to disturb these birds while they are in or around the nest.

A Habitat Management Plan (HMP) will be required to detail habitat management of the site including all mitigation, enhancement and management measures during the period of construction and operation of the development, to protect the species rich coastal grassland, and enhance the habitat for the small blue butterfly, the great yellow bumble bee and to ensure no adverse impacts upon the species rich coastal grassland on the site. The HMP must also include removal measures for the invasive plant species *Montbretia Crocosmia x crocosmiiflora*, found within the site.

- 5.5 **Environmental Health** do not object to the application, it did have some minor concerns in relation to noise with the proposed turbines that have now been removed from the proposed application. In terms of the composting toilets, they must not give rise to an odour at any residential or sensitive property. Furthermore, they should have adequate hand washing facilities. As the proposed development is connecting to the mains water supply this should not be an issue. The bothies are well away from any properties and the toilets should not give any rise to odour. In terms of the disposal of composted material, the same requirement applies that it must not impact on the neighbouring properties. Should there be a future issue with the composting hedge with odour then Environmental Health can deal with it in terms of Statutory Nuisance. An informative should be included with any permission that the bothies will come under the new mandatory licensing scheme for short term let properties under The Civic Government (Scotland) Act 1982 (Licensing of Short-term Lets) Order 2022. Any short-term let must comply with the licensing scheme and satisfy the licence conditions adopted by the Council for such properties.
- 5.6 **Flood Risk Management Team (FRMT)** do not object to the application. The location the predicted 1 in 100 year coastal flood level is 3.94mAOD. With climate change allowance of 0.89m and freeboard of 0.6m that means the proposed development (buildings and access) need to be above 4.83mAOD and the minimal finished floor level at 5.43mAOD. As this is the case the proposed site does not give rise to coastal flooding. From the site sections the buildings and access appear to be a minimal of between 7mAOD and maximum of 8mAOD.
- 5.7 **Historic Environment Team (Archaeology)** does not object to the application, subject to an appropriate condition to ensure a basic record of the current firing range building is made in advance of its redevelopment. This includes a programmed of work for the survey, evaluation, preservation and recording of any archaeological and historic features.
- 5.8 **Historic Environment Team (Conservation)** does not object to the application. It did initially raise an objection in relation to the initial proposal due to the impact on the setting of Castletown Battery, battery 505m N of Tralorn Schedule Monument (SM13625) and the insensitive development of the associated rifle-range structure. This objection was withdrawn due to the revised proposal. However, it noted that the proposed development of Caithness' coastline with holiday-pods is inappropriate. The coast is one of Caithness' key landscape and natural heritage assets, and the proposed development is not going to enhance this setting. It has concerns with the siting on this location as it is on a unique part of the coastline. In particular, the coastline has a 3-way combination of character types: historical (war defences); built heritage (the Battery SAM) and; the natural heritage of the coastline. The site is outwith Castletown settlement development area, within which there are more suitable locations for tourism accommodation development. Although HET do not

object to the proposed development it notes that the proposal fails to accord with the principles and policies contained within the HwLDP, Policies 48 and 57, that 'proposals should not have an unacceptable impact on the natural, built or cultural heritage and amenity value of the area'.

- 5.9 **Historic Environment Scotland (HES)** do not object to the application. The development site lies close to two archaeological sites that are recognised as being of national importance and designated as scheduled monuments under the Ancient Monuments and Archaeological Areas Act 1979: Castlehill, broch 400m NNW of Kyclerhea (SM 2189) and Castletown Battery, battery 505m N of Tralorn (SM 13625), but is not likely to have a significant, detrimental impact on their settings. HES confirm that the mitigation secured through the amendments to the application are suffice in order to ameliorate impacts on the scheduled monuments.
- 5.10 **NatureScot** were consulted but did not provide any comments as the proposal did not meet their criteria for consultation.
- 5.11 **Scottish Water** do not object to the application. There is currently sufficient capacity in the LOCH CALDER Water Treatment Works to service the development. There is also sufficient capacity for a foul only connection in the CASTLETOWN BATTERY Waste Water Treatment works to service the development should composting toilets not work.
- 5.12 **Transport Planning** : No objections

## 6. DEVELOPMENT PLAN POLICY

The following policies are relevant to the assessment of the application

### **National Planning Framework 4 2023**

- 6.1
1. Tackling the climate and nature crises
  2. Climate mitigation and adaptation
  3. Biodiversity
  4. Natural Places
  7. Historic Assets and Places
  10. Coastal Development
  14. Design Quality and Place
  30. Tourism

### 6.2 **Highland Wide Local Development Plan 2012**

- 28 - Sustainable Design
- 29 - Design Quality and Place-making
- 31 - Developer Contributions
- 34 - Settlement Development Areas
- 36 - Development in the Wider Countryside
- 43 - Tourism
- 44 - Tourist Accommodation
- 49 - Coastal Development
- 56 - Travel
- 57 - Natural, Built and Cultural Heritage
- 58 - Protected Species

- 59 - Other important Species
- 60 - Other Importance Habitats
- 61 - Landscape
- 64 - Flood Risk
- 65 - Waste Water Treatment
- 66 - Surface Water Drainage
- 77 - Public Access

### 6.3 **Caithness and Sutherland Local Development Plan (CaSPlan) 2018:**

- Castletown Settlement Development Area (SDA) – Much of Castletown was planned on a grid pattern and built during its time at the centre of a thriving flagstone industry in the 19th Century. The shore front at Castlehill was pivotal to the production and transportation of the flagstone which was exported all over the world. Nowadays the harbour is only used by several small boats and many of the buildings along the coast line have fallen into ruin. This rich history remains clearly visible in the landscape and there is great potential to convert and redevelop the area into a well defined tourism destination. Some of the placemaking priorities include opportunities to provide high quality tourism facilities along the shore front which could provide an anchor for further development, whilst enhancing green corridors surrounding the village and protect these areas from development.
- Special Landscape Areas (SLA) of particular relevance is Dunnet SLA located approximately 840m southeast of the site. The western boundary of Dunnet SLA was extended more recently to include all of Dunnet Bay.

### 6.4 **Highland Council Supplementary Planning Policy Guidance**

- Developer Contributions (March 2018)
- Flood Risk and Drainage Impact Assessment (Jan 2013)
- Green Networks (Jan 2013)
- Highland Historic Environment Strategy (Jan 2013)
- Highland's Statutorily Protected Species (March 2013)
- Managing Waste in New Developments (March 2013)
- Special Landscape Area Citations (June 2011)
- Standards for Archaeological Work (March 2012)
- Sustainable Design Guide (Jan 2013)

## 7. **OTHER MATERIAL POLICY CONSIDERATIONS**

### 7.1 **Other Guidance**

- Creating Places (Scottish Government 2013)
- Historic Environment Policy for Scotland (HEPS, 2019)
- PAN 60 – Planning and Natural Heritage (Jan 2008)
- PAN 61 – Sustainable Urban Drainage Systems
- PAN 2/2011 – Planning and Noise

## **8. PLANNING APPRAISAL**

- 8.1 Section 25 of the Town and Country Planning (Scotland) Act 1997 requires planning applications to be determined in accordance with the development plan unless material considerations indicate otherwise.

### **Determining Issues**

- 8.2 This means that the application requires to be assessed against all policies of the Development Plan relevant to the application, all national and local policy guidance and all other material considerations relevant to the application.

### **Planning Considerations**

- 8.3 The key considerations in this case are:
- a) compliance with the development plan and other planning policy;
  - b) siting, design and amenity;
  - c) access, parking and servicing;
  - d) water, flood risk and drainage;
  - e) natural heritage;
  - f) built and cultural heritage;
  - g) developer contributions; and
  - h) any other material considerations.

### **Development plan/other planning policy**

- 8.4 NPF4 Policies 1-3 apply to all development proposals nationwide. When considering all development proposals, significant weight will be given to the global climate and nature crises. Development proposals will be sited and designed to minimise lifecycle greenhouse gas emissions as far as possible. Development proposals will contribute to the enhancement of biodiversity, including where relevant, restoring degraded habitats and building and strengthening nature networks and the connections between them. Proposals should also integrate nature-based solutions, where possible. Policy 4 (NPF4) re-enforces these policies in that it protects biodiversity and natural assets, which in turn play a crucial role in carbon reduction whilst protecting and enhancing natural heritage; Policy 10 (NPF4) encourages natural solutions to coastal protection and will support development in coastal areas that takes into account future vulnerability to climate change. Policy 7 (NPF4) is relevant to this proposal as it protects and enhances the historic environment assets and places whilst supporting the transition to net zero. It also recognises the social, environmental and economic value of the historic environment, to our economy and cultural identity. Policy 7 states that development proposals affecting schedule monuments will only be supported where:
- i. direct impacts on the scheduled monument are avoided;
  - ii. significant adverse impacts on the integrity of the setting of a scheduled monument are avoided; or

- iii. exceptional circumstances have been demonstrated to justify the impact on a scheduled monument and its setting and impacts on the monument or its setting have been minimised.

8.5 Highland wide Local Development Plan (HwLDP) Policy 28 requires sensitive siting and high-quality design in keeping with local character and historic and natural environments, and the use of appropriate materials. This Policy also requires proposed developments to be assessed on the extent to which they are compatible with service provision, as well as their impact on individual and community residential amenity. Policy 29 repeats the emphasis on good design in terms of compatibility with the local landscape setting and in terms of place making. Policy 31 requires an assessment for Developer Contributions. The principal application site is not located within a main or a growing settlement as defined by the CaSPlan. As such the site is located within 'wider countryside' and so HWLDP Policy 36 applies. Policy 36 and the adopted Rural Housing guidance supports development proposals if they are not judged to be significantly detrimental in terms of the following matters: siting and design, pattern of development, landscape character, impact on locally important croftland and servicing, particularly roads.

8.6 NPF4 Policy 30 is the main policy against which the planning assessment is based. The intent of Policy 30 is to encourage, promote and facilitate sustainable tourism development which benefits local people, is consistent with net zero and nature commitments, and inspires people to visit Scotland. HWLDP Policies 43 and 44 are also relevant and follow the same general direction as NPF4 Policy 30. Proposals for tourist facilities and accommodation will, similarly, be assessed in terms of their scale and fit with the settlement pattern of the area. Proposals are encouraged that would increase the length of peoples stay, increase visitor spending or promote a wider spread of visitors and would safeguard, promote responsible access, interpretation and effective management or enhancement of natural, built and cultural heritage features. All proposals should avoid siting on the better part of a croft in terms of its agricultural value and minimise the impact of development on the continued working of the holding. Policies 65 and 66 require foul and surface water drainage to meet standards that minimise the risk of pollution and flooding.

NPF4 policy 10 'Coastal Development' is also of relevance. The area of coastline is categorised as 'undeveloped' within the Highland Council's Coastal Development Strategy (2010). As such, Policy 10 b) is taken to apply. This sets out that support is only given where the development supports the blue economy and does not require further coastal defences.

### **Principle of Development**

8.7 Under both NPF4 and HwLDP policy, tourist accommodation within the wider countryside will be supported where: it can be demonstrated that a demand exists for this type of accommodation; and where it can be achieved without adversely affecting the local landscape character or amenity. Proposals for tourism related development will take into account the contribution made to the local economy, compatibility with the surrounding area in terms of the nature and scale of then activity and impacts of increased visitors, impacts on communities, for example by hindering the provision of homes and services for local people; opportunities for sustainable travel and appropriate management of parking and traffic generation and

scope for sustaining public transport services particularly in rural areas; accessibility for disabled people; measures taken to minimise carbon emissions and opportunities to provide access to the natural environment.

- 8.8 The accommodation proposed is generally unsuitable for permanent residential use, by the virtue of design, site layout, lack of garden and the shared use of communal facilities, being serviced off a single parking courtyard area, with a communal refuse bin store. Due to the siting and spacing the units would not have individual curtilages or amenity space. As such, the form of accommodation being proposed, although capable of being occupied independently, is clearly designed as holiday letting accommodation and as such, considering the location outwith the Castletown Settlement Development Area (SDA), would not conflict with or hinder the provision of homes for local people. A condition is also attached in this respect, to ensure that the proposed units remain as tourist accommodation as applied for.
- 8.9 The applicant has put forward a high-quality year-round off grid bothy scheme, providing market research as part of their design statement that indicates sustained demand for self-catering tourist accommodation within the Highland Council area. It is accepted that the proposal is located close to the North Coast 500 (NC500) tourist route, which provides access to Castletown as a popular tourist destination its own right while also acting as a gateway for onward travel on the North Coast 500 tourist route. Demand for tourist accommodation is evident and may help to lengthen visitor's stay within the area, contributing to the local economy and providing employment opportunities.

### **Siting, Design and Amenity**

- 8.10 The proposal includes a service building that would be located on site of the former rifle range building. The building will be prefabricated, timber clad with a turf roof. It includes an office reception, store and toileting facilities, with a grey water soakaway. The proposed No.4 bothy type holiday accommodation structures will be sited in a linear row to the north side of the core path, with open views across the north coast and Dunnet Bay SLA. Each bothy is designed with a compact dual pitched timber clad structure with turf roof, solar panels and flue as each pod includes a wood fired stove. Each also includes a private deck area with external shower and composting toilet cubicle located on the shore side towards the north to maximise privacy from the core path towards the south. The cubicles include timber enclosures integrated into the decking.
- 8.11 It is proposed that the water will be heated via a kettle or the wood fired stoves. The electrics will be provided via roof mounted solar PV array with battery and inverter for internal lighting and charging personal devices. The prefabricated construction uses a materially and thermally efficient system built at the Cruive base in Fife and transported to Caithness to ensure a high-quality product, which minimises waste and construction impact on the site.
- 8.12 The applicant has proposed some screening with bunding of the proposed car parking, turning, communal bins and composting areas from the coastal views through earth bunding. Whilst no screening has been provided for the bothies, they are considered low impact, with the turf roofs blending them into the environment and reducing visual and landscape effects, particularly from Dwarwick Peir and the

opposite side of Dunnet Bay (Dunnet SLA). Whilst the Historic Environment Team had concerns in relation to the siting of the bothies, the landscape and visual effects have been significantly reduced through the removal of the solar array, then the subsequent removal of five 6m turbines located adjacent to each building through negotiations with the agent. The service building was also amended to a temporary structure to allow the preservation of the remains of the rifle building structure, protecting the historic fabric. As such public views of the site will be very limited with the bothies only being visible from the coastal path.

- 8.13 Furthermore, it is proposed that minimal landscaping will take place, although there will be a requirement for the compensatory planting, this will be conditioned to include native, wildflowers, plants and hedging due to the loss of some habitat. This will be secured through the Habitat Management Plan (HMP). The limited construction works onsite and the offgrid bothies together reduce the impact of the proposed development to an acceptable level in terms of visual and landscape effects.
- 8.14 Although the development will introduce a greater level of activity in the vicinity of the site, particularly during peak season when the units may be at full occupancy. The closest property to the site is approximately 452m from the office/service building and approximately 527m from the closest bothy. There are no other properties in the close vicinity, there are other residential properties located on Battery Road and within the main village of Castletown, and on much higher ground. It is considered that the site is sufficiently remote from residential properties to alleviate any impact upon individual residential amenity in terms of noise, odour, disturbance or privacy/overlooking. Any lighting will be minimal, reducing glare and the risk of any significant lighting pollution. Torches are to be made available to users for wayfinding.
- 8.15 It is considered that whilst the development introduces modern features to this historic landscape, the development will add character to the area, creating positive spaces, whilst protecting the cultural heritage, enhancing the natural heritage and bringing economic benefits.

### **Access, Parking and Servicing**

- 8.16 The proposal will take vehicle access off Main Street (A836) and Battery Road, leading to a track that extends down to the west end of the site where the parking and turning area is proposed. The junction is within the 30mph speed limit. The applicant has proposed that visibility splays of 2.4m x 70 to the left and 2.4m x 120m to the right could be formed around the upgraded access junction. A condition is attached to secure that the access is formed to the correct standard in advance of development commencing. The applicant has shown eight parking spaces, within the site. The Transport Planning Team were consulted but did not raise any objection to the proposed development. The central turf tracks will be largely maintained to help provide habitat for the Small Blue Butterfly should it be found to occupy the area, as it is known to be nearby.
- 8.17 Concerns have been raised in relation to traffic safety, in particular the proximity of the A836 to Battery Road and the junction not being sufficient to cope with the increase in capacity. The junction is in poor condition and the development proposes to make improvements to the junction, through widening and resurfacing it. This will



improve the visibility splays and space thus improving road safety. As set out above this will be secured by condition. It should be noted the bothies will be constructed off site which will minimise wastage and construction traffic impacts on site, screw pile foundations will also be used to minimise construction works and the impact of ground works.

- 8.18 Part of the 1.9km Battery Walk (CA03.01) core path runs through the site, providing access to the coastal walk and heritage of the area. It is proposed that appropriate signage will be erected to raise awareness of both walkers and guests of the core paths, these will be secured through a planning conditioning for approval in consultation with the Access Officer. It is considered that the proposed development would improve outdoor access between the two core paths, the historic environment and the coast. However, it should be noted that any upgrades to the paths will be minimal to ensure that the development remains a low impact development.
- 8.19 Concerns were raised in relation to there being gates installed, restricting access for outdoor recreational users and tourists who visit the scenic walk and cultural heritage in the area. However, the applicant has confirmed within their amended Supporting Information that there will be no gates at either of the access points to the proposed development. Furthermore, information will be provided to guests on their obligations and best practice to stay safe around active farmland with livestock. Similarly, guidance will be available for the safe use of the beach and foreshore.
- 8.20 The applicant has not provided any bicycle storage, this will be secured via an appropriate planning condition for a minimum of 8 bicycles, the bothies would provide the opportunity for guests to utilise the public and active travel options available on the edge of Castletown.

### **Water, Flood Risk and Drainage**

- 8.21 The proposal will connect into the mains water supply. As the development is proposed to be eco-friendly it includes dry composting toilets, with grey water treatment and soakaways. Composting toilets are provided to minimise water use and need for a sewerage treatment plant. The compost will be used on site for planting a hedge along the field boundary which will enhance and promote biodiversity, provide habitat that supports the Great Yellow bumblebee (*Bombus distinguendus*, GYBB).
- 8.22 The applicant has undertaken trial pit testing for the proposed sustainable drainage soakaways to facilitate grey water and surface water soakaways, with percolation tests indicating ground conditions are suitable. This is therefore considered to be acceptable.
- 8.23 While the representation comments regarding the drainage arrangements are noted, the application site has not raised any objection or concern from the Council's Flood Risk Management Team, nor is the site located within an area considered at risk of flooding on the SEPA maps. The feasibility of the proposed drainage infrastructure will require further scrutiny by Building Standards as a building warrant would be required.

## Natural Heritage

- 8.24 A protected species survey was undertaken in May 2022, including badgers, bats, otters, breeding birds and insects. Further to this a Stage 2 bat activity survey was carried out with no bats recorded leaving the structure or flying in the general area. The survey noted no sign of badger or bats. Two potential otter holts were located within 200m of the site boundary, with a further five lie-ups located along the edge of the site, two which contained signs of otters. Pre-construction surveys will be required for otters, within two months of the start of the works. The survey recommended a range of mitigation measures for otters, these should be included within a Habitat Management Plan (HMP) that will be secured through an appropriate planning condition.
- 8.25 Representations were received in relation to the disturbance of ground nesting birds (curlews, lapwing, ringed plover, skylark, common gull). The applicant's survey identified a small number of bird species suspected as breeding birds on site, including linnet, meadow pipit and reed bunting. The survey noted that the development is not designed to alter the habitats on site significantly, and therefore there will be space for breeding birds. Appropriate mitigation will be secured via the HMP.
- 8.26 Representations were made due to the loss of some valuable habitat for the Great Yellow bumblebee (*Bombus distinguendus*, GYBB), including the impacts of the smoke generated from the proposed wood burning stoves in the bothies.
- 8.27 The Bumblebee Conservation Trust and the Council's Ecologist also raised concerns as the site represents one of the best naturally flower-rich habitats on mainland Scotland used by the rare Scottish priority species. Habitat connectivity across the north coast is vital to the continued existence of the GYBB, and the Battery Road habitat links populations of this rare and vulnerable species between Thurso and Dunnet. As a result the current distribution of GYBB reflects the distribution of flower-rich machair and locations where traditional crofting practices are still maintained.
- 8.28 Although the bees are not a protected species, they are a valued asset to the area and are a rare species. The GYBB are on the Highland Nature Biodiversity Action Plan (HNBAP) and UKBAP as a priority species so is of high conservation concern and the Council must give it protection under policy 59 of the HWLDP. A condition will be imposed to ensure that the available habitat is increased for the GYBB as part of the development through the HMP. On that basis if any habitat is lost through this development, it should be compensated for, over and above. Taking this into account and the compensatory planting of a combination of native plants that will be approved as part of the HMP in consultation with the Council's Ecologist the development would not have a significant impact on the GYBB population. The HMP will ensure that planting is undertaken and maintained so that the proposed development does not represent a threat to the viability of the north Caithness GYBB population.
- 8.29 Concerns had been raised in relation to the scraping and widening of the existing access track which may have resulted in the loss of wildflowers including kidney vetch. However, no kidney vetch (the food source of the small blue butterfly) has been identified during the most recent survey works. Due to the anecdotal evidence

of this species being present there are recommendations within the report for a further survey later in the year. The Council's ecologist is satisfied this can be secured through the HMP.

- 8.30 The planting of indigenous hardy plants raised concerns, as five of the proposed species of plants are not native to the UK (*Rosa rugosa*) and one is not native to Scotland (sea buckthorn). Both these species are considered invasive in some areas. Furthermore, although native, gorse is considered invasive in some areas. The species of plants will be native to the area and secured via the HMP.

### **Cultural Heritage**

- 8.31 The Castletown Battery is located immediately adjacent to the western boundary of the development site. The coastal artillery battery was constructed around 1866, it is now visible as a mortared stone wall and earth bank, with two cannon embrasures. A small sunken chamber (likely the powder room) lies at the east end of the battery and a rectangular building at the west. The battery is located on the coast overlooking Dunnet Bay. The monument was constructed in response to the perceived threat of a French invasion and manned by a local volunteer artillery battalion. The cultural significance of the monument is vested in its survival as one of best-preserved examples of late 19<sup>th</sup>-century artillery batteries, and it sits within a coastal landscape that is little altered from when the battery was built. Historic Environment Scotland notes that its setting is primarily functional and related to its position on an open headland with clear outward views covering the approaches to Dunnet Bay, the earth bulwark means that the battery is reasonably concealed when viewed from the sea. These clear outward views and its relatively hidden nature, amongst the cliffs and rural background in inner views from the sea, form key characteristics of its setting, and retention of these is important to an understanding and appreciation of the monument's cultural significance.
- 8.32 The remains of the Rifle Range are an integral part of the military heritage of Castletown and the Parish of Olig. The Castletown Heritage Society have raised concerns that the Repurposing of the former rifle range and associated building would affect the visual memory and physical memory of this part of the facility that played in the defence of the area, and render site visits impossible. It suggested that a full archaeological survey of the site should be undertaken, laying out any artefacts relating to the facility, this was also requested by the Council's archaeologist and will be secured via a planning condition. Concerns were also raised in relation to the proximity to the Castletown Battery, battery 505 N of Tralorn Scheduled Monument (SM13625), due to the increased number of visitors that may lead to degradation over time of the late nineteenth century artillery battery within a coastal landscape. However, it is considered that many visitors to the area would be interested in the history of the area. Therefore, it is not anticipated that the proposed development would significantly increase the number of visitors to an extent that it would deteriorate the landscape to such that the historic nature could not be appreciated.
- 8.33 The Castlehill broch is located approximately 300m south-east of the development site. It contains the remains of a broch, a complex stone-built substantial roundhouse, dating from the Iron Age (between 600 BC and AD 400). The mound is located about 7m above sea level, 20m inland from the shore of Dunnet Bay. When

originally built in the Iron Age, the broch would have been an impressive drystone tower that likely served a domestic yet defensive function for an extended family group. Over time the broch has collapsed, leaving a large grass-covered mound, although it is likely that an element of stone-robbing has taken place over time. The cultural significance of the monument is vested not only in its survival as a buried broch with a high potential for a complex sequence of buried remains, but also in its contextual value as being part of a wider network of brochs across Caithness and as a feature in the local landscape.

- 8.34 The setting of brochs often includes a visual relationship to other broadly contemporary Iron Age sites in the landscape, and there are a number of other scheduled brochs in the Castletown area that all form part of each other's' settings. Brochs were often positioned in locally prominent parts of the landscape, exploiting small changes of topography so as to have oversight of the surrounding lands and of passage through them, or in coastal locations where they could exert an element of authority over the maritime realm.
- 8.35 Whilst the proposed bothies would probably be visible, either wholly or in part, from both monuments, HES are content that the volume, massing and external treatment of each bothy will mean that they are unlikely to either visually challenge from the local prominence of the broch mound or detract significantly from the open unfettered coastal landscape that forms part of the setting of the battery in inward views from the sea. The formation of an office/service building via the repurposing of the firing range building is likewise unlikely to have a significant adverse impact on the setting of either monument as the massing and footprint of the replacement structure is not too dissimilar from what is there already, and therefore the visual impact of the change would be minimal. HES noted that the proposed parking area and turning circle at the western end of the site is likely to have a minor adverse impact on the setting of Castletown Battery, less through its formation but more through its use. This was removed during the determination of the application to reduce these impacts. Part of the cultural significance of the monument is the intactness of its setting, so having vehicles routinely parked here will alter the character of the immediate landscape setting of the battery and has the potential to draw the eye towards this section of coastline in inward views from the sea, and given that the battery is reasonably concealed from the sea this could therefore have an adverse impact on its setting. HES also note that consideration has been given to mitigating this impact through the formation of a 1.5m high earth bund to the immediate seaward side of the proposed parking area. This should have the effect of lessening the visual impact of parked vehicles in inward views of Castletown Battery from the sea that form part of the monument's setting. Provided that this is implemented, HES are content that the severity of any impacts on the setting of the monuments are not likely to be sufficiently adverse as to raise issues of national significance.
- 8.36 Given the above, whilst there will a small change to the character of the setting of both monuments, it is not of a severity to challenge the legibility of either monument's position within its landscape any more so than what is already there. As such, the proposed development is unlikely to have a significant adverse impact on the setting of the monuments and is considered to be acceptable provided the proposed mitigation is secured through planning conditions.

## **Developer Contributions**

- 8.37 Policy 31 requires that, where developments create a need for new or improved public services, facilities or infrastructure, the developer makes a fair and reasonable contribution in cash or kind towards these additional costs or requirements. As tourist letting accommodation, the proposals are not eligible for developer contributions towards local educational capacity. However, as the development is adjacent to a site of cultural and historic significance, contributions towards public art are justified in this case. The additional signage required for the core paths and the display or artefacts and/or display panel, will be secured via a specific condition.

## **Other material considerations**

- 8.38 There are no other material considerations.

## **Non-material considerations**

- 8.39 The issue of speeding traffic, lack of street lighting (in this instance), wild campers and names of the bothies are not a material planning consideration.

## **Matters to be secured by Legal Agreement / Upfront Payment**

- 8.40 None

## **9. CONCLUSION**

- 9.1 The demand for the proposed tourist accommodation is accepted considering the location of the proposals. It is considered that the proposed low impact bothies and the associated supporting infrastructure can be accommodated satisfactorily on the site without detriment to the local visual amenity.
- 9.2 All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

## **10. IMPLICATIONS**

- 10.1 Resource: Not applicable
- 10.2 Legal: Not applicable
- 10.3 Community (Equality, Poverty and Rural): Not applicable
- 10.4 Climate Change/Carbon Clever: Biodiversity gains through a Habitat Management Plan (HMP)
- 10.5 Risk: Not applicable
- 10.6 Gaelic: Not applicable

## 11. RECOMMENDATION

### Action required before N decision issued

**Subject to the above actions**, it is recommended to **GRANT** the application subject to the following conditions and reasons

1. The development to which this planning permission relates must commence within THREE YEARS of the date of this decision notice. If development has not commenced within this period, then this planning permission shall lapse.

**Reason:** In accordance with Section 58 of the Town and Country Planning (Scotland) Act 1997 (as amended).

2. No development or work (including site clearance) shall commence until a programme of work for the evaluation, preservation and recording of any archaeological and historic features affected by the proposed development/work, including a timetable for investigation, all in accordance with the attached specification, has been submitted to, and approved in writing by, the Planning Authority. The approved programme shall be implemented in accordance with the agreed timetable for investigation.

**Reason:** In order to protect the archaeological and historic interest of the site.

3. No development shall commence until full details of a covered and secure communal bicycle storage/racking system for 8 bicycles have been submitted to, and approved in writing by, the Planning Authority. Thereafter, the storage/racking system shall be installed in accordance with these approved details prior to the first occupation of the development hereby approved.

**Reason:** In order to facilitate the use of a variety of modes of transport.

4. No development shall commence until a Habitat Management Plan (HMP) has been submitted to, and approved in writing by, the Planning Authority, in consultation with the Council's Ecology Officer, providing for measures to protect and manage habitat and species within and adjoining the application site. The approved HMP, which shall be implemented in full unless otherwise agreed in writing, shall include:

- i. pre-commencement surveys including otters, great yellow bumble bee, small blue butterfly and breeding birds,
- ii. all mitigation, enhancement and management measures, during the period of construction and operation of the development, to protect the species rich coastal grassland, and enhance the habitat for the small blue butterfly and the great yellow bumble bee (including native planting or wildflowers and hedging);
- iii. all mitigation enhancement and management measures, during the period of construction and operation of the development, to protect otters and ground nesting birds;

- iv. removal measures for the invasive plant species Montbretia Crocosmia x crocosmiiflora, found within the site; and
- v. include provision for regular monitoring and review to be undertaken against the HMP objectives and measures for securing amendments or additions to the HMP in the event that the HMP objectives are not being met.

Unless and until otherwise agreed in advance in writing with the Planning Authority, the approved HMP (as amended from time to time with written approval of the Planning Authority) shall be implemented in full.

**Reason:** To protect and enhance the nature conservation interests of the area, including the management of vegetation, mitigate any effects on habitat and avoid adverse effects on species of nature conservation interest.

5. No development shall commence until a scheme to deal with potential contamination on site has been submitted to and agreed in writing by the Planning Authority. The scheme shall include:
- i. the nature, extent and type of contamination on site and identification of pollutant linkages and assessment of risk (i.e. a land contamination investigation and risk assessment), the scope and method of which shall be submitted to and agreed in writing by with the Planning Authority, and undertaken in accordance with PAN 33 (2000) and British Standard BS 10175:2011+A2:2017 Investigation of Potentially Contaminated Sites - Code of Practice;
  - ii. the measures required to treat/remove contamination (remedial strategy) including a method statement, programme of works, and proposed verification plan to ensure that the site is fit for the uses proposed;
  - iii. measures to deal with contamination during construction works;
  - iv. in the event that remedial action be required, a validation report that will validate and verify the completion of the agreed decontamination measures;
  - v. in the event that monitoring is required, monitoring statements shall be submitted at agreed intervals for such time period as is considered appropriate by the Planning Authority.

Thereafter, no development shall commence until written confirmation has been received that the scheme has been implemented, completed and, if required, monitoring measurements are in place, all to the satisfaction of the Planning Authority.

**Reason:** In order to ensure that the site is suitable for redevelopment given the nature of previous uses/processes on the site.

6. No development or work (including site clearance) shall until a Heritage Interpretation and Access Plan has been submitted to, and approved in writing by, the Planning Authority. The Plan shall:

- i. identify all historic sites within and/or adjacent to the application site boundary which would benefit from improved access and interpretation; and
- ii. outline proposals for the improvement of identified historic sites which should include interpretative resources, signage, footpath provision and the promotion of a Heritage Trail; and
- iii. include proposals for implementation of the Plan.

The approved Heritage and Access Plan shall be implemented prior to the first use of the development hereby approved, or, if different, in accordance with the approved Plan.

**Reason:** In order to promote the archaeological/historical interest of the site.

7. No other development shall commence until the site access between Battery Road and Main Street (A836) has been upgraded in accordance with The Highland Council's Access to Single Houses and Small Housing Developments guidelines with:
  - i. the junction formed to comply with drawing ref. G03 REV D (Proposed Site Layout Plan); and
  - ii. visibility splays of 2.4m x 70m to the left and 2.4m x 120m to the right (the X dimension and Y dimension respectively) in each direction formed from the centre line of the junction.

Within the stated visibility splays, at no time shall anything obscure visibility between a driver's eye height of 1.05m positioned at the X dimension and an object height of 0.60m anywhere along the Y dimension.

**Reason:** To ensure that an adequate level of access is timeously provided for the development; in the interests of road safety and amenity.

8. The development shall be used for holiday letting purposes only and the approved pods shall not be used as a principal private residence or be occupied by any family, group or individual for more than three months (cumulative) in any calendar year.

**Reason:** To ensure that the development does not become used for permanent residential occupation in recognition of the lack of private amenity space and in accordance with the use applied for.

9. The parking and turning provision shown on the approved plans shall be provided prior to the first use of the development and thereafter maintained in perpetuity.

**Reason:** In order to ensure that the level of off-street parking is adequate.

10. The new passing place located on Battery Road as indicated on the approved plans shall be constructed in full prior to the first occupation of the development hereby approved.



**Reason:** In order to ensure the safety and free flow of traffic on the public road.

11. Public access to the Core Path within, or adjacent to, the application site shall at no time be obstructed or deterred by construction-related or operational activities, unless otherwise approved in writing by the Council's Access Officer as a temporary measure required for health and safety or operational purposes. Under such circumstances, any temporary obstruction or deterrent shall cover only the smallest area practicable and for the shortest duration possible, with waymarked diversions provided as necessary.

**Reason:** In order to safeguard public access during the construction and operation of the development.

12. Any existing access through the application site to the adjacent croftland shall at no time be obstructed or deterred by construction-related or operational activities, and shall be maintained in perpetuity.

**Reason:** To ensure that the development will not impede access to, or have an adverse impact on the purposeful use of, another part of the croft or other croftland.

13. Prior to the first occupation of the development directional signage will be installed that clearly identifies the core path. Details of all such signage shall be submitted to, and approved in writing by, the Planning Authority prior to development commencing. All approved signage shall be erected prior to the first occupation of the development.

**Reason:** In the interests of pedestrian safety.

14. Prior to the first occupation of the development warning signage will be installed to alert visitors to the area of the presence of otters. Details of all such signage shall be submitted to, and approved in writing by, the Planning Authority prior to development commencing. All approved signage shall be erected prior to the first occupation of the development.

**Reason:** To minimise disturbance to nature conservation interests and ensure the protection of protected species and habitats.

## **REASON FOR DECISION**

The demand for the proposed tourist accommodation is accepted considering the location of the proposals. It is considered that the proposed low impact bothies and the associated supporting infrastructure can be accommodated satisfactorily on the site without detriment to the local visual amenity.

## **INFORMATIVES**

### **Initiation and Completion Notices**

The Town and Country Planning (Scotland) Act 1997 (as amended) requires all developers to submit notices to the Planning Authority prior to, and upon completion of, development. These are in addition to any other similar requirements (such as

Building Warrant completion notices) and failure to comply represents a breach of planning control and may result in formal enforcement action.

1. The developer must submit a Notice of Initiation of Development in accordance with Section 27A of the Act to the Planning Authority prior to work commencing on site.
2. On completion of the development, the developer must submit a Notice of Completion in accordance with Section 27B of the Act to the Planning Authority.

Copies of the notices referred to are attached to this decision notice for your convenience.

### **Environmental Health**

The Civic Government (Scotland) Act 1982 (Licensing of Short-term Lets) Order 2022 introduced a new mandatory licensing scheme for short term let properties on 1st October 2022. Any short term let properties must comply with the licensing scheme and satisfy the licence conditions adopted by the Council for such properties. The licensing scheme requires new short term lets to be licensed by the Council prior to being occupied. Existing operators have until 1st October 2023 to apply for a licence. Further information on short term lets can be found on the Highland Council website [www.highland.gov.uk](http://www.highland.gov.uk)

### **Flood Risk**

It is important to note that the granting of planning permission does not imply there is an unconditional absence of flood risk relating to (or emanating from) the application site. The granting of planning permission does not remove the liability position of developers or owners in relation to flood risk.

### **Scottish Water**

You are advised that a supply and connection to Scottish Water infrastructure is dependent on sufficient spare capacity at the time of the application for connection to Scottish Water. The granting of planning permission does not guarantee a connection. Any enquiries with regards to sewerage connection and/or water supply should be directed to Scottish Water on 0845 601 8855.

### **Septic Tanks and Soakaways**

Where a private foul drainage solution is proposed, you will require separate consent from the Scottish Environment Protection Agency (SEPA). Planning permission does not guarantee that approval will be given by SEPA and as such you are advised to contact them direct to discuss the matter (01349 862021).

### **Local Roads Authority Consent**

In addition to planning permission, you may require one or more separate consents (such as road construction consent, dropped kerb consent, a road openings permit, occupation of the road permit etc.) from the Area Roads Team prior to work

commencing. These consents may require additional work and/or introduce additional specifications and you are therefore advised to contact your local Area Roads office for further guidance at the earliest opportunity.

Failure to comply with access, parking and drainage infrastructure requirements may endanger road users, affect the safety and free-flow of traffic and is likely to result in enforcement action being taken against you under both the Town and Country Planning (Scotland) Act 1997 and the Roads (Scotland) Act 1984.

Further information on the Council's roads standards can be found at: <http://www.highland.gov.uk/yourenvironment/roadsandtransport>

Application forms and guidance notes for access-related consents can be downloaded from:

[http://www.highland.gov.uk/info/20005/roads\\_and\\_pavements/101/permits\\_for\\_working\\_on\\_public\\_roads/2](http://www.highland.gov.uk/info/20005/roads_and_pavements/101/permits_for_working_on_public_roads/2)

### **Mud and Debris on Road**

Please note that it is an offence under Section 95 of the Roads (Scotland) Act 1984 to allow mud or any other material to be deposited, and thereafter remain, on a public road from any vehicle or development site. You must, therefore, put in place a strategy for dealing with any material deposited on the public road network and maintain this until development is complete.

### **Construction Hours and Noise-Generating Activities**

You are advised that construction work associated with the approved development (incl. the loading/unloading of delivery vehicles, plant or other machinery), for which noise is audible at the boundary of the application site, should not normally take place outwith the hours of 08:00 and 19:00 Monday to Friday, 08:00 and 13:00 on Saturdays or at any time on a Sunday or Bank Holiday in Scotland, as prescribed in Schedule 1 of the Banking and Financial Dealings Act 1971 (as amended).

Work falling outwith these hours which gives rise to amenity concerns, or noise at any time which exceeds acceptable levels, may result in the service of a notice under Section 60 of the Control of Pollution Act 1974 (as amended). Breaching a Section 60 notice constitutes an offence and is likely to result in court action.

If you wish formal consent to work at specific times or on specific days, you may apply to the Council's Environmental Health Officer under Section 61 of the 1974 Act. Any such application should be submitted after you have obtained your Building Warrant, if required, and will be considered on its merits. Any decision taken will reflect the nature of the development, the site's location and the proximity of noise sensitive premises. Please contact [env.health@highland.gov.uk](mailto:env.health@highland.gov.uk) for more information.

### **Protected Species – Halting of Work**

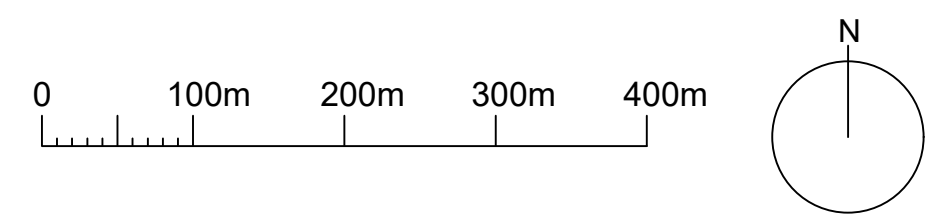
You are advised that work on site must stop immediately, and NatureScot must be contacted, if evidence of any protected species or nesting/breeding sites, not previously detected during the course of the application and provided for in this permission, are found on site. For the avoidance of doubt, it is an offence to deliberately or recklessly kill, injure or disturb protected species or to damage or destroy the breeding site of a protected species. These sites are protected even if

the animal is not there at the time of discovery. Further information regarding protected species and developer responsibilities is available from NatureScot: <https://www.nature.scot/professional-advice/protected-areas-and-species/protected-species>

Signature: Dafydd Jones  
Designation: Area Planning Manager - North  
Author: Claire Farmer  
Background Papers: Documents referred to in report and in case file.  
Relevant Plans:

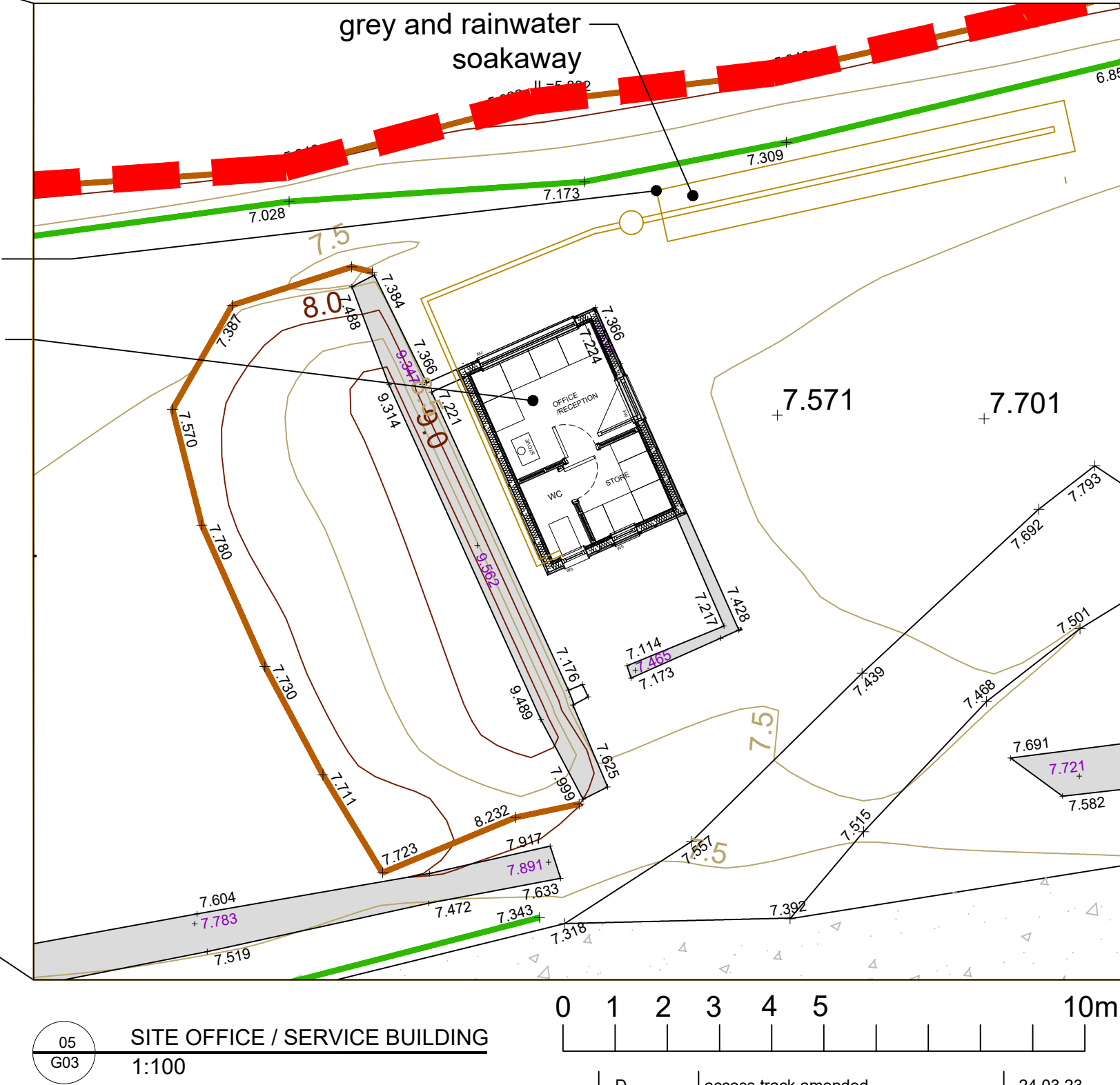
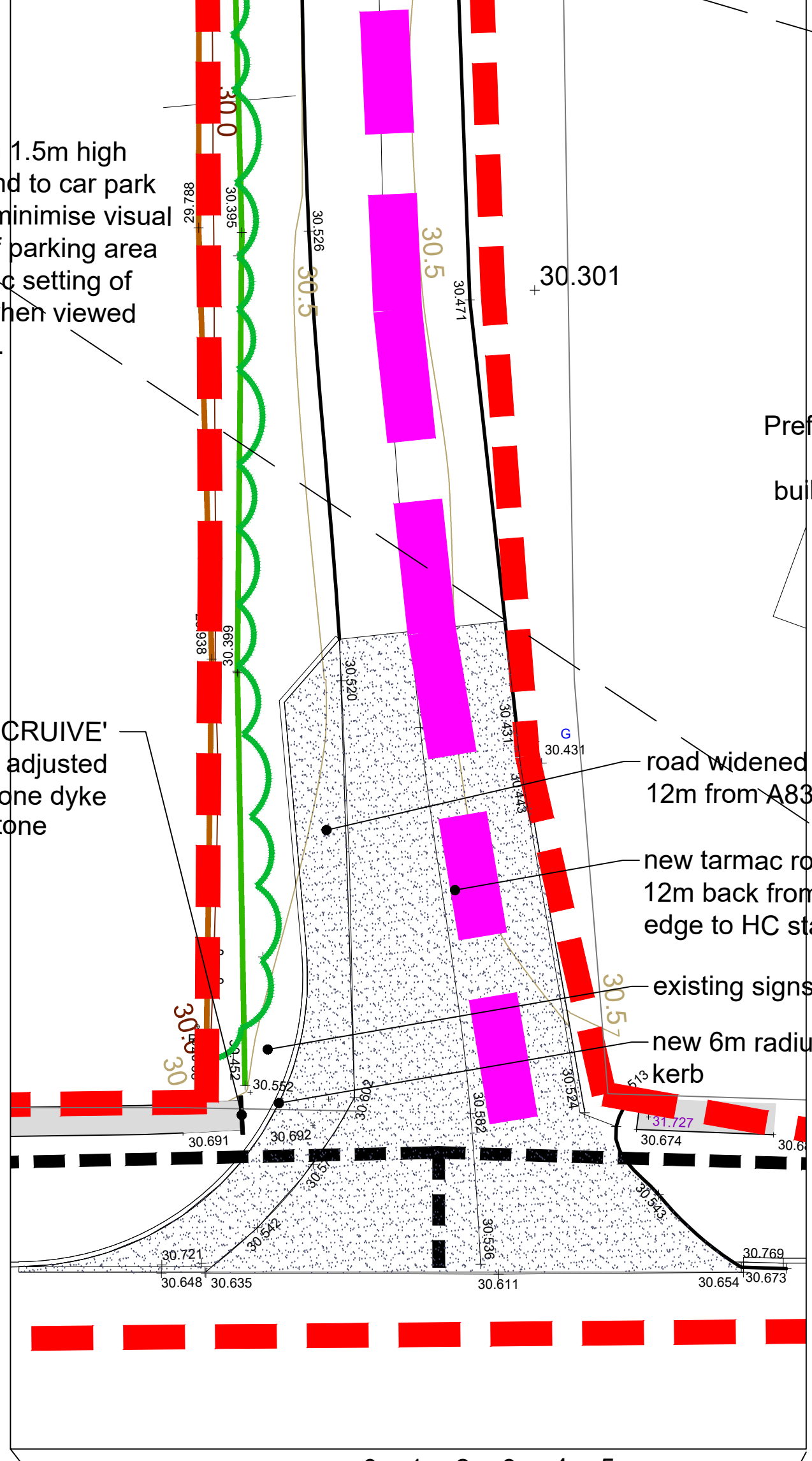
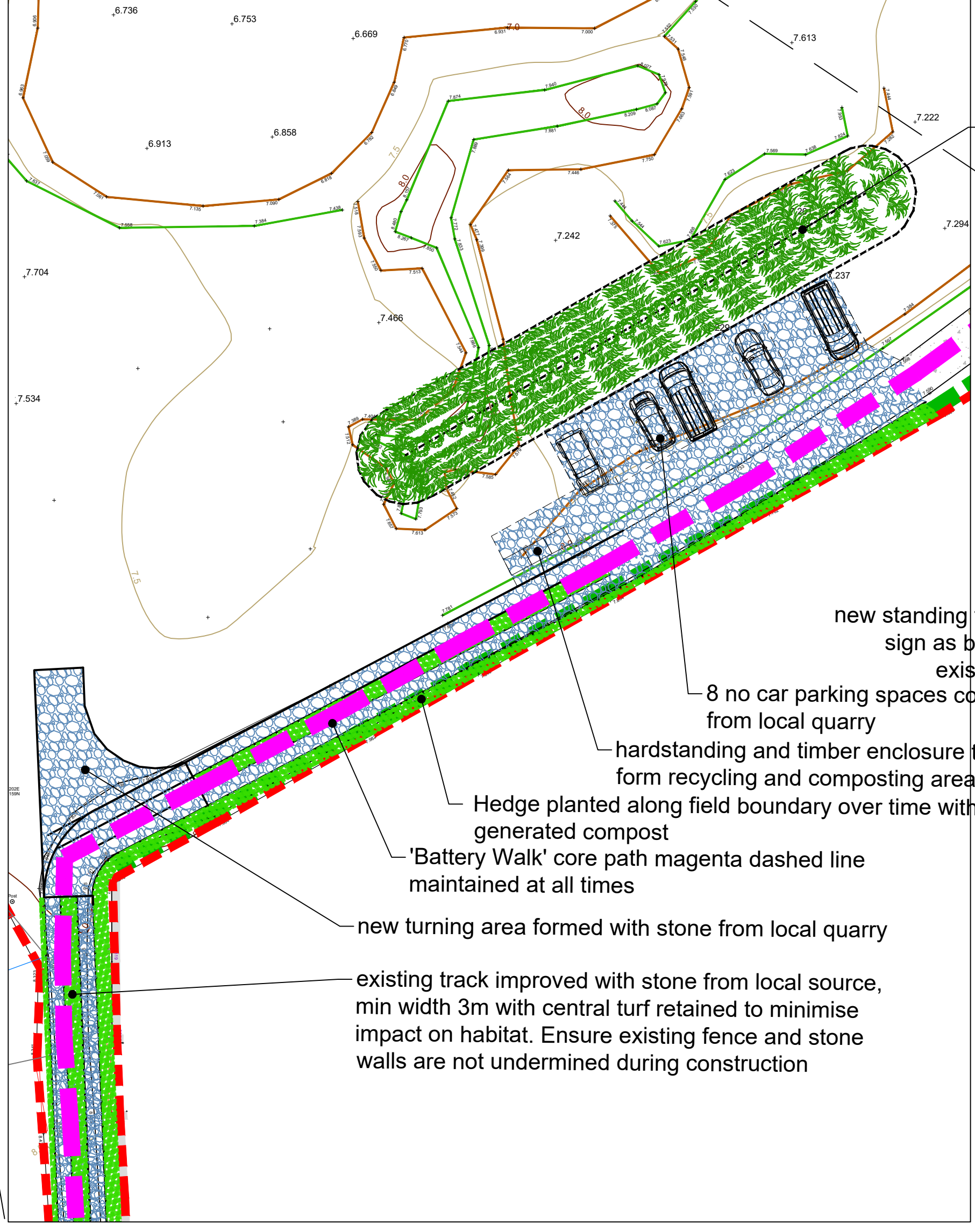
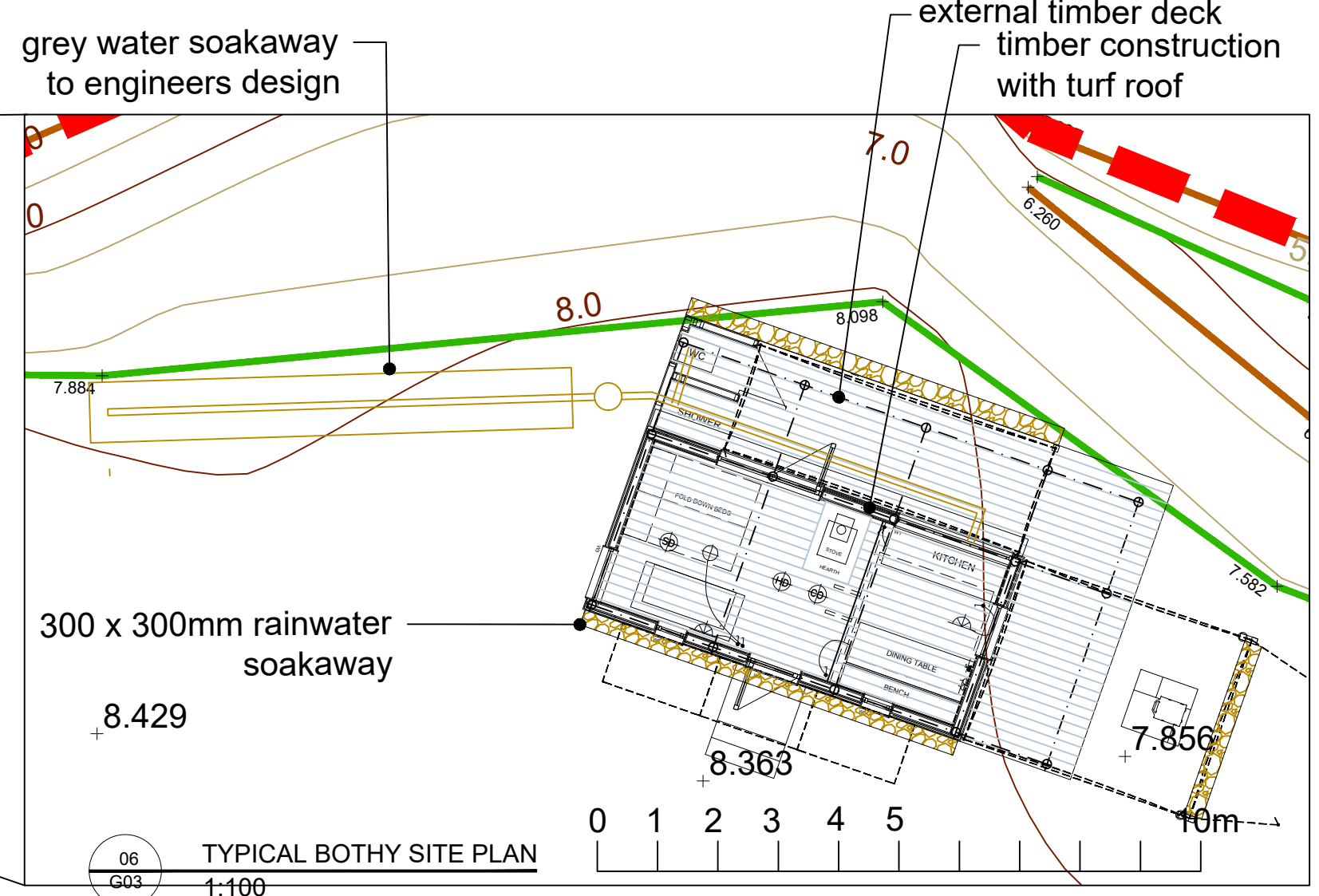
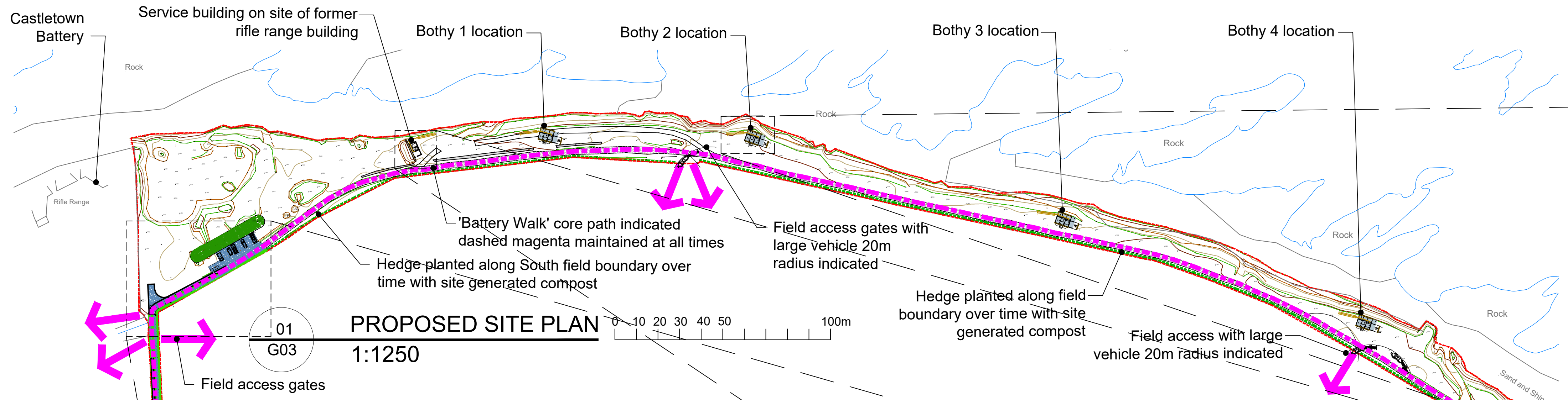
Plan 1	- Location Plan	G01 REV A
Plan 2	- Proposed Site Layout Plan	G03 REV D
Plan 3	- Proposed Site Layout Plan	G04 REV C
Plan 4	- Site Section Plan	G06 REV B
Plan 5	- Elevations	G07 REV D
Plan 6	- Floor / Elevation Plan	G09 REV B
Plan 7	- Visualisations	G10 REV B
Plan 8	- Drainage Construction Details	22.0954-520
Plan 9	- Sectional Plan – Sign Detail	C02 REV A
Plan 10	- Visualisations – Site Office	SK01 REV A





<b>G Paterson Architect Ltd</b>			Client <b>Cruive</b>		
A: 14 Sandylands Road, Cupar, Fife, KY155JS T: 01334 657666 M: 07771644517 E: gary@gpatersonarchitect.scot Director: Gary Paterson Company Number: SC482336			Project <b>Bothy retreat development Battery Road Castletown</b>		
Revision	Description	Date	Drawing Title <b>Location Plan</b>		
A	core path and neighboring access points added	31.01.23	STAGE: <b>PLANNING</b>		
Scale 1:5000 @ A2			Date Jan 22	Drawn By GP	
Job No. <b>363</b>			Dwg No. <b>G01</b>	Rev <b>A</b>	





min 7m long x circa 4.4m passing place at mid point of access. Ensure existing fence and stone wall are not undermined during construction

existing track improved with stone from local source, min width 3m with central turf retained to minimise impact on habitat. Ensure existing fence and stone walls are not undermined during construction

minimum 1.5m high earth bund to car park edge to minimise visual impact of parking area on historic setting of battery when viewed from sea.

grey water soakaway to engineers design  
Prefabricated timber clad with turf roof service building on site of former rifle range building

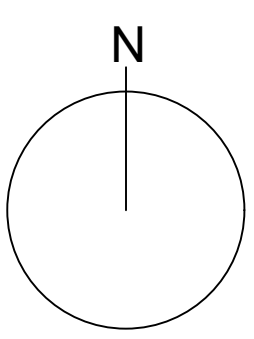
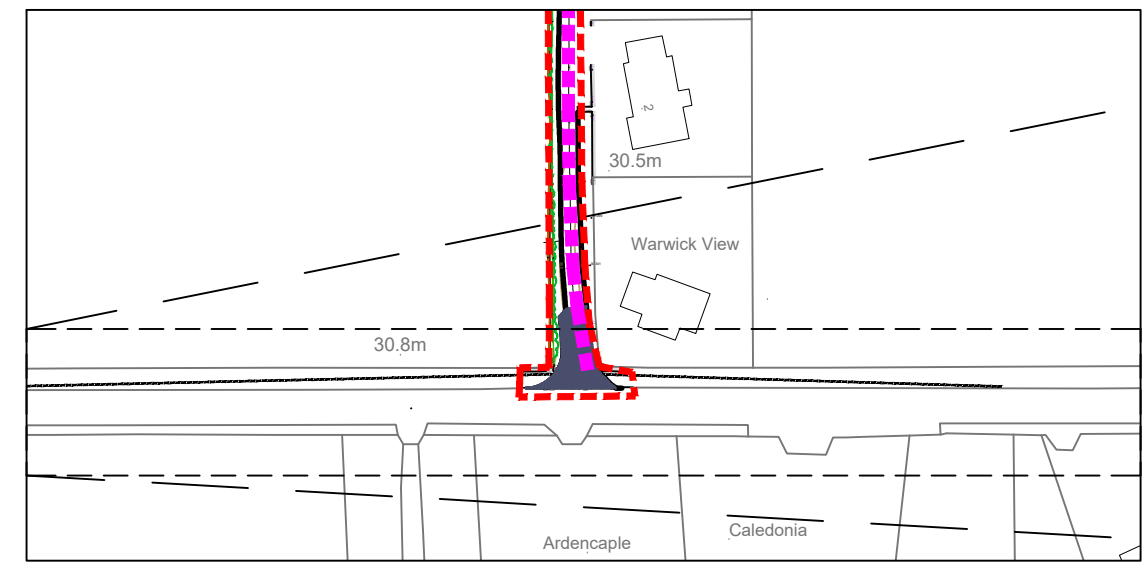
road widened to 5.5m for 12m from A836 road edge

new tarmac road surface 12m back from A836 road edge to HC standard.

existing signs reinstated  
new 6m radius concrete kerb

existing track improved with stone from local source, min width 3m with central turf retained to minimise impact on habitat. Ensure existing fence and stone walls are not undermined during construction

'Battery Walk' core path maintained at all times



Revision	Description	Date
D	access track amended	24.03.23
C	core path and field access points added	31.01.23
B	parking and turning area and service building amended ground pv omitted	03.11.22
A	passing place added, CRUIVE sign adjusted	15.08.22

Status: **PLANNING**

G Paterson Architect Ltd

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E: gary@gpatersonarchitect.scot  
Director: Gary Paterson  
Company Number: SC482336

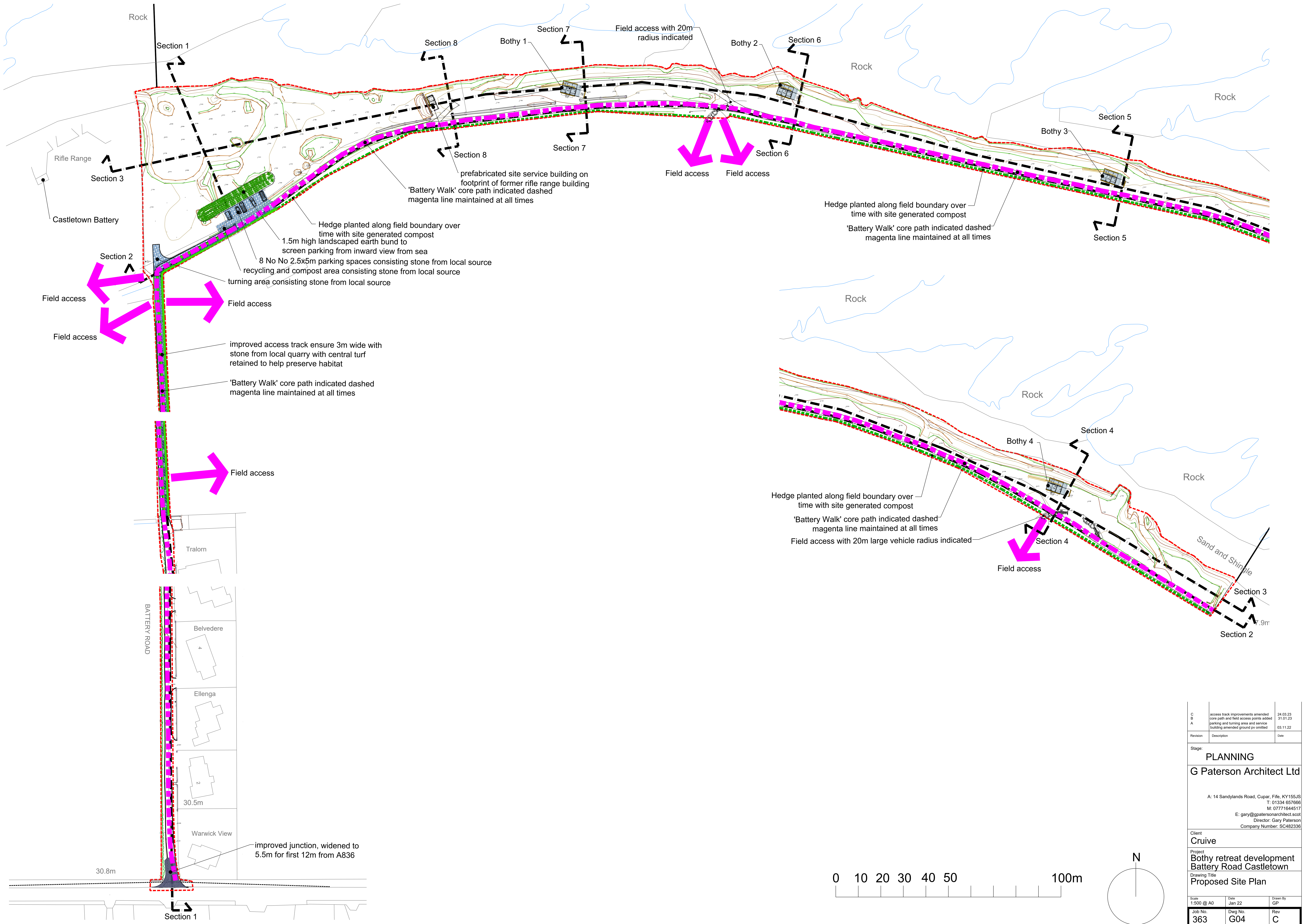
Client: **Cruive**  
Project: **Bothy retreat development Battery Road Castletown**

Drawing Title: **Proposed Site Plan**

Scale	Date	Drawn By
1:1250 @ A1	Feb 22	GP

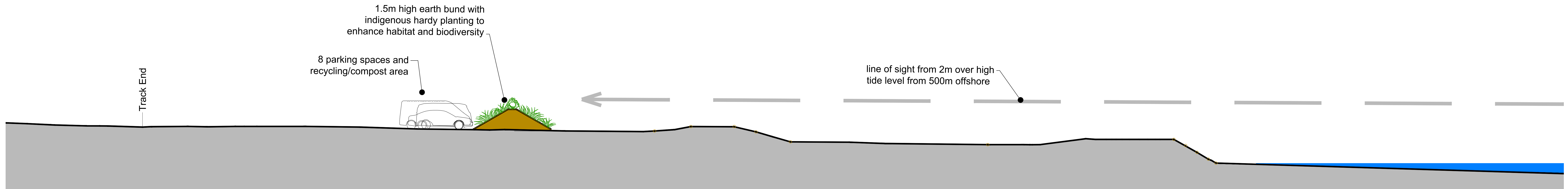
Job No.	Dwg No.	Rev
363	G03	D



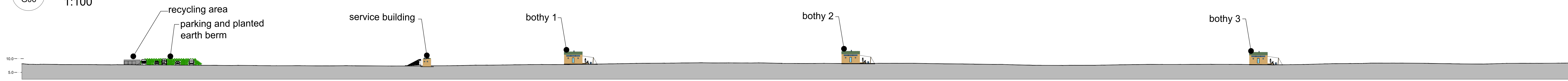


C	access track improvements amended	24.03.23
B	core path and field access points added	31.01.23
A	parking and turning area and service building amended ground provided	03.11.22
Revision	Description	Date
Stage: <b>PLANNING</b>		
<b>G Paterson Architect Ltd</b>		
A: 14 Sandylands Road, Cupar, Fife, KY155JS T: 01334 657666 M: 07771644517 E: gary@gpatersonarchitect.scot Director: Gary Paterson Company Number: SC482336		
Client: <b>Cruive</b>		
Project: <b>Bothy retreat development Battery Road Castletown</b>		
Drawing Title: <b>Proposed Site Plan</b>		
Scale: 1:500 @ A0	Date: Jan 22	Drawn By: GP
Job No: 363	Dwg No: G04	Rev: C

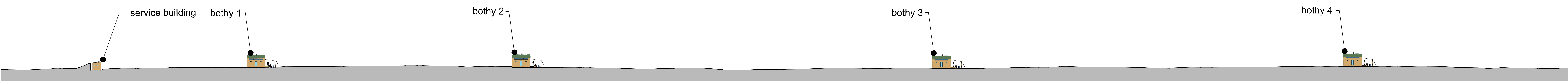




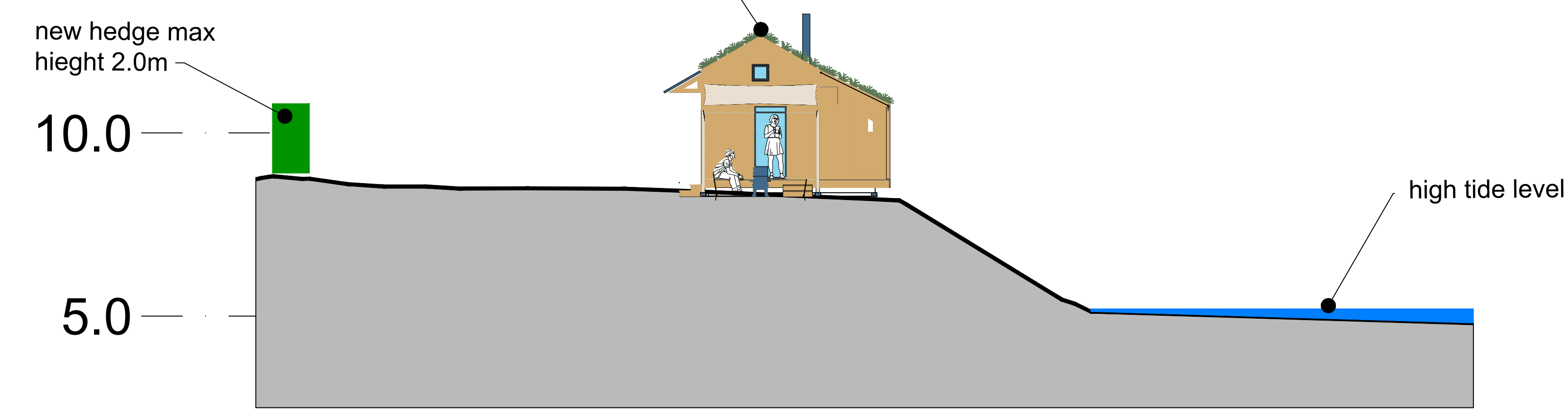
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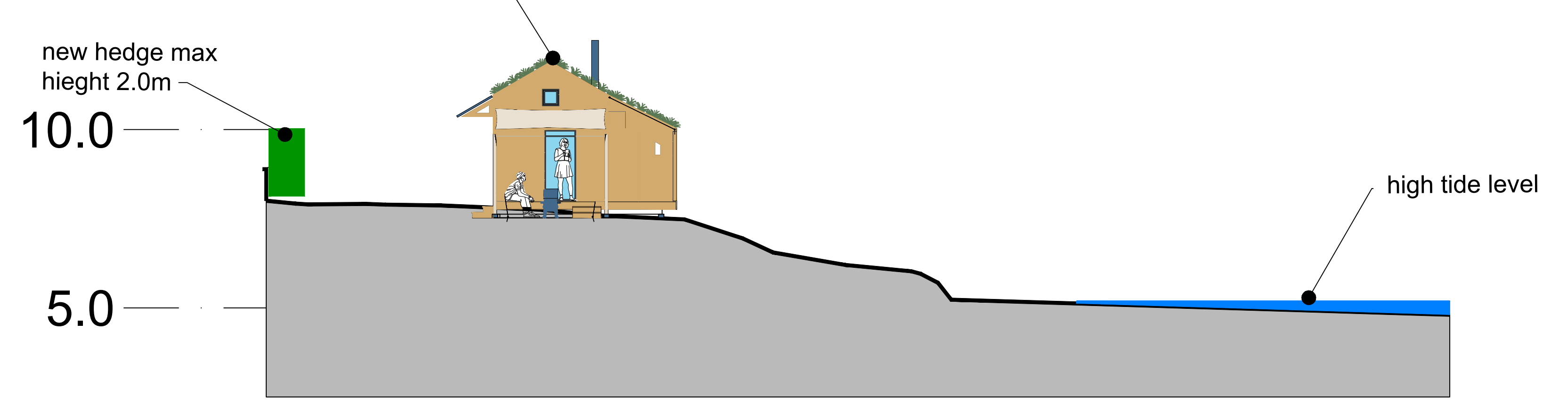
02 SECTION 2  
G06 1:500



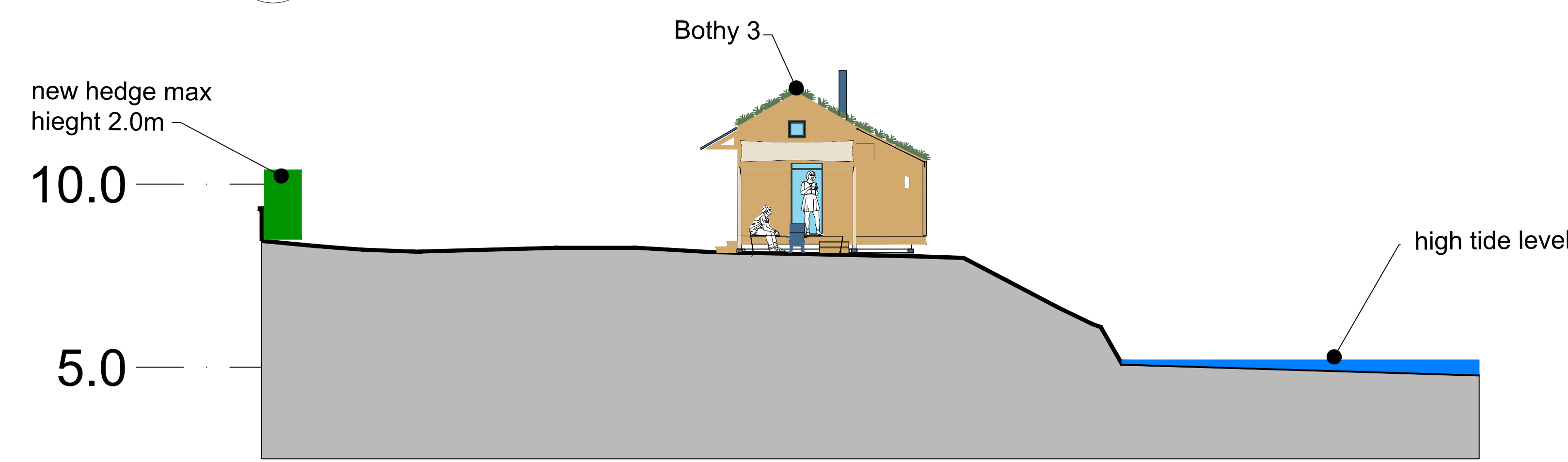
03 SECTION 3  
G06 1:500



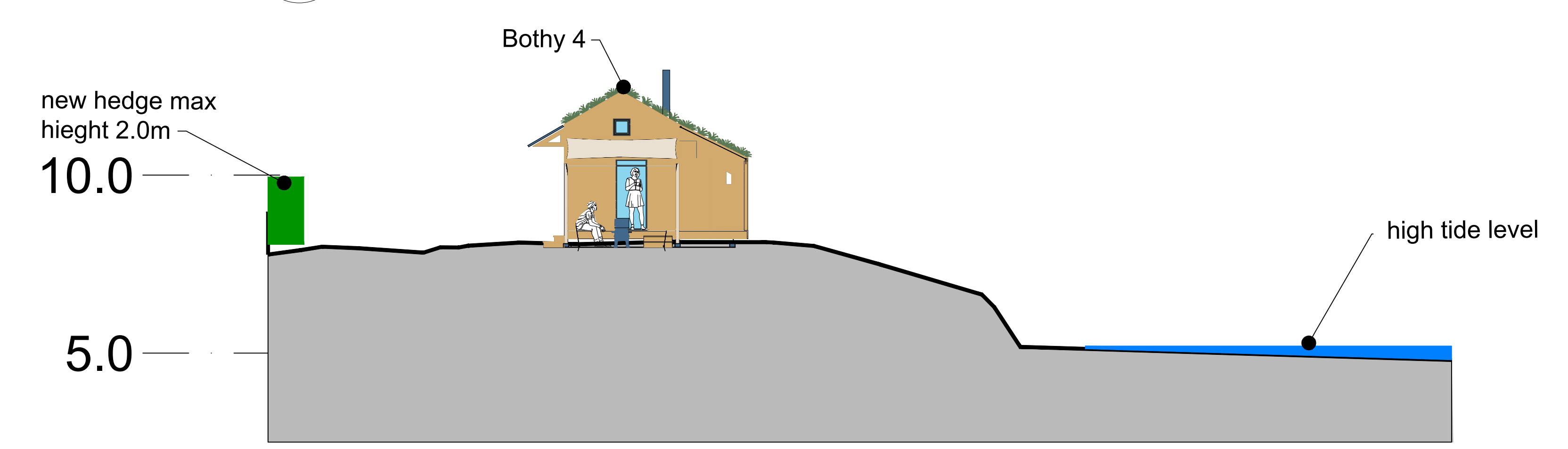
04 SECTION 4  
G06 1:100



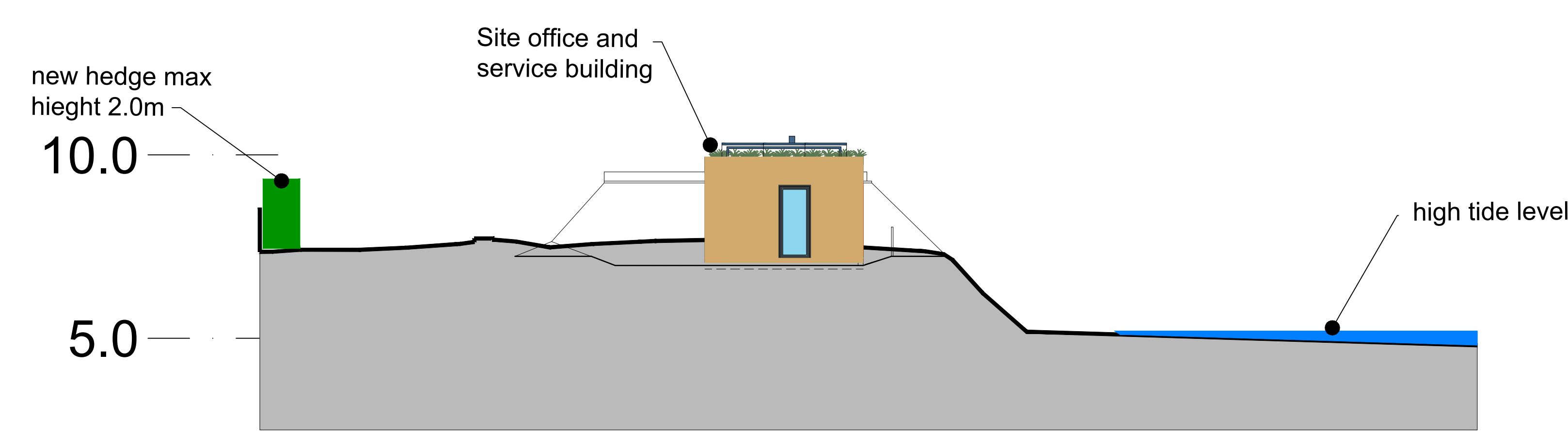
05 SECTION 5  
G06 1:100



06 SECTION 6  
G06 1:100



07 SECTION 7  
G06 1:100



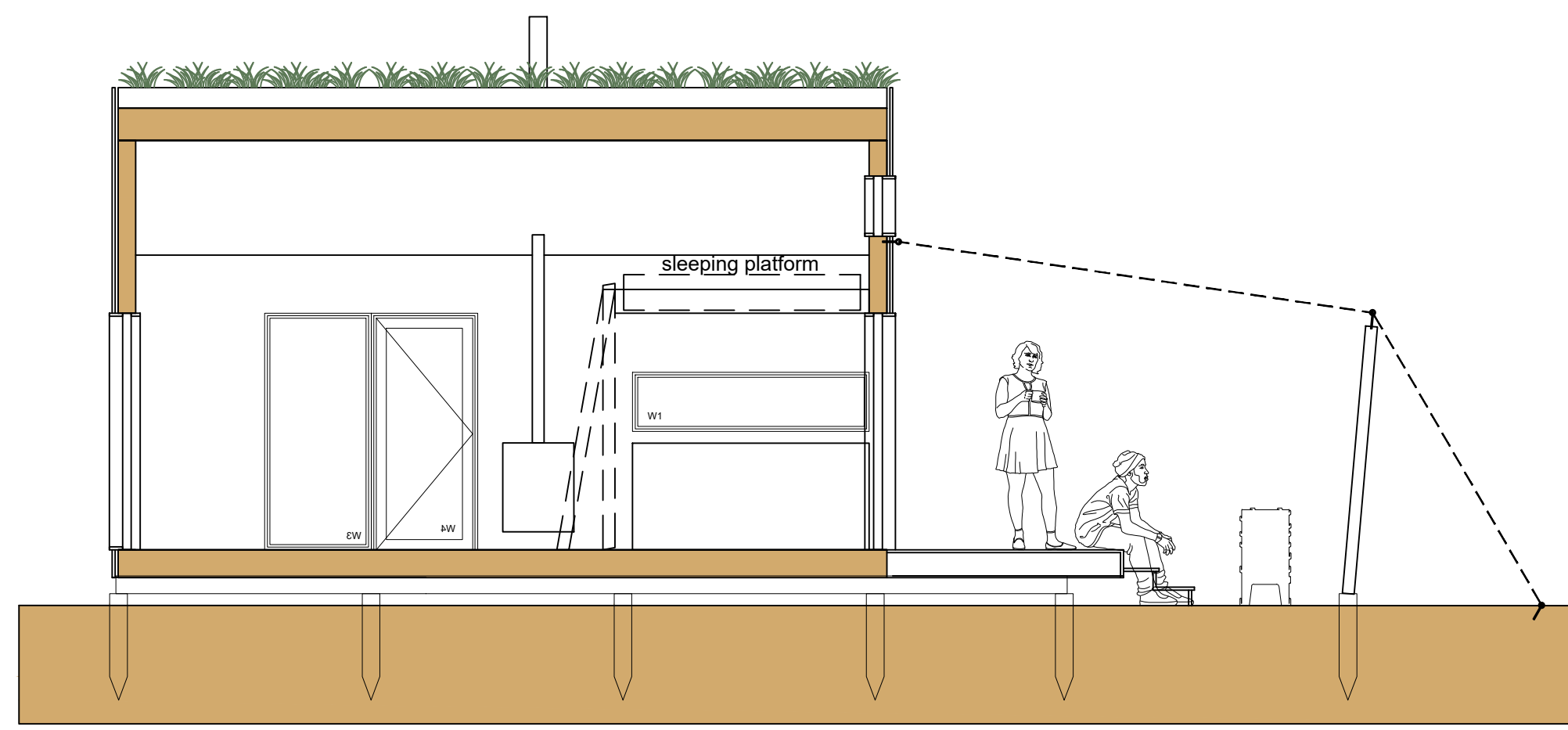
08 SECTION 8  
G06 1:100

B	Hedge indicated	24.03.23
A	Parking repositioned detail added	10.03.23
Revision	Description	Date
Stage:		
PLANNING		
G Paterson Architect Ltd		
A: 14 Sandylands Road, Cupar, Fife, KY15 5JS		
T: 01334 657666		
M: 07771644517		
E: gary@gpatersonarchitect.scot		
Director: Gary Paterson		
Company Number: SC482336		
Client:		
Cruive		
Project:		
Bothy retreat development		
Battery Road Castletown		
Drawing Title:		
Proposed Site Sections		
Scale:	Date:	Drawn By:
1:500/ 100 @ A0	July 22	GP
Job No.	Dwg No.	Rev
363	G06	B

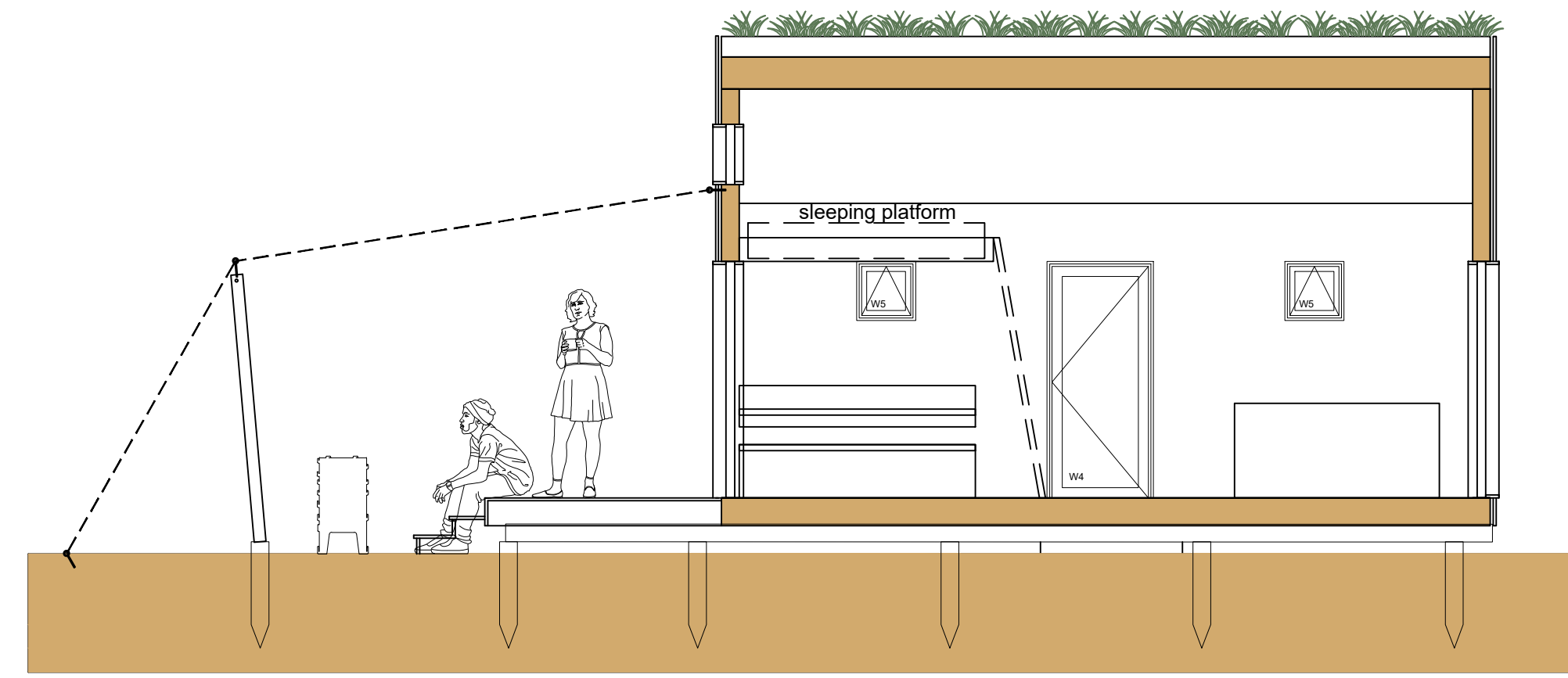


**SPECIFICATION:**

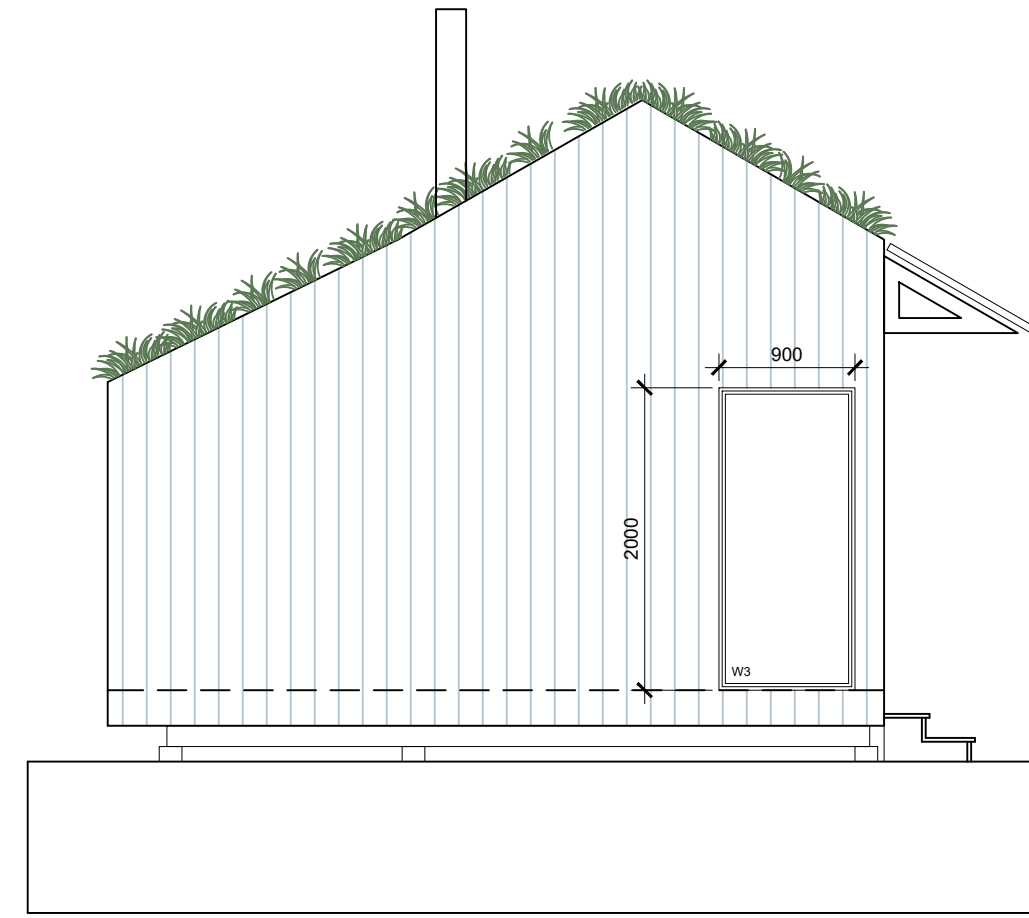
- 10. SUB STRUCTURE  
17 No screw piles to support timber sub frame for acceptance of bothys and deck.
- 20. ROOF  
150mm turf substrate with local flora and fauna on EPDM membrane on VCL on insulated OSB cassettes.
- 21. EXTERNAL WALLS  
Vertical 150 x 14mm timber board on treated battens and counterbattens on breather membrane on OSB insulated cassettes.
- 22. FLOOR  
Timber floor boards on insulated OSB cassettes with waterproof liquid applied, on timber sub frame and concrete foundation to St Eng detail.
- 31. WINDOWS AND DOORS  
Timber alu clad windows and doors by Nordvest or equal.
- 32. SLEEPING PLATFORM  
200x50mm timber frame with mattress recessed into frame with decorative plywood to underside.
- 50. SPACE HEATING  
Wood fired stove on Caithness flagstone hearth with fire retardant board / flagstone on adjacent wall.
- 51. WATER HEATING  
Via kettles on Wood fired stoves
- 91. EXTERNAL COOKING AREA  
1m x 1m x 50mm Caithness flagstone hearth with metal external oven anchored down as necessary.
- 52. SURFACE DRAINAGE  
EPDM with secret rainwater gutter and outlet.
- 53. SANITARYWARE  
Timber enclosures integrated into external deck with Separett Villa 9215 Urine Diverting Composting Toilet with low wattage vent to outside and liquids to grey water soakaway. Camping shower on hoist over deck with floor drain to grey water soakaway.
- 54. ELECTRICS  
Building mounted solar PV array with battery and inverter for internal lighting and charging personal devices.
- 90. EXTERNAL DECK AREA  
To consist 100-150 x 25mm timber boards on timber frame to suit on sub frame. Deck to be level with internal floor. Bespoke canopy secured with wires and rope to deck and ground mounted posts with screw pile footings and held in place with tension wires anchored to ground anchors as necessary.
- 91. COVERED BBQ AREA  
Caithness flagstone hearths for multifunction corten steel stove, BBQ, pizza oven and smoker. Bespoke canopy extended over hearth area and supported with timber posts and wires/ropes pegged into ground.



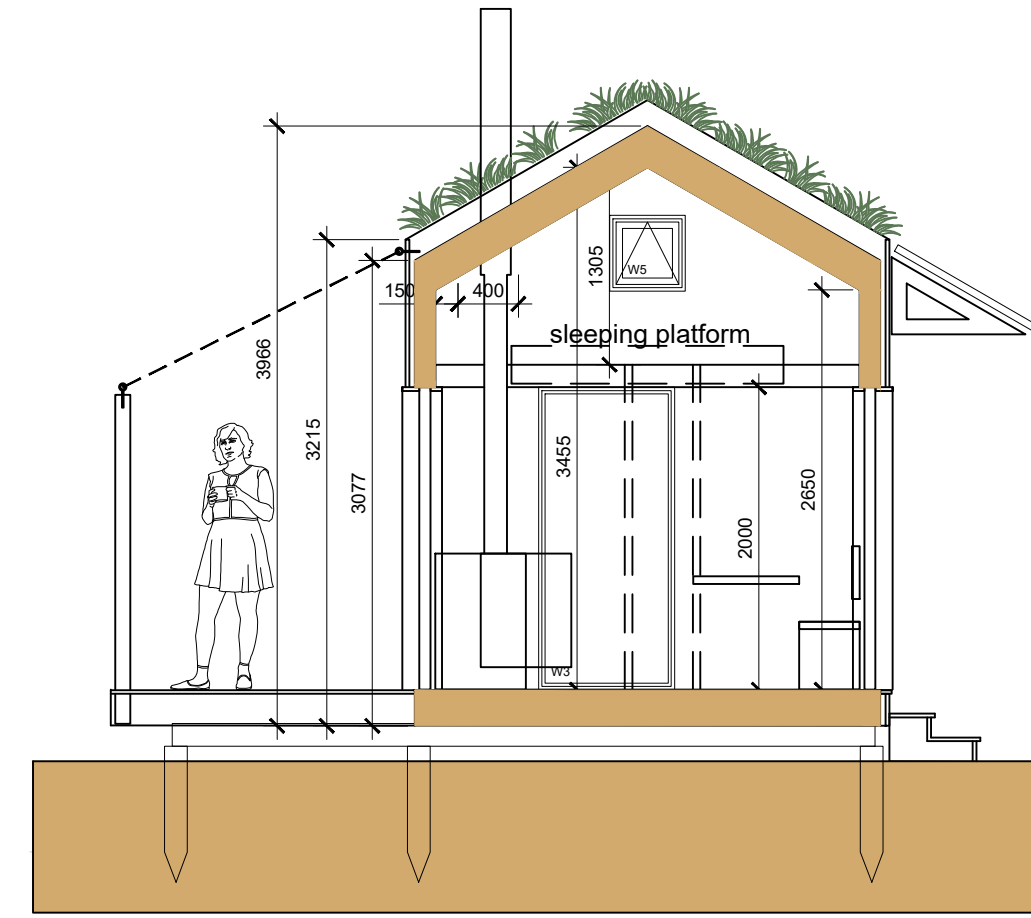
01 TYPICAL LONG SECTION LOOKING NORTH  
G07 1:50



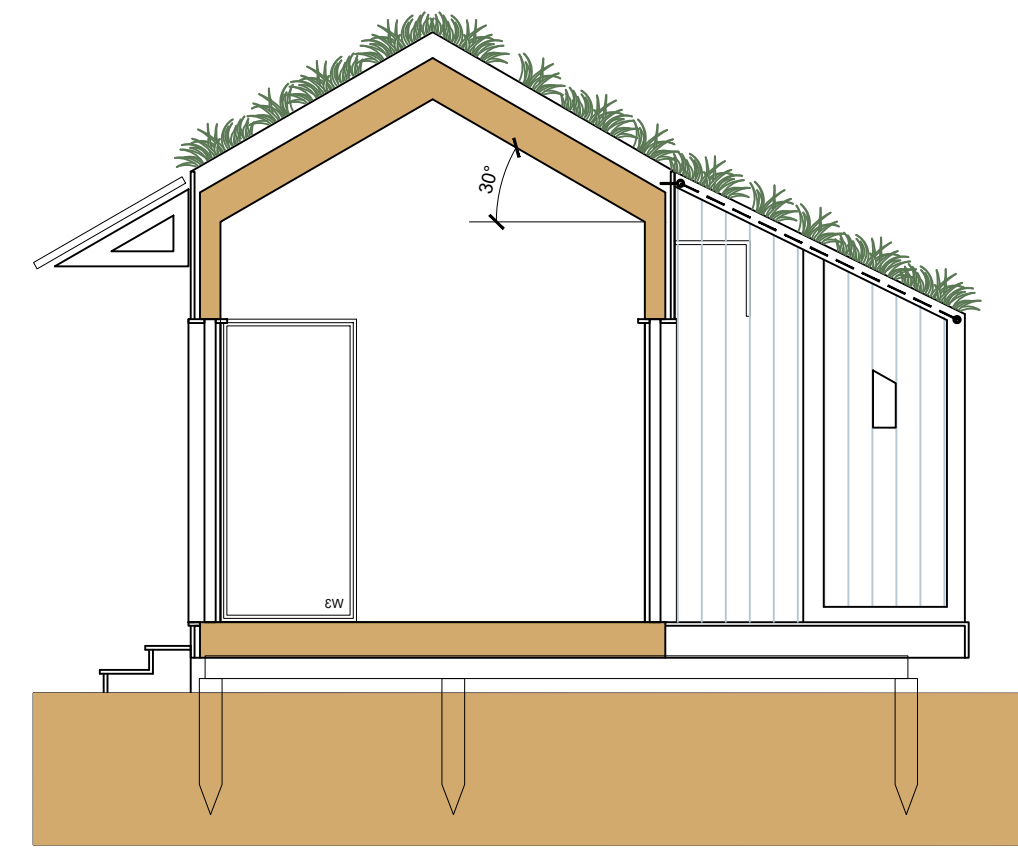
02 TYPICAL LONG SECTION LOOKING SOUTH  
G07 1:50



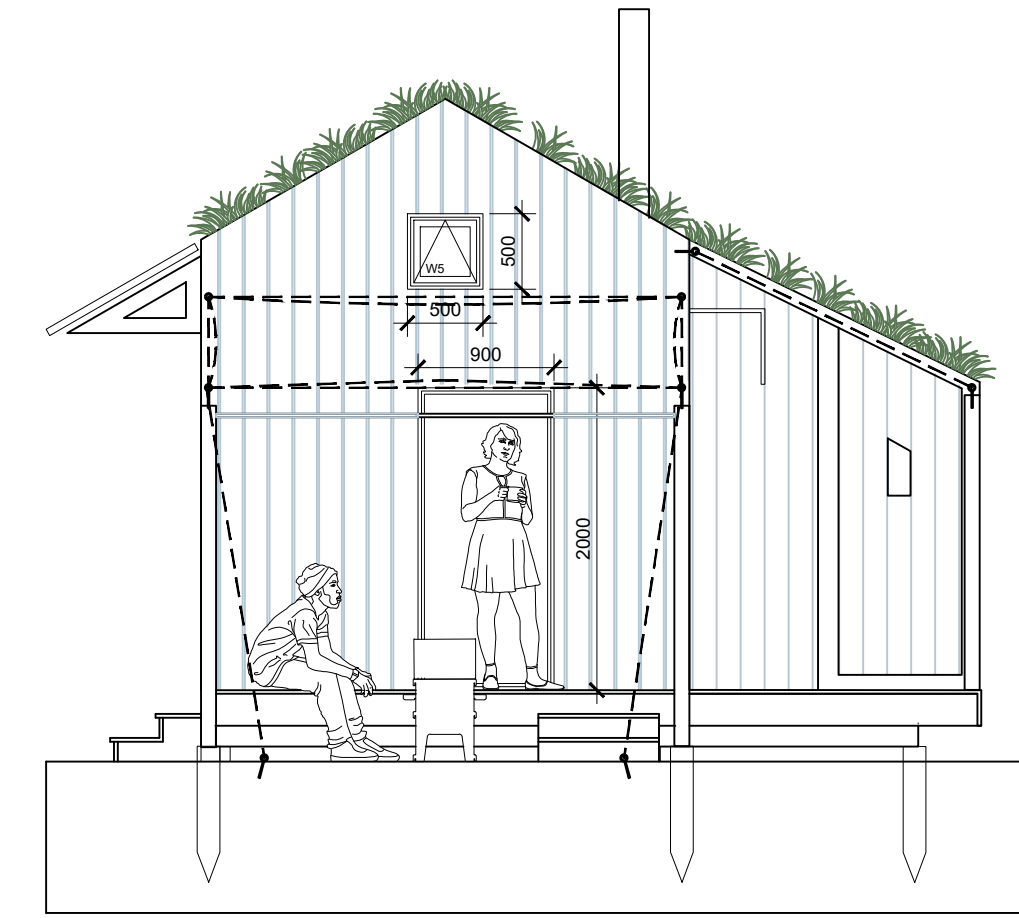
03 TYPICAL WEST ELEVATION  
G07 1:50



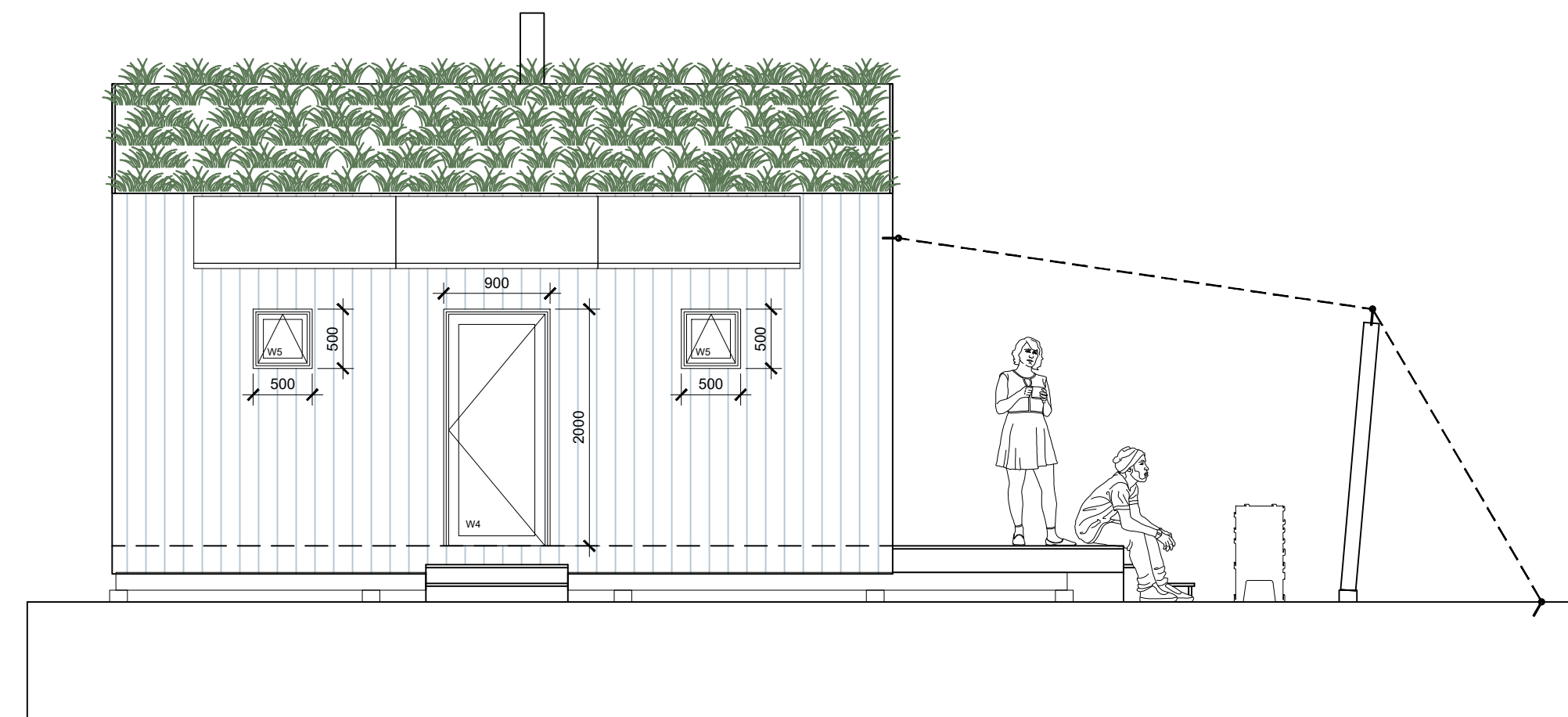
04 TYPICAL CROSS SECTION LOOKING EAST  
G07 1:50



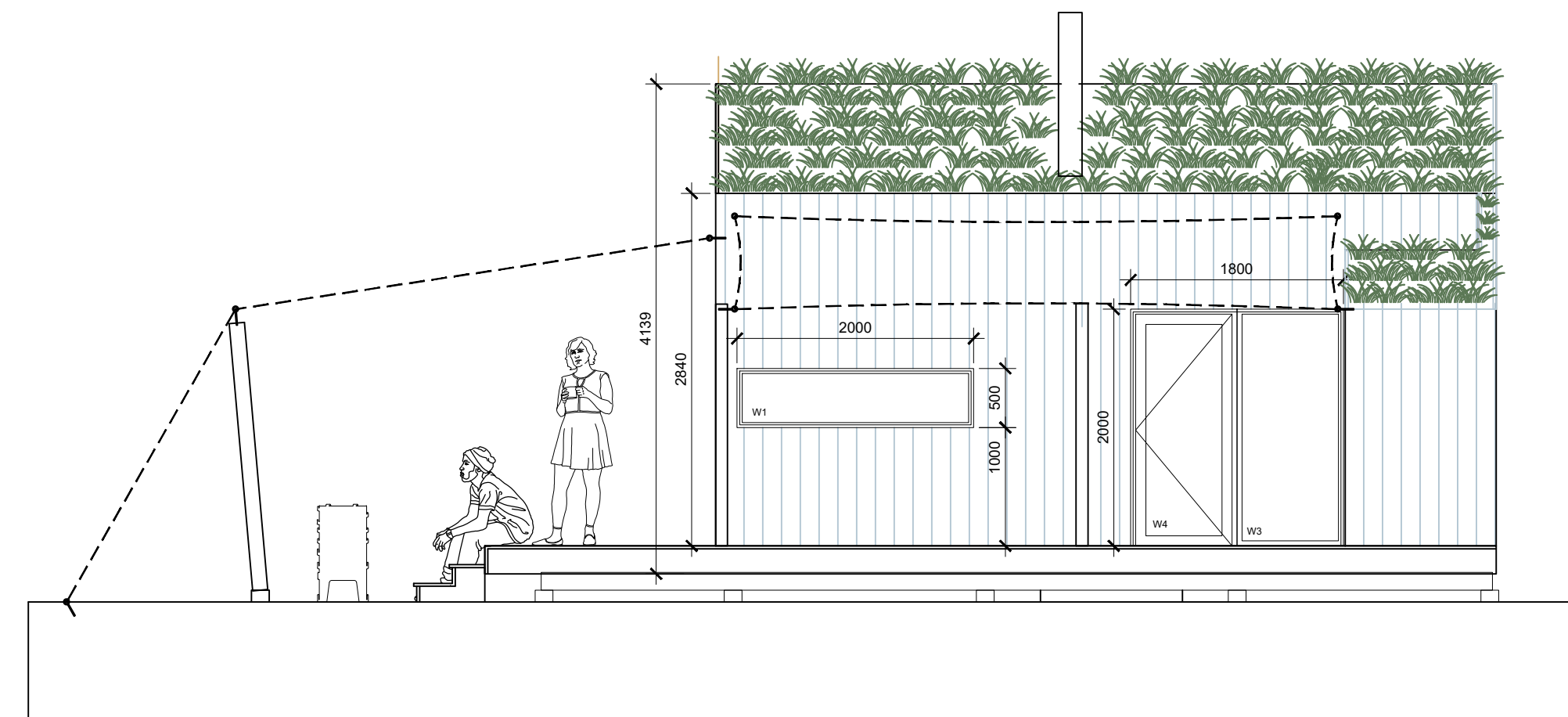
05 TYPICAL CROSS SECTION LOOKING WEST  
G07 1:50



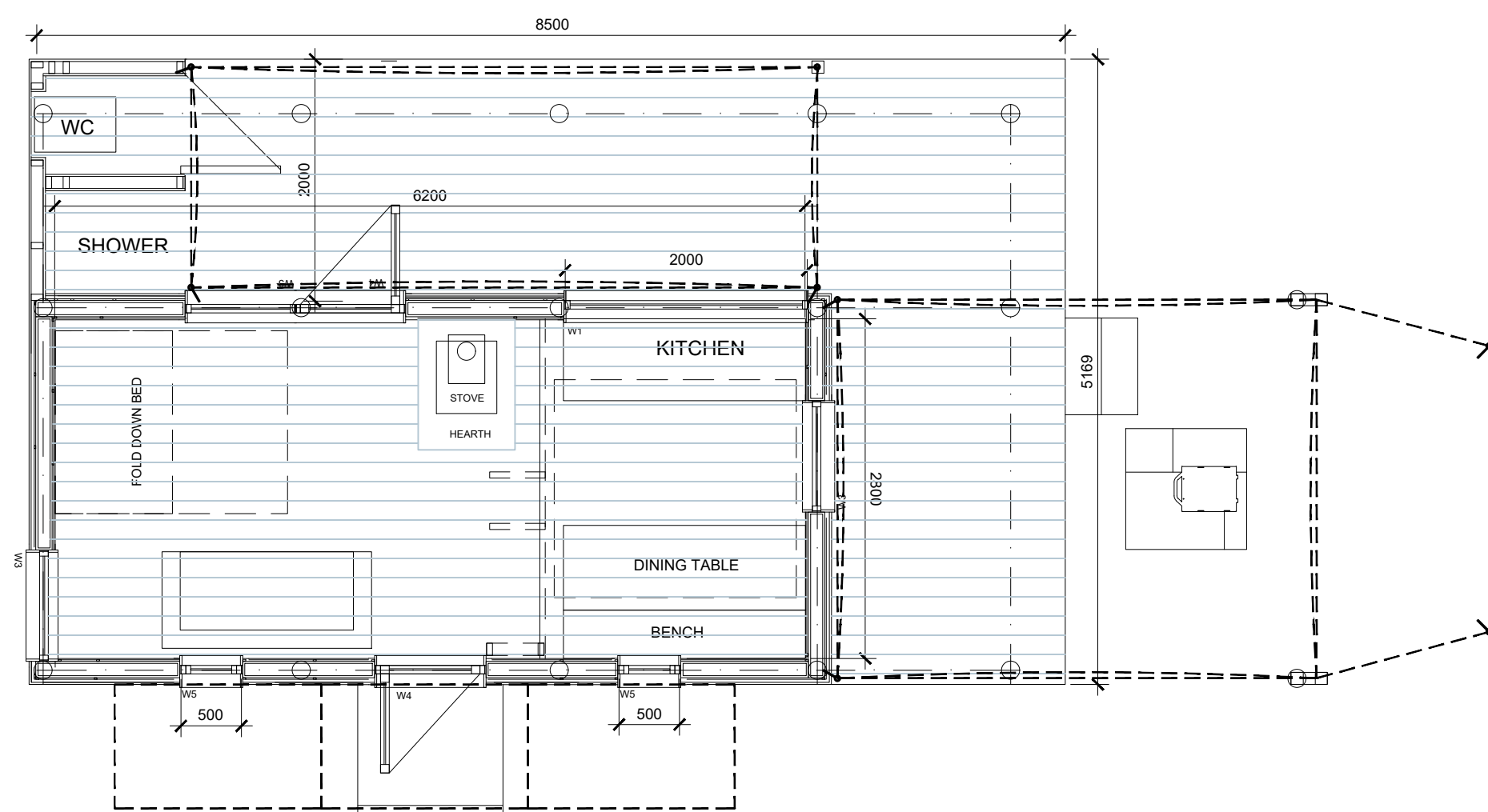
06 TYPICAL EAST ELEVATION  
G07 1:50



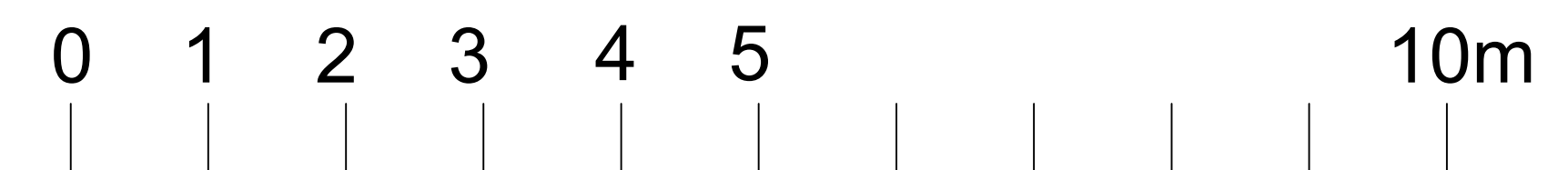
07 TYPICAL SOUTH ELEVATION  
G07 1:50



08 TYPICAL NORTH ELEVATION  
G07 1:50



05 TYPICAL NORTH ELEVATION  
G07 1:50



D	revised planning issue	10.03.23
C	micro wind removed WC spec	30.01.23
B	added micro wind and solar pv added screw pile footings	03.11.22
Revision	Description	Date
STATUS:		
<b>PLANNING</b>		
G Paterson Architect Ltd		
A: 14 Sandylands Road, Cupar, Fife, KY155JS T: 01334 657666 M: 07771644517 E: gary@patersonarchitect.scot Director: Gary Paterson Company Number: SC482336		
Client		
<b>CRUIVE</b>		
Project		
<b>Bothy retreat development Battery Road Castletown</b>		
Drawing Title		
<b>Proposed plan section and elevations</b>		
Scale	Date	Drawn By
1:50 @ A1	Feb 2022	GP
Job No.	Dwg No.	Rev
363	G07	D



**OFFICE SERVICE BUILDING SPECIFICATION:**

**PROPOSAL:**  
Former Rifle range building has collapsed on 2 sides and does not have a roof and is arguably not safe in current condition. The proposal is to record and display artifacts as appropriate make safe and place a prefabricated service building on the existing footprint to allow the site to minimize the impact on the site during the life of the building whilst allowing the remains of the rifle range building to remain intact. Building form, timber cladding and turf roof all help minimize visual impact on site.

**SUB STRUCTURE:** existing concrete base

**ROOF:** 150mm turf substrate with local flora and fauna on EPDM membrane on VCL on insulated timber structure

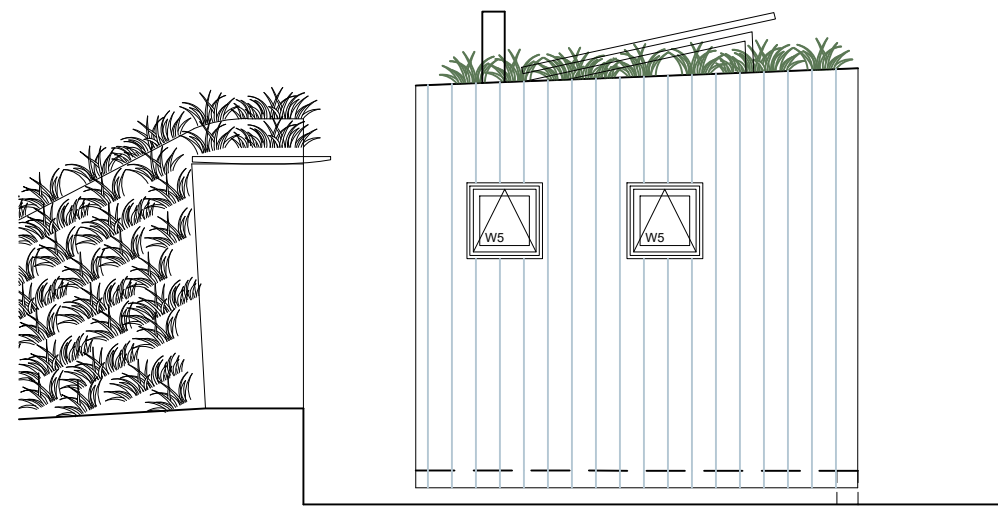
**EXTERNAL WALLS:** vertical 14mm treated sarking board on treated battens and counterbattens on breather membrane on insulated timber structure

**WINDOWS AND DOORS:** high quality timber alu clad windows and doors by Nordvest.

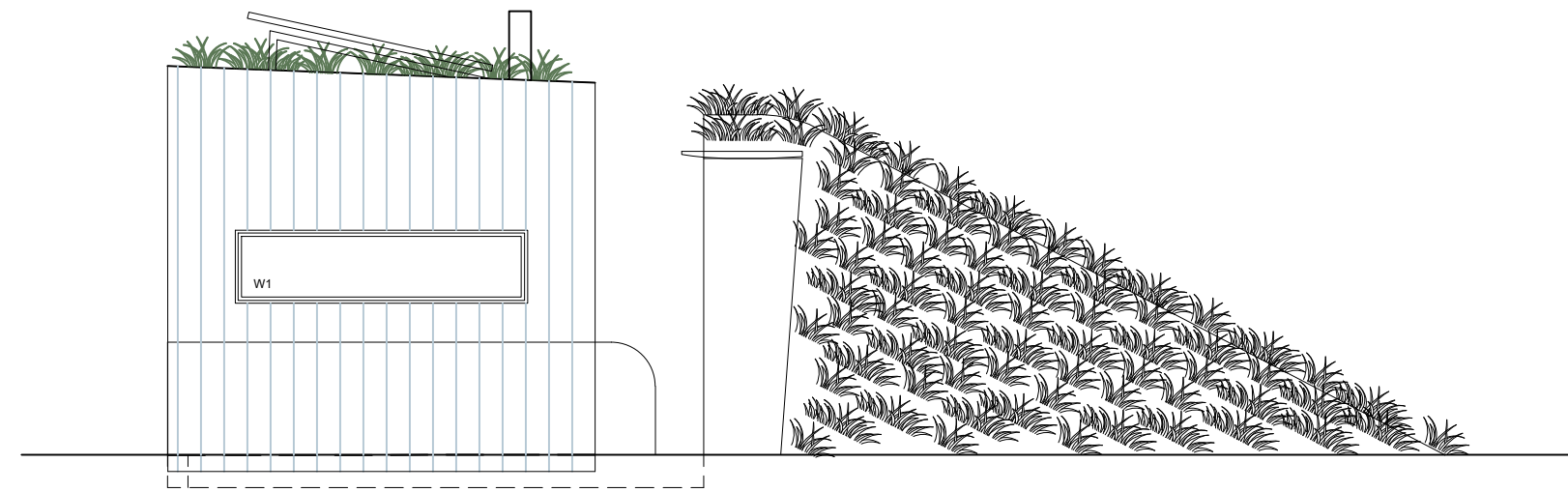
**OFFICE HEATING:** Multi fuel stove to double wall steel flue 600mm over roof.

**DRAINAGE:** Seperett Villa 9215 Urine Diverting composting WC and WHB to grey water soakaway to engineers detail.

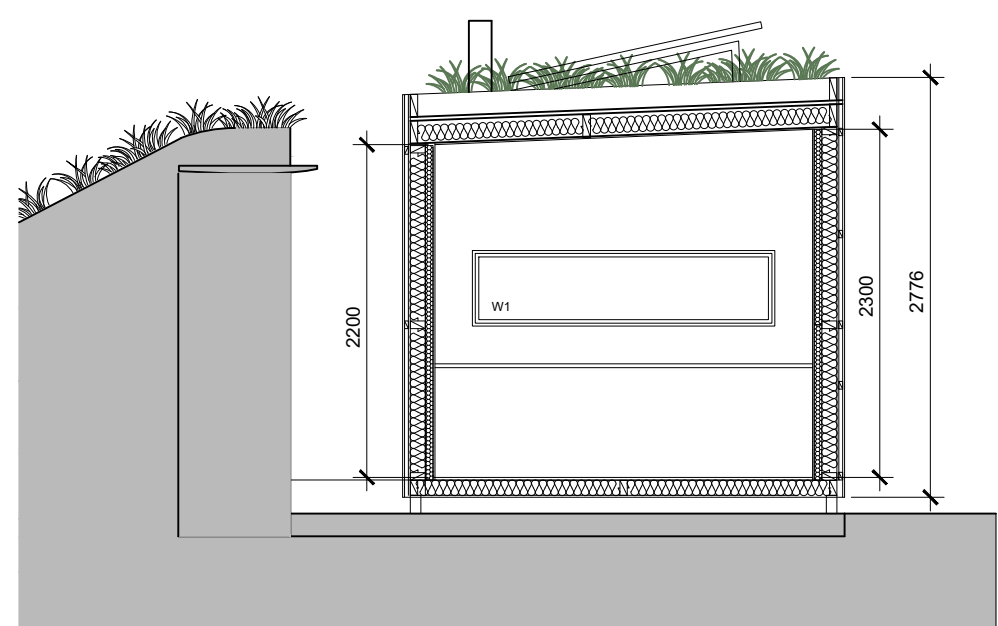
**ELECTRICS:**  
Roof mounted solar array to provide autonomous off grid solution



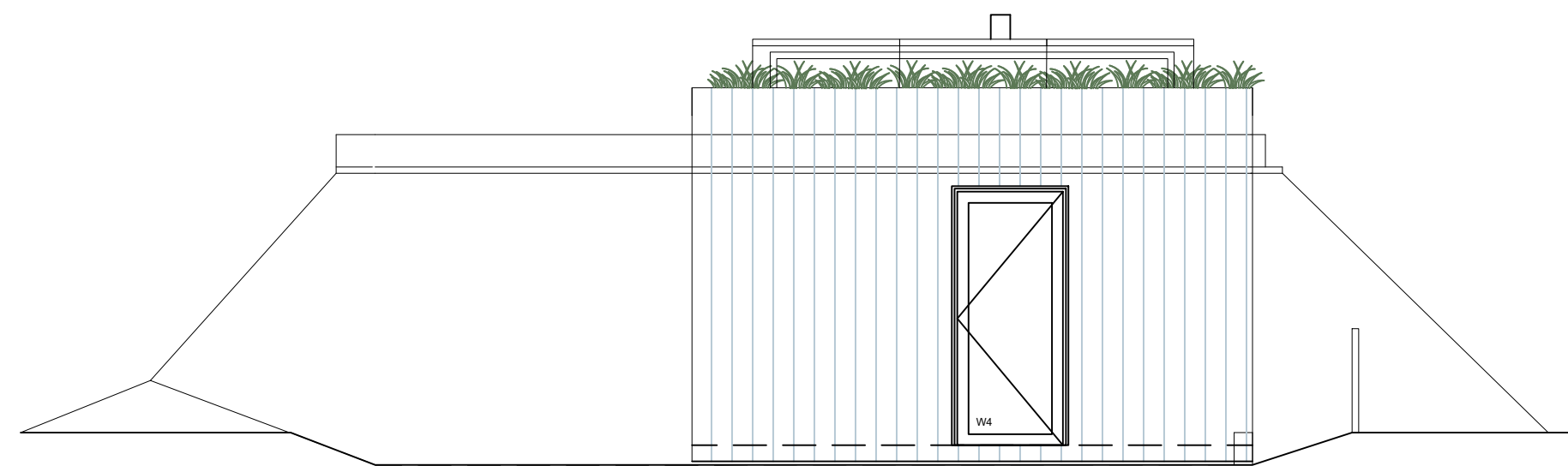
01 SOUTH ELEVATION  
G09 1:50



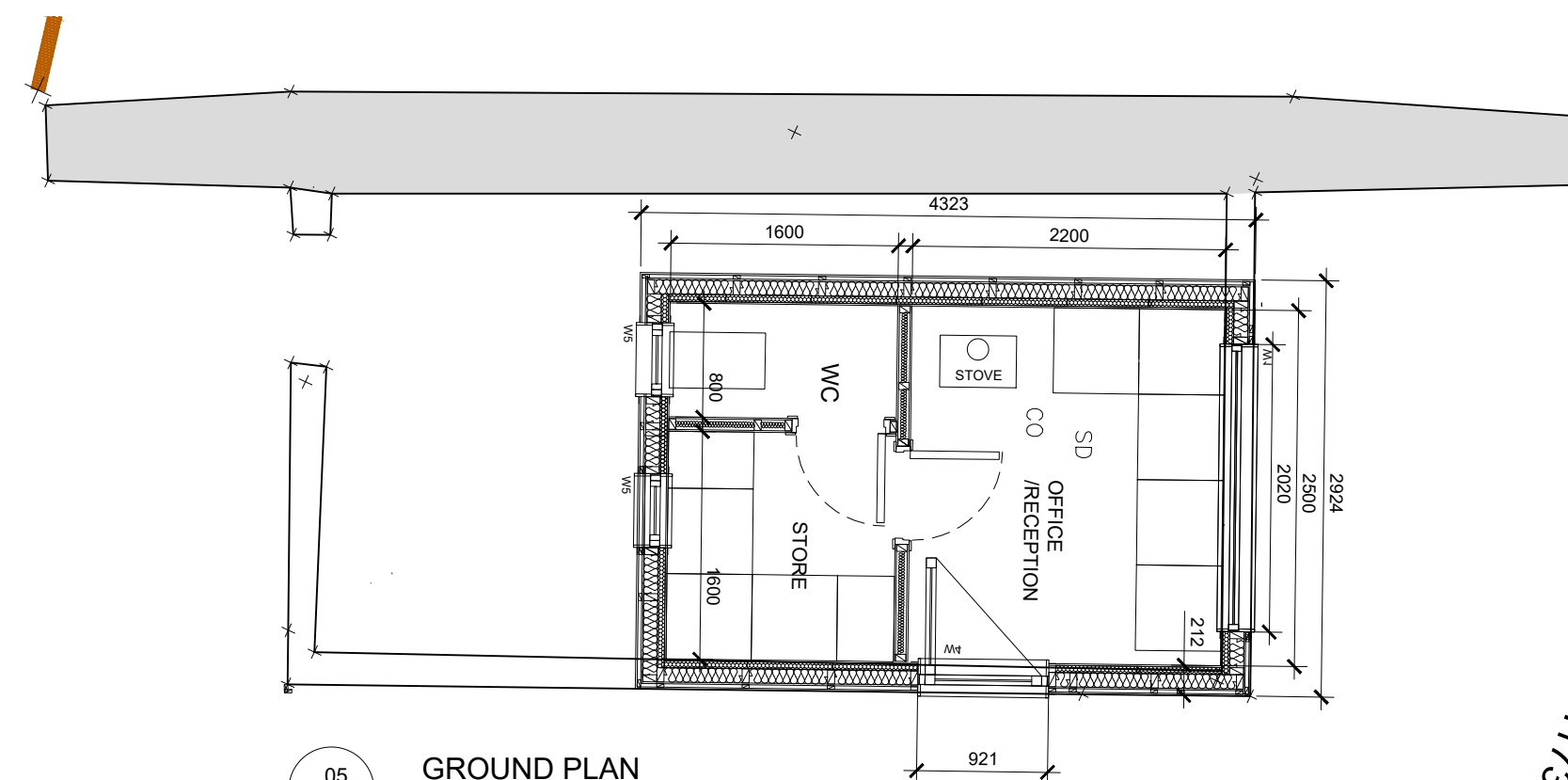
02 SOUTH ELEVATION  
G09 1:50



03 CROSS SECTION  
G09 1:50

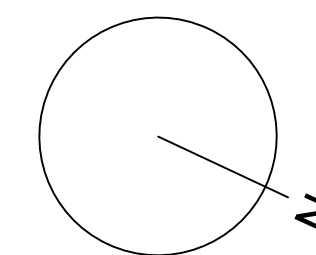


04 EAST ELEVATION  
G09 1:50



05 GROUND PLAN  
G09

1:173



Revision	Description	Date
B	Micro Wind Turbine removed. Spec of compost WC added	30.01.23
A	Building prefabricated, roof mounted PV and micro wind added	03.11.22

STATUS:  
**PLANNING**

**G Paterson Architect Ltd**

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Director: Gary Paterson  
Company Number: SC482336

Client  
**CRUIVE**

Project  
**Bothy retreat development  
Battery Road Castletown**

Drawing Title  
**Site office/ service building  
plan section and elevations**

Scale	Date	Drawn By
1:50 @ A1	July 2022	GP

Job No.	Dwg No.	Rev
363	G09	B





01  
G10 VIEW FROM DWARWICK PIER

bothy 4

bothy 3

bothy 2

bothy 1

office service building'



02  
G10 AERIEL VIEW FROM CASTLEHILL HARBOUR

bothys

bothy 4

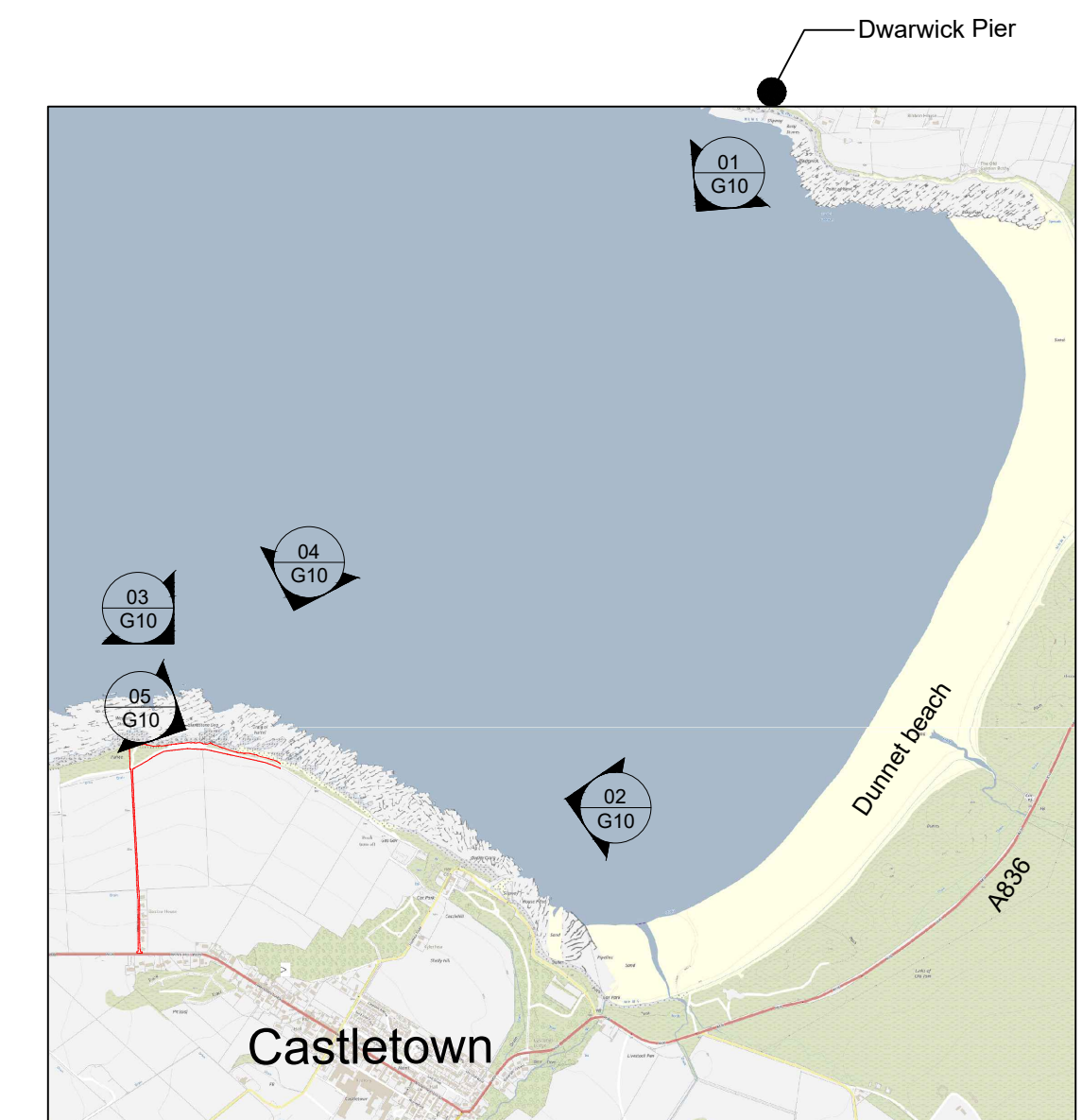
bothy 3

bothy 2

bothy 1



03  
G10 AERIEL VIEW FROM WEST



06  
G10 IMAGE KEY DIAGRAM  
1:25,000



04  
G10 AERIEL VIEW FROM NORTH

bothy 4

bothy 3

bothy 2

bothy 1

bothy 4

bothy 3

bothy 2

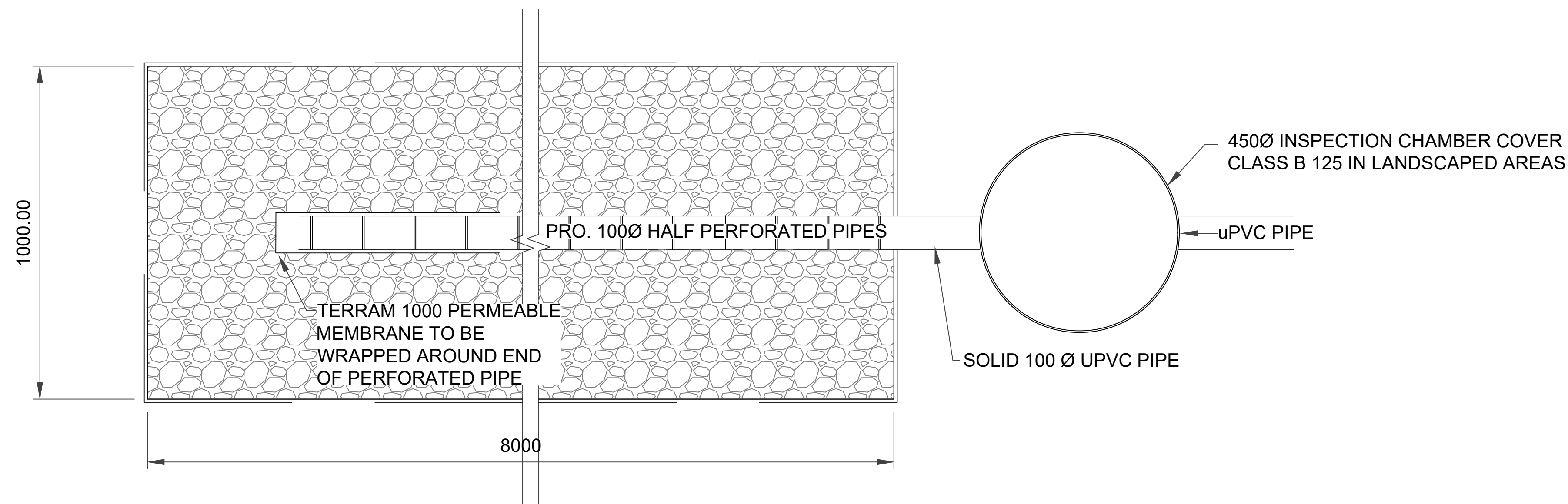
bothy 1



05  
G10 VIEW FROM CORE PATH LOOKING EAST

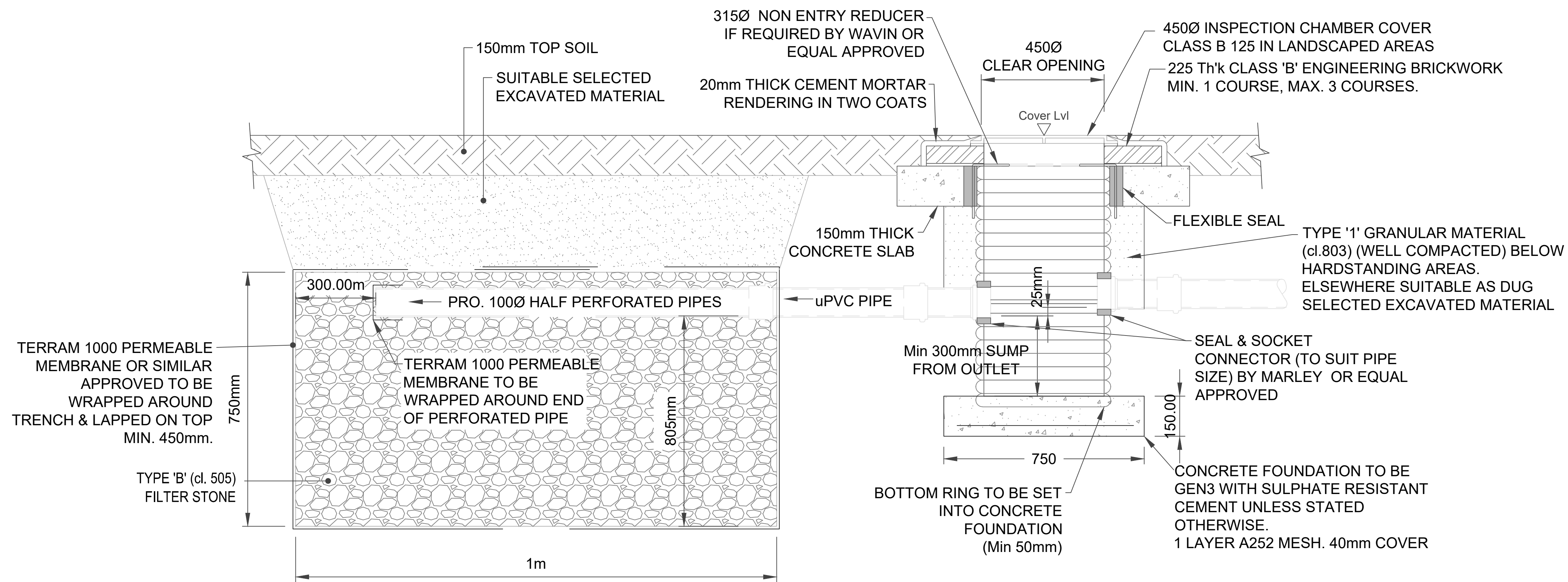
B	wind turbines removed	09.03.23
A	service building updated and micro wind turbines added	25.11.22
Revision Description Date		
STATUS:		
PLANNING		
G Paterson Architect Ltd		
A: 14 Sandylands Road, Cupar, Fife, KY155JS T: 01334 657666 M: 07771644517 E: gary@gpateronarchitect.scot Director: Gary Paterson Company Number: SC482336		
Client		
CRUIVE		
Project		
Bothy retreat developemnt Battery Road Castletown		
Drawing Title		
Visualisations		
Scale	Date	Drawn By
NTS	July 2022	GP
Job No.	Dwg No.	Rev
363	G10	B





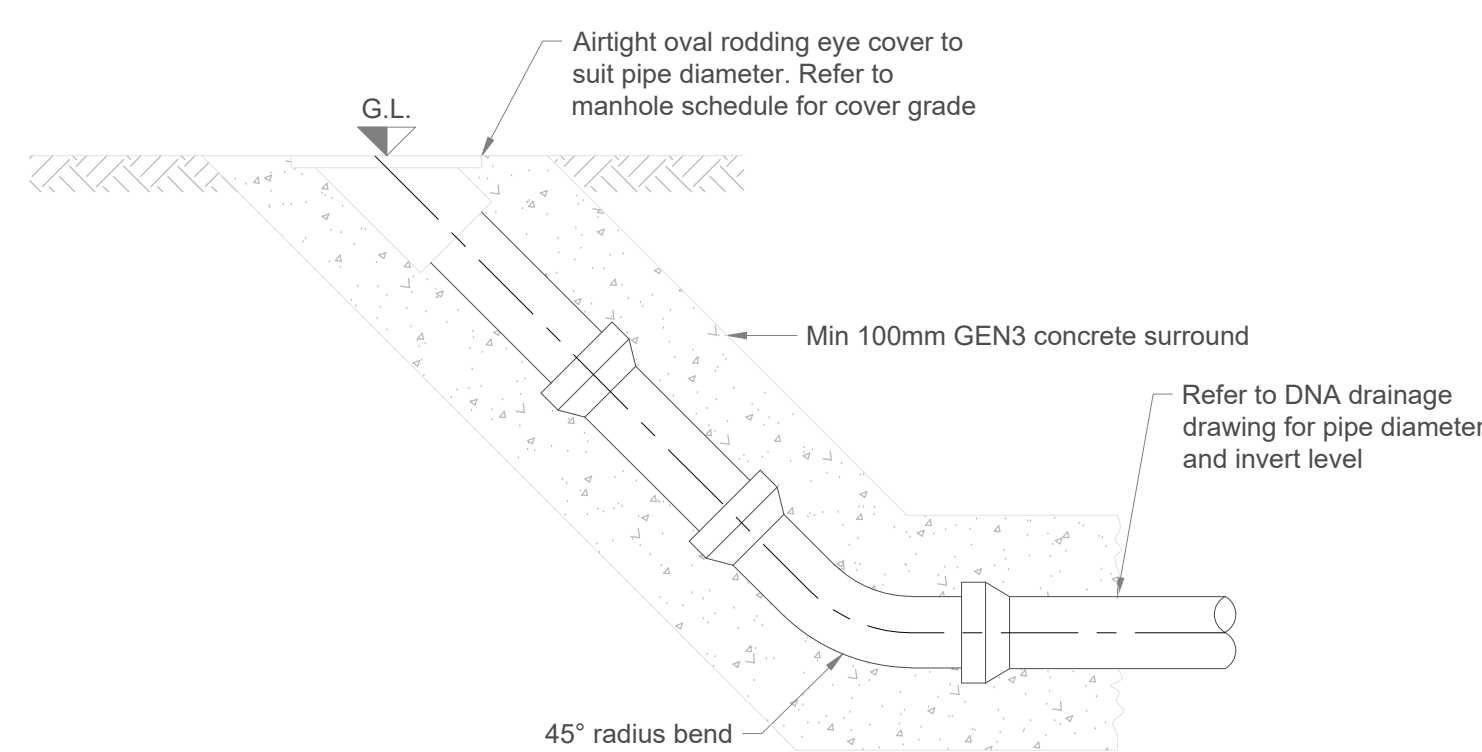
PLAN ON TYPICAL S.W SOAKAWAY & 450Dia SILT TRAP DETAIL

1:20



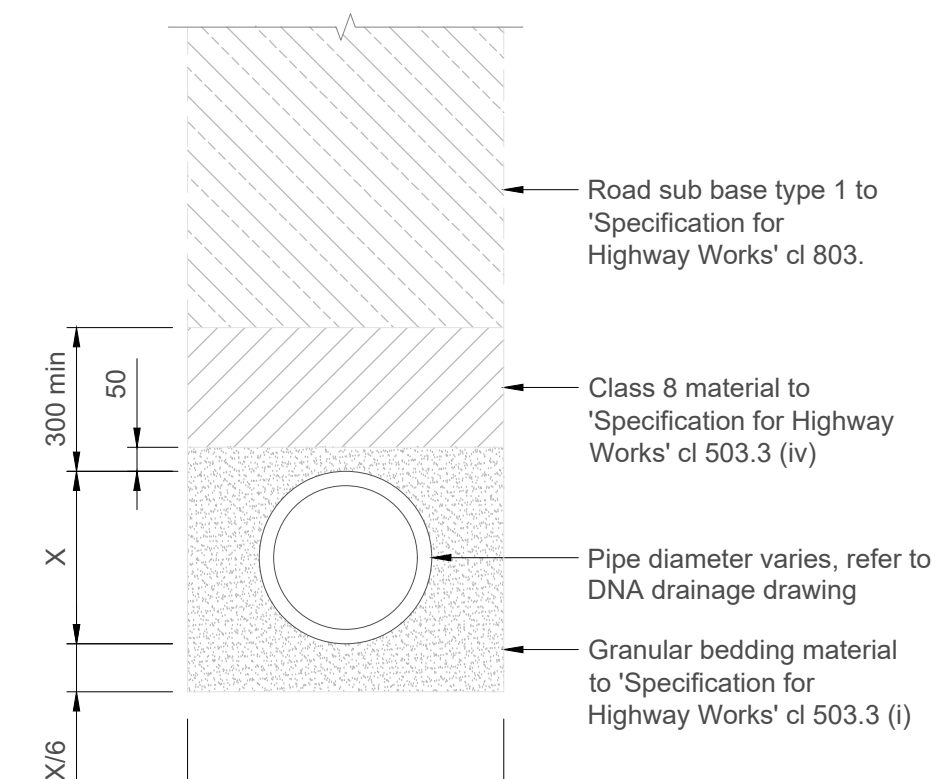
SECTION THROUGH TYPICAL S.W SOAKAWAY & 450Dia SILT TRAP DETAIL

1:20



RODDING EYE DETAIL

1:20



TYPE S PIPE TRENCH & BEDDING

1:20

DO NOT SCALE FROM THIS DRAWING, WORK TO FIGURED DIMENSION ONLY.

**NOTES**

- All DNA drawings are to be read in conjunction with the relevant Architect's and Service Engineer's drawings and Specifications.
- All dimensions and setting out to be confirmed by the Architect.

**MANHOLE NOTES**

- All works, unless noted otherwise, are to be carried out in accordance with Sewers for Scotland (current edition) and to the approval of the local authority.
- Manhole covers are to be:
  - Within trafficked areas:** Saint-Gobain, Inter-AX2 D400-n, heavy duty, double triangular, ductile iron to BS EN 124, 675x675 clear opening, non-rock, or equal approved.
  - Within pedestrians areas only:** Saint-Gobain, Trojan medium duty B125, single piece, ductile iron to BS EN 124, 675x675 clear opening, screw down cover or equal approved.
  - Within building footprint:** Doubled sealed. Refer to manhole schedule for grade.
- All manhole & access covers in open spaces to be ventilated. Refer to manhole schedule for cover grade and location.
- First flexible joints in pipes adjacent to a manhole shall be a maximum of 600mm from inside face of manhole, connecting to rocker pipe. Refer to Sewers for Scotland (current edition) for rocker pipe length.
- Manhole chamber rings shall be precast units to BS:8911; part 200 (cl:4.2.28).
- Manhole chamber rings to be bedded with mortar, proprietary bitumen or resin mastic sealant, or equal approved material. Bedding to be applied in accordance with the manufacturer's instructions.
- Bottom chamber ring bearing on base slab to have minimum 75mm embedment.
- Manhole access hole and step rungs positions to be located to give greatest free areas of benching immediately below.
- Double step rungs to protrude into 675mm square access opening shaft cover slab.
- Double step rungs in accordance with BS EN 13101.
- Top step rung to be located not less than 700mm from the finished manhole cover level.
- Where outlet pipe diameter is greater than inlet, pipe soffits to be the same level unless noted otherwise. A minimum fall of 50mm should be achieved across the manhole in the direction of flow.
- Self-cleaning toe holes to be provided where channels exceed 600mm wide.
- Manholes with outgoing pipes greater than 450mm diameter shall be fitted with guard bars, safety chains or other safety devices.
- All precast concrete parts (i.e. Pipes, manhole rings etc.) are to be design sulphate class ds-2 and chemical design dc-3, in accordance with BRE special digest 1 and BS:5911.
- Refer to site investigation report or structural foundation drawings for in-situ concrete sulphate classification.

Rev	Description	Chk	Drn	Date

Information

Client  
**CRUIVE**

Architect  
**G PATERSON ARCHITECT LTD.**

Job Title  
**GLAMPING HUTS AT MILL PARK CASTLETOWN**

Sheet Title  
**DRAINAGE CONSTRUCTION DETAILS**

Drawn by  
**?**

Designed  
**RE**

Checked  
**?**

Scale  
**AS NOTED**

Date  
**JUNE 22**

Drawing number  
**22.0954 - 520**

Revision  
**-**

Information

David Narro Associates  
Consulting Structural & Civil Engineers

34-36 Argyle Place, Edinburgh, EH1 1JT  
0131 2295553

26 James Morrison Street, Glasgow, G1 5PE  
0141 552690

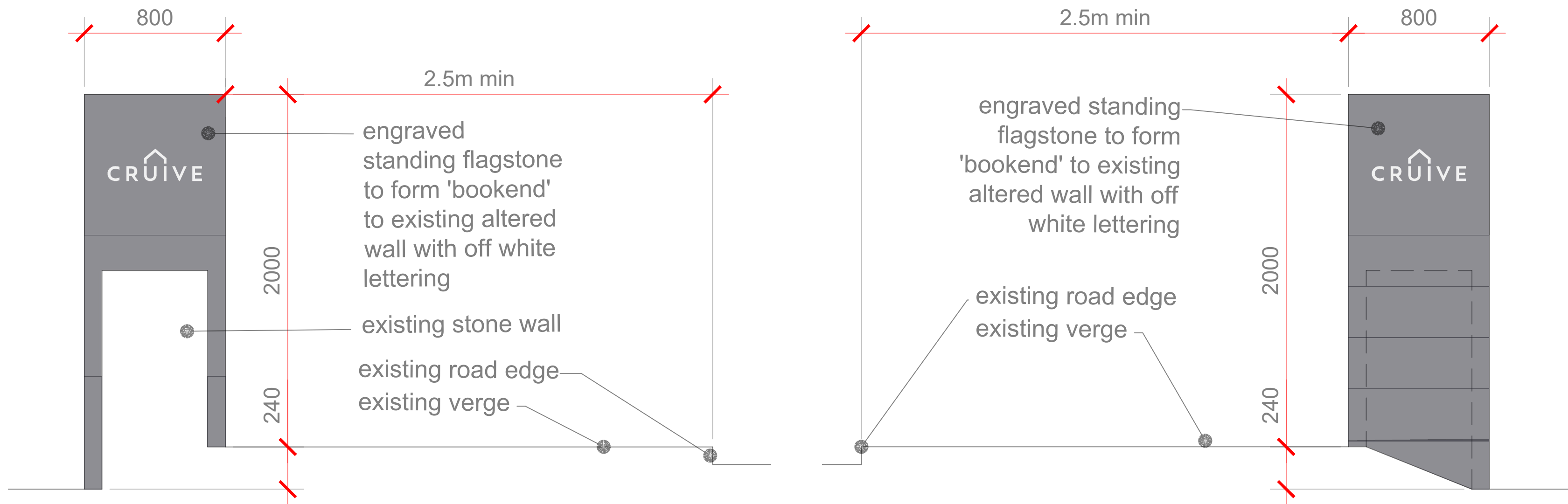
Alton House, 4 Balfour Road, Inverness, IV3 5PJ  
01463 216060

5 Viewfield Place, Stirling, FK8 1NQ  
01786 49562

Banchory Business Centre, Banchory, Aberdeenshire, AB31 5ZU  
01330 862799

W: <http://www.davidnarro.co.uk> E: [mail@davidnarro.co.uk](mailto:mail@davidnarro.co.uk)

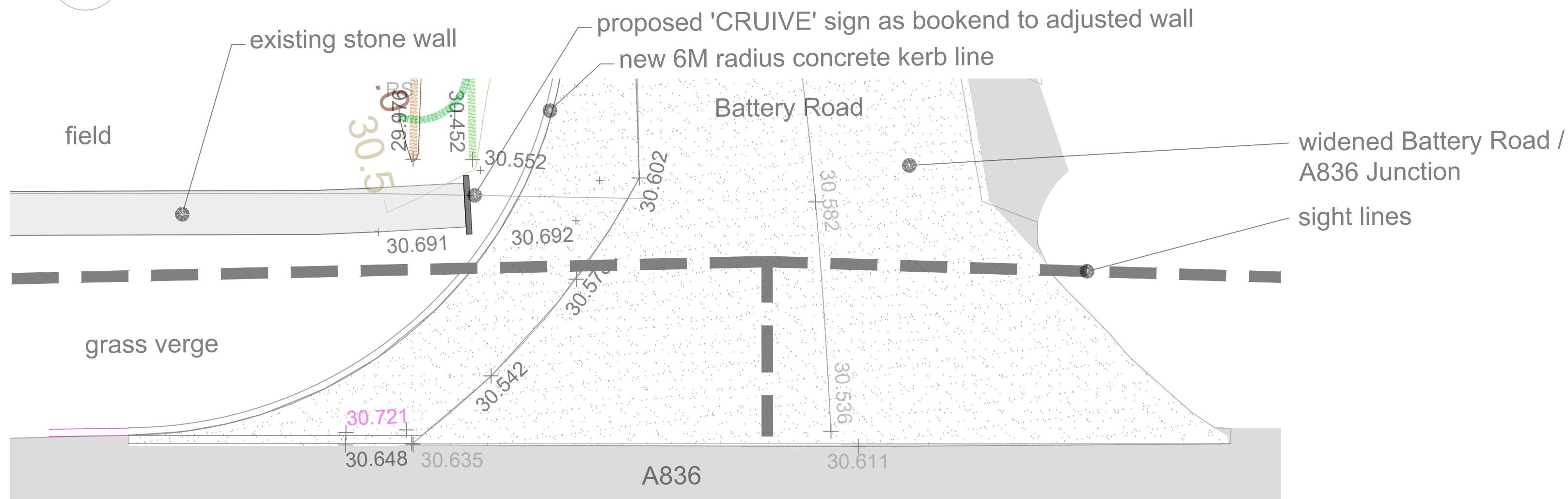




**SIGN DETAIL:**  
 800 x 2000 x 75mm  
 Caithness flagstone  
 'bookend' to adjusted  
 existing dry stone dyke  
 with engraved 'CRUIVE'  
 off white lettering both  
 sides so can be viewed  
 from East and West  
 direction of travel.

01  
C02  
**CRUIVE SIGN VIEWED FROM WEST**  
1:20

02  
C02  
**CRUIVE SIGN VIEWED FROM EAST**  
1:20



03  
C02  
**PLAN SHOWING SIGN LOCATION**  
1:50

Revision	Description	Date
A	'Cruive' sign amended	15.08.22
<b>PLANNING</b>		
G Paterson Architect Ltd		
<small>A: 14 Sandylands Road, Cupar, Fife, KY15 5J5 T: 01334 857666 M: 07771644517 E: gary@patersonarchitect.scot Director: Gary Paterson Company Number: SC482336</small>		
Client <b>Cruive</b>		
Project <b>Bothy Retreat development Battery Road Castletown</b>		
Drawing Title <b>Sign Detail</b>		
Scale 1:20 / 50 @A3	Date July 22	Drawn By GP
Job No 363	Dep. No C02	Rev A





02 SK01 IMAGE KEY DIAGRAM 1:25,000

01 SK01 SERVICE BUILDING FROM BEACH

A	micro wind turbine removed	01.02.23
Revision	Description	Date
STATUS: <b>PLANNING</b>		
<b>G Paterson Architect Ltd</b>		
A: 14 Sandylands Road, Cupar, Fife, KY155JS T: 01334 657666 M: 07771644517 E: gary@gpatersonarchitect.scot Director: Gary Paterson Company Number: SC482336		
Client <b>CRUIVE</b>		
Project <b>Bothy retreat development Battery Road Castletown</b>		
Drawing Title <b>Site office/ service building photo montage</b>		
Scale NTS	Date November 2022	Drawn By GP
Job No. <b>363</b>	Dwg No. <b>SK01</b>	Rev <b>A</b>